

AGENDA

Meeting of the Subdivision Authority Electronic Meeting Tuesday, October 5, 2021 at 3:00 p.m.

A. APPROVAL OF AGENDA

1. Agenda for the October 5, 2021 Meeting of the Subdivision Authority

B. PL20210015

Gateway at Three Sisters Mountain Village Subdivision of Lots 3, 4, 5, Block 18, Plan 0812528 and Lot 1, Block 19, Plan 1510968 into 1 multi-family residential lot, 10 commercial lots, municipal and environmental reserve lots, public utility lots and roads 1.

1. Administration Summary of Changes
2. Questions from the Subdivision Authority
3. Decision

C. ADJOURNMENT

PL20210015 Schedule A

1. All conditions of this Subdivision Approval are to be carried out by the developer to the sole satisfaction and in the sole and absolute discretion of the Town of Canmore unless otherwise indicated in these conditions of approval.
2. The Subdivision shall be effected by registered Plan of Survey. The final Plan of Survey submitted for endorsement shall identify all Environmental Reserves (ER), Municipal Reserves (MR), Public Utility Lots (PUL) and the locations of all rights-of-way required to be registered on title under these conditions of subdivision approval.
3. Prior to construction of municipal services and infrastructure, the Developer shall enter into a Subdivision Servicing Agreement pursuant to Section 655 of the Municipal Government Act (the “Agreement”) to contain among other things:
 - a. The construction, at the Developer’s sole cost, of all roads, water, sanitary, storm systems, pathways, and landscaping as needed to service the subdivision, including the construction of off-site turbo roundabout to provide access to the subdivision; and
 - b. The provision of securities for the performance of the Developer’s obligations under the Agreement in a form and amount acceptable to the Town to be paid at execution of the Agreement.
4. Prior to submitting detailed design the developer shall prepare and have approved a Transportation Impact Assessment prepared by a qualified professional for the subject site that reflects currently approved land uses and densities (i.e. DC1-98 & Resort Centre ASP). The 2020 Global Transportation Impact Assessment can be used as a reference if the unit counts and trip generations can be demonstrated to be approximately equivalent to currently approved land uses and densities.
5. Prior to construction of municipal services and infrastructure, the developer shall submit and have approved detailed engineering drawings prepared by a qualified professional in accordance with the Integrated Transportation Plan, Open Space and Trails Plan and Engineering Design and Construction Guidelines, including roads, water, sanitary, storm water, solid waste, transit facilities, pathways, signage and pavement markings, lighting and shallow utilities. The detailed engineering drawings shall include an Erosion and Sediment Control Plan prepared by a qualified professional in accordance with City of Calgary standards. The design and construction methods must be in accordance with the recommendations of the McIntosh Lalani Ltd. geotechnical report or their recommendations during construction.
6. The developer shall:
 - a. prepare and have approved by the Town of Canmore detailed engineering drawings for the off-site turbo roundabout;
 - b. prepare a VISSIM simulation with the detailed engineering design; and
 - c. submit a roadside development permit to Alberta Transportation for approval of the roundabout on behalf of the Town of Canmore
7. The developer shall provide detailed engineering drawings for all off-site pedestrian crosswalks and pathways around the entire turbo roundabout to the satisfaction of the Town of Canmore prior to submission to Alberta Transportation. The developer shall construct all improvements as identified in the engineering drawings approved by Alberta Transportation.

8. The detailed engineering drawings referenced in #5 shall include a Development Grading Plan prepared by a qualified professional. The grading plan shall be based on ground surveys of the existing site conditions and contain:
 - a. original grades and design grades;
 - b. building grade information for proposed lots;
 - c. all cut and/or fill areas exceeding 1.0 metre will be identified using a colored or shaded gradation by depth;
 - d. surface drainage patterns within and adjacent to the subdivision;
 - e. driveway locations for each lot;
 - f. and all underground utility service stubs to be shown.

Where overland flow is required to be directed over and across adjacent parcels, easements are required to be prepared and registered concurrent with plan of subdivision.

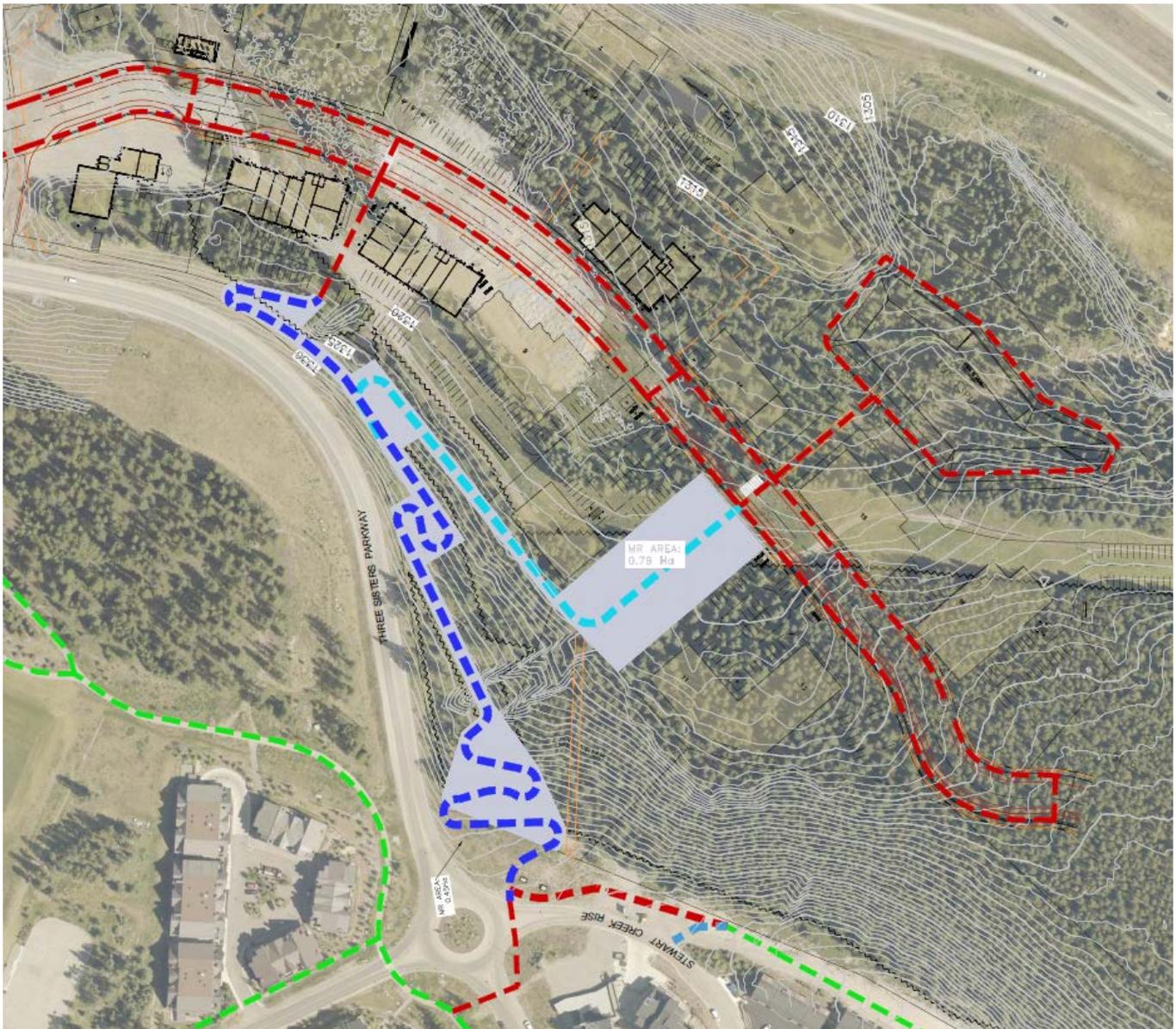
9. The developer shall submit a Steep Creek Hazard study prepared by a qualified professional. The developer shall demonstrate that the recommendations of the study are incorporated in the detailed design.
10. The developer shall submit detailed engineering drawings of the principal commercial street prepared by a qualified professional as conceptually shown in the application submission. The design shall meet the following criteria at a minimum:
 - a. In the locations where the Transportation Impact Assessment prepared by WSP recommends all-way stops, the bend-out pathway configuration shall be removed; Snow storage must be accommodated in the road ROW at intersections and driveways to a similar extent to comparable facilities in the EDGC The design of pathway crossings at intersections and driveways should meet the Town Engineering Design and Construction Guidelines, acknowledging that the EDCG may not have detailed specifications on this subdivision road cross sections or design. Placement of streetlights, planting and furnishings between bike path and sidewalk must facilitate the storage of snow from the vehicular travel lanes and bike path by being placed closer to the sidewalk.
 - b. The number of driveways onto the principal commercial street must be minimized to reduce conflict points with pedestrian and cyclists.
 - c. Proposed access must be shown for 'Lot ~~43~~¹¹' (as identified in the submitted Tentative Plan of Subdivision).
 - d. A swept path analysis shall be provided (WB-20) for each of the intersections and commercial access point within the subdivision area.
 - e. The roadway alignment and/or location of commercial accesses along the principal commercial street shall be adjusted where a bypass for the through lane has been added to accommodate left turns, to the satisfaction of the Town.
 - f. Pathway crossing locations to account for desire lines and connections to external pathways and transit stops.
 - g. Raised crossing design must be updated to include adequate width and pavement markings for shared-use by pedestrians and cyclists.
 - h. Regular level landing areas shall be provided in the space between sidewalk and bike path for sections of roadway that have a longitudinal grade of 5% or higher.
 - i. Pedestrian-level lighting shall be provided within the cross-section.

11. The developer shall submit detailed engineering drawings of the secondary access road prepared by a qualified professional. The design shall meet the following criteria at a minimum:
 - a. The intersection with the principal commercial street to meet at a 90-degree angle.
 - b. A collector road right-of-way must be dedicated to provide walking, cycling and transit connectivity.
 - c. Street lighting must be provided in accordance with the Engineering Design and Construction Guidelines.
 - d. The right -of-way width referenced in (b) above must be level. No retaining walls will be permitted within the road right-of-way.
12. Transit facilities (including shelter, bike racks, waste and recycling receptacles and pedestrian-level lighting) and transit lay-by area shall be included in the detailed design drawings.
13. Private utilities shall be designed to not be located within Town utility rights-of-way for water, sanitary or storm mains.
14. The developer shall design the sanitary system with all new public sanitary mains to be located on public lands, such as road or PUL. Specifically, the section near the sanitary gravity and force mains from the lift station to the road shall be addressed.
15. The developer shall submit detailed engineering drawings of the sanitary lift station that adhere to the specifications of the Engineering Design and Construction Guidelines, the City of Calgary “Wastewater Lift Station Design Guidelines’ and the EPCOR specifications provided to the applicant on March 3 2020.
16. The developer shall design the water system with all new public water mains to be located on public lands, such as road or PUL. Specifically, the loop through ‘Lots 3 and 4’ (as identified in the Tentative Plan of Subdivision) must be removed and the design changed to private service line stubs.
17. The developer shall demonstrate that the water distribution system design is adequately looped for redundancy. The watermain connection to Stewart Creek Phase 3 should be designed to follow the pathway alignment to minimize disturbance, where practical and feasible.
18. The developer shall design the stormwater system to direct any stormwater collected on individual titled lots to the public stormwater mains in the principal commercial street with suitable water quality pre-treatment prior to discharge to the public mains. Private storm water pipes shall not traverse through adjacent lots.
19. As part of the detailed engineering design drawings, the developer shall submit an updated Technical Memorandum addressing the storm pond design required to reduce ponding depth or to manage any consequences of ponding at a depth exceeding 1.5m and shall incorporate any recommendations from the Technical memorandum into the detailed engineering design drawings. Concurrent with the detailed engineering design submission, the developer shall submit an analysis of the frequency of ponding exceeding 100mm in the storm pond, along with an assessment of potential impacts to downstream water quality should the pond be proposed for use as a dog park.
20. The developer shall submit detailed engineering drawings of the emergency access prepared by a qualified professional in accordance with the Engineering Design and Construction Guidelines. The emergency access shall be designed for all-seasons access by emergency vehicles. The developer must prepare an access right-of-way and maintenance agreement for the emergency access to the satisfaction of the Town of Canmore, which shall be registered concurrent with the plan of subdivision.
21. The developer shall liaise with the shallow utility providers to determine their requirements for maintenance and expansion of the shallow utilities and to incorporate these requirements into the detailed engineering design.

22. The developer shall design and construct, as conceptually shown in Figure 1:
- a 3.5 m asphalt pathway connecting from the existing pathways at the Stewart Creek roundabout along the general alignment of existing URW Plan 021 2809, to the principal commercial street.
 - a 3.5 m asphalt pathway connecting to the pathway in (a) above to a location further southeast along the principal commercial street, connecting to the stormwater pond trail.
 - A gravel trail around the stormwater pond.

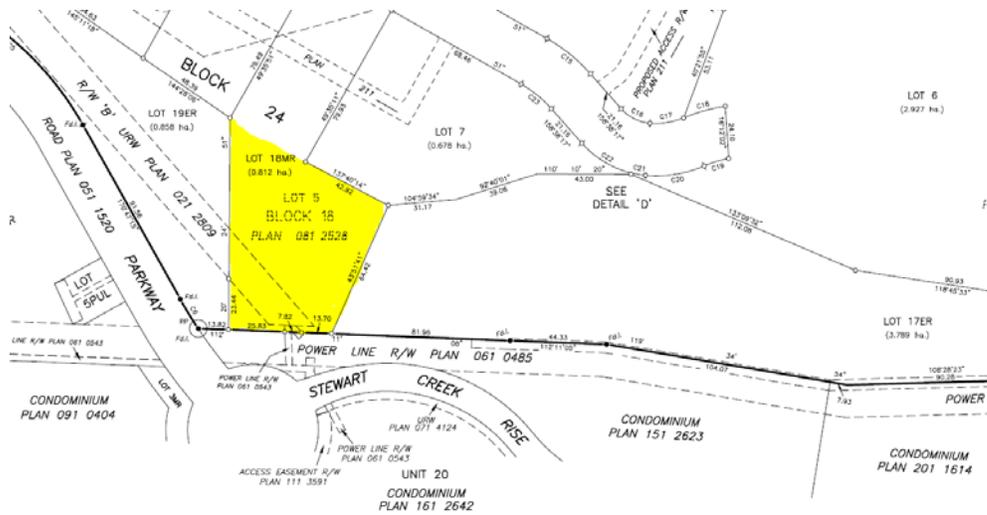
If during the detailed design process the developer proposes an alternative alignment for the pathways in (a) and (b) which satisfies the Town's EDCG and the design criteria in condition #23, it may be considered for acceptance by the Town of Canmore.

Figure 1.



23. The design of pathways in Condition #~~21-22~~ shall meet the following criteria:
 - a. accommodate maintenance by snow-clearing equipment;
 - b. avoid being aligned in such proximity to Three Sisters Parkway that roadway snow clearing equipment will throw snow onto people using the trail system below;
 - c. include regular level landing areas adjacent to the pathways where longitudinal grade exceeds 5%; and
 - d. include pedestrian level lighting.
24. A Landscaping Plan shall be prepared by a qualified professional in accordance with the Engineering Design and Construction Guidelines. The Landscaping Plan shall show all proposed Municipal Reserve, public utility lots, utility rights-of-way, boulevards, medians, pathways and trails.
- ~~25.~~ Prior to construction of municipal services and infrastructure, a Construction Management Plan, prepared by a qualified professional shall be submitted to the satisfaction of the Town of Canmore.
- ~~25-26.~~ The detailed engineering drawings shall identify the location and details of the street lighting to be provided by the developer. The developer shall construct the street lighting at its own expense.
- ~~27.~~ Prior to endorsement of the Plan of Survey, the developer shall have accepted CCC for the water and sanitary systems, the principal commercial street and secondary access road needed to service the subdivision in accordance with the approved detailed engineering drawings.
- ~~26.~~ Prior to endorsement of the Plan of Survey, the developer shall have constructed or paid to have constructed all on-site and off-site municipal services and infrastructure including roads, water, sanitary, storm systems, pathways and shallow utilities as needed to service the subdivision in accordance with the approved detailed engineering drawings.
- ~~27-28.~~ The developer shall grade the subdivision area and construct all stormwater conveyance systems in accordance with the approved detailed engineering drawings and Stormwater Management plan to ensure proper stormwater management. At or before the FAC stage, the developer shall provide the Town with any plans or maintenance documentation of the stormwater management facilities not already in the possession of the Town of Canmore, where required by the Town of Canmore at the CCC stage.
- ~~28-29.~~ Any slopes resulting from grading of this subdivision that exceed 1:3 shall require slope stability confirmation from a qualified engineer. Any retaining walls resulting from grading of this subdivision that exceed 1m shall be designed and certified by a qualified engineer.
- ~~29-30.~~ The developer shall install all transit facilities, traffic and road signs in accordance with the approved detailed engineering drawings.
- ~~30-31.~~ The developer shall prepare the necessary plans and documentation to grant new or amend existing easements in favour of shallow utility service providers, including but not limited to telephone, cable, electricity and natural gas, as approved on the detailed engineering drawings. All required easements shall be registered concurrent with the plan of subdivision.
- ~~31-32.~~ The boundary of Lot ~~18MR-16MR~~ as shown on the submitted Tentative Plan of Subdivision shall be adjusted to adequately consider steep and unstable slope conditions. The approximate 0.5 hectare area of steep and unstable land as shown in Figure 2 shall be designated Environmental Reserve (ER).

Figure 2



32.33. Prior to endorsement, the developer shall have prepared by a qualified professional a geotechnical investigation of the slopes within and adjacent to Lot ~~14MR~~14MR. The report should address slope stability and methods for slope stabilization where required. The Town may require the boundary of the proposed Lot ~~14MR~~14MR and Lot ~~17ER~~15ER to be adjusted to accommodate for identified unstable slopes. The developer shall construct any recommended mitigations to stabilize the slopes and trail.

33.34. The developer shall provide 10% of the calculated developable area of lands as Municipal Reserve to the satisfaction of the Town. The approximate area of Municipal Reserve lands shall be equal to approximately 1.3 hectares. The Municipal Reserve dedication must include:

- a. 0.2 hectare deferred reserve caveat to be registered on title of Lot 6, Block 24.
- b. 0.196 hectares of a portion of Lot ~~14MR~~12MR, Block 24, and up to 0.32 hectares if the developer proposes to use the storm pond for a dog park and can provide engineering evidence of the storm facility being acceptable for a dog park use. Only those lands less than 5:1 slope will be accepted as MR within the lot identified as Lot ~~14~~12, Block 24.
- c. 0.203 hectares for portions of Lot ~~16MR~~14MR, Block 24 which are less than 5:1 slope and are not deemed unstable lands in the geotechnical report required pursuant to ~~#34~~#32.
- d. 0.32 hectares for areas within Lot ~~18MR~~16MR that do not qualify as Environmental Reserve, for passive park or pathway connections.
- e. 0.4 hectares minimum for an off-leash dog park, in a location deemed suitable at the discretion of the Town, if (b) above cannot be met.
- f. Other outstanding MR requirements must be used for pathway connections and local passive parks.

34.35. Prior to endorsement of the Plan of Survey, Substantial Performance or equivalent CCC for the turbo roundabout shall have been issued by Alberta Transportation~~accepted by the Town~~.

35.36. Prior to endorsement, the developer shall prepare a Restrictive Covenant for applicable development setbacks to steep slopes in accordance with the McIntosh Lalani Engineering Ltd. geotechnical investigation, or for any lots noted in a subsequent investigation undertaken as a condition of this approval at detailed design. The Town of Canmore must be a party to the Restrictive Covenant

indicating it cannot be discharged without the Town's consent and the covenant shall be registered concurrent with the plan of subdivision.

~~36.37.~~ Prior to endorsement, payment of all third-party costs incurred by the Town in reviewing all engineering, environmental, legal, planning documents and information submitted by the applicant.

~~37.38.~~ Prior to endorsement, all outstanding taxes to be paid to the Town pursuant to Section 654 of the Municipal Government Act.

~~38.39.~~ Prior to endorsement, the developer shall prepare an easement or other such agreement for the proposed snow storage area on Lot 7 for use by Lots 1 through ~~1311~~, Block 24 as identified on the drawings submitted in the application. The easement must be registered concurrent with the plan of subdivision.

~~39.40.~~ Prior to endorsement, the developer shall prepare any access rights-of-way or easements, or utility rights-of-ways where deemed necessary by the Town for public infrastructure or public access for registration concurrent with the plan of subdivision.

~~40.41.~~ Prior to endorsement, the developer shall prepare shared driveway access easements, where required. The easements must be registered concurrent with the plan of subdivision.

~~41.42.~~ The developer shall provide for community mailboxes in accordance with the specifications provided by Canada Post in a location determined in consultation with Canada Post.

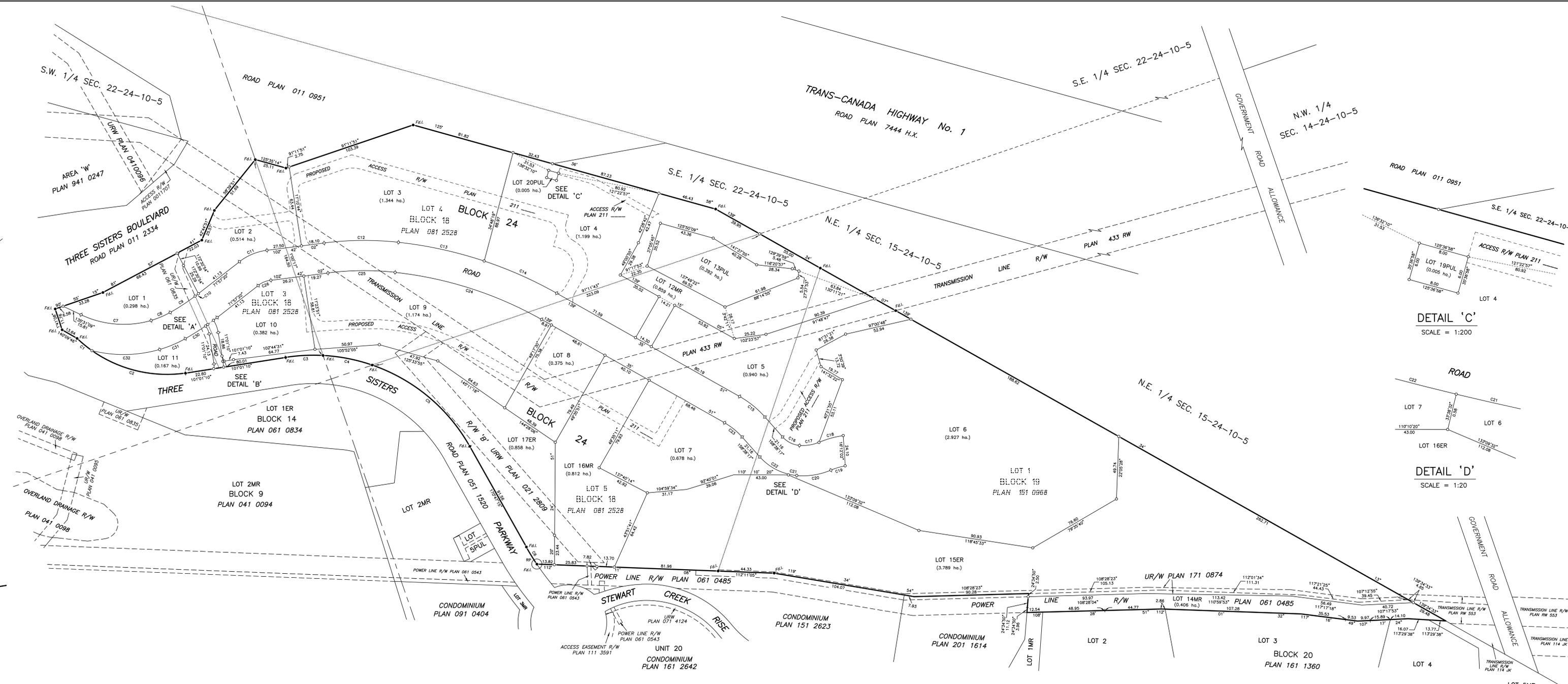
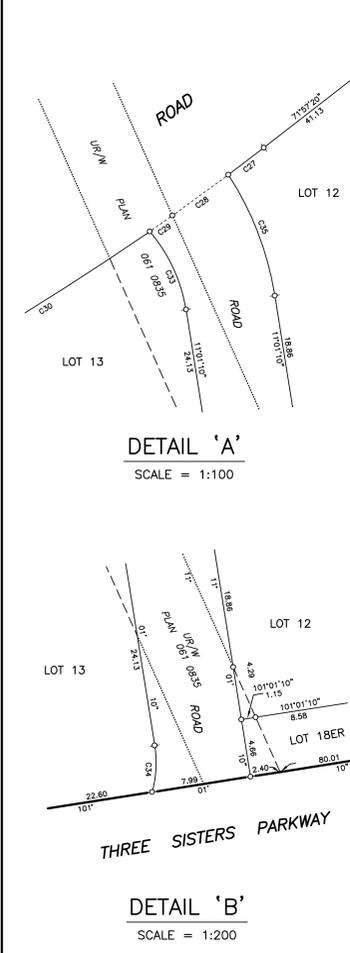
Advisory Comments

The developer is encouraged to consider engaging with the Stoney Nakoda Nation to establish Indigenous Street names.

Reasons for Decision:

1. The proposed subdivision is in general conformance with the Stewart Creek Area Structure Plan.
2. The proposed use of the subject lands is compatible with adjacent uses.
3. Access to the proposed lots will be adequate and from public roads.
4. The proposed parcels will be served by municipal services.
5. The topography and soil characteristics are appropriate for development.
6. Impacts of flooding, subsidence, or erosion of land can be adequately mitigated.
7. Stormwater collection and disposal is adequate.
8. The subject land is suitable for the purposes for which the subdivision is intended.
9. Adequate municipal reserve is provided by way of dedication and deferred reserve caveat as required in the conditions of subdivision.

CURVE	RADIUS	ARC	CHORD	BEARING	DELTA
C1	102.450	15.333	15.319	169.9804°	87.0109°
C2	102.450	77.845	75.986	123.3625°	43.3208°
C3	141.250	29.689	29.433	107.0135°	12.0233°
C4	141.250	35.641	35.511	131.0515°	16.0437°
C5	141.250	102.715	100.467	149.6710°	41.3953°
C6	208.750	15.951	15.947	168.5845°	4.2241°
C7	78.000	56.766	55.522	114.4015°	41.4154°
C8	89.500	16.362	16.324	87.0436°	13.2918°
C9	183.000	23.568	23.522	76.9834°	7.2245°
C10	183.000	3.187	3.187	22.2716°	0.9952°
C11	44.500	23.879	23.883	87.1941°	35.4492°
C12	249.000	58.746	58.619	109.2733°	13.3111°
C13	324.000	89.432	89.319	122.2140°	12.1655°
C14	324.000	62.742	62.644	134.0259°	11.0543°
C15	78.000	25.921	25.802	149.0704°	19.0228°
C16	20.000	15.225	14.880	138.4946°	43.3701°
C17	32.000	15.736	15.590	105.9554°	28.1042°
C18	76.203	19.918	19.861	84.1951°	14.5834°
C19	52.203	11.803	11.778	83.1511°	12.5177°
C20	56.000	27.541	27.264	100.5554°	28.1042°
C21	44.000	6.621	6.615	119.1954°	8.3717°
C22	44.000	26.975	26.459	141.9929°	34.9944°
C23	54.000	17.945	17.863	149.0704°	19.0228°
C24	300.000	122.402	121.855	129.5435°	23.2238°
C25	225.000	53.092	52.969	109.2733°	13.3111°
C26	20.500	11.000	10.869	87.1941°	30.4442°
C27	207.000	3.989	3.989	72.2713°	0.9952°
C28	207.000	5.561	5.561	73.4313°	1.3229°
C29	207.000	2.215	2.215	74.4752°	0.5649°
C30	207.000	16.988	16.981	72.4306°	5.1341°
C31	83.500	22.012	21.961	87.0436°	13.2918°
C32	102.000	54.153	53.919	109.0140°	32.2538°
C33	12.750	7.028	6.940	175.1335°	31.3510°
C34	9.000	3.793	3.785	23.0533°	24.0854°
C35	25.200	10.599	10.433	179.0420°	23.5411°



LAND TITLES OFFICE

PLAN NO. _____

ENTERED AND REGISTERED ON _____

INSTRUMENT NO. _____

A.D. REGISTRAR

CANMORE, ALBERTA

TENTATIVE PLAN

SHOWING A PROPOSED SUBDIVISION OF LOT 3, LOT 4, AND LOT 5, BLOCK 18, PLAN 081 2528, AND LOT 1, BLOCK 19, PLAN 151 0968 ALL WITHIN THE N.E.1/4 SEC.15, TWP.24, RGE.10, W5M. AND S.1/2 SEC.22, TWP.24, RGE.10, W5M.

BY: WARREN LIPPITT, 2020

SCALE = 1:1000

- LEGEND:**
- STATUTORY IRON POST FOUND ● F.S.I.
 - SURVEY MONUMENTS UNLESS SHOWN OTHERWISE ○ P.L.I.
 - ENVIRONMENTAL RESERVE ER
 - MUNICIPAL RESERVE MR
 - UTILITY RIGHT OF WAY UR/W
 - RIGHT OF WAY R/W
 - CHORD CH
 - RADIAL BEARINGS RB
 - RADIUS R
 - CENTRAL ANGLE OF CURVE Δ
 - SECTION SEC.
 - TOWNSHIP TWP.
 - RANGE RGE.
 - MERIDIAN M.
 - HECTARE ha.
 - GEO-REFERENCE POINT ○ RP

NOTES:

DISTANCES SHOWN ARE IN METRES AND DECIMALS THEREOF AND ARE BETWEEN SURVEY MONUMENTS UNLESS SHOWN OTHERWISE.

BEARINGS ARE GRID AND ARE DERIVED FROM GNSS TIES TO ALBERTA SURVEY CONTROL MARKERS ASCM 404251 (MkI, 404251) AND ASCM 130302 (MkI, 130302).

A COMBINED SCALE FACTOR OF 0.999790 CAN BE APPLIED TO REDUCE DISTANCES TO MEAN SEA LEVEL AND THE PROJECTION PLANE FOR CALCULATION OF 3TM COORDINATES.

COORDINATES ARE 3TM, BASED ON NAD83(ORIGINAL) DATUM AND REFERENCE MERIDIAN 114° WEST LONGITUDE.

AREA AFFECTED BY THE REGISTRATION OF THIS PLAN SHOWN BOUNDED THUS: AND CONTAINS: 18.77 ha.

THE 3TM COORDINATES OF THE GEO-REFERENCE POINT ARE: NORTHING: 5657898.87 EASTING: -92112.08

SURVEYOR:

NAME: WARREN LIPPITT, A.L.S. AND _____

SURVEYED BETWEEN THE DATES OF _____ AND _____ IN ACCORDANCE WITH THE PROVISIONS OF THE SURVEYS ACT.

REGISTERED OWNERS:

THREE SISTERS MOUNTAIN VILLAGE PROPERTIES LTD.

SUBDIVISION AUTHORITY:

NAME: TOWN OF CANMORE

FILE NO. _____

McElhanney
Land Surveys (Alta.) Ltd.

203 - 502 BOW VALLEY TRAIL, CANMORE, ALBERTA T1W 1N9
PH (403) 678-6363, FAX (800) 407-3895

FILE: 20-169 20-109750-0-3TM.DWG DATE PLOTTED: APR/22/2021