

Agenda
Canmore Planning Commission
Canmore Civic Centre – Council Chambers
Wednesday, April 26, 2023, at 2:00 p.m.

- A. Call to Order
- B. Approval of Minutes of the March 1, 2023, meeting
- C. Development Permit Application:
 - 1. PL20220303
1734 Bow Valley Trail
Block A, Plan 6122JK
98 Unit Visitor Accommodation with one Commercial Unit
Variances: Building Height, Eaveline Height, Floor Area Above Eaveline
 - 2. PL20230005
1 Silvertip Trail
Lot 1, Block 17, Plan 971 1512
Excavation, Stripping and Grading
No Variances
- D. Other Planning Business
None
- E. Next Scheduled Meeting
May 31, 2023
- F. Adjourn

**TOWN OF CANMORE
MINUTES
Canmore Planning Commission
Council Chambers
Wednesday, March 1, 2023, at 2:00 p.m.**

MEMBERS PRESENT

Whitney Smithers (Chair)	General Manager of Municipal Infrastructure
Florian Jungen	Public Representative
Miranda Spessot	Public Representative
Brian Talbot	Public Representative
Tanya Foubert	Councillor
Jeff Hilstad	Councillor

MEMBERS ABSENT

Shawn Kennedy	Public Representative
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ADMINISTRATION PRESENT

Lauren Miller	Planning & Development Manager
Riley Welden	Development Planner
Jolene Noël	Secretary (Recorder)

CALL TO ORDER

Chair W. Smithers called the Wednesday, March 1, 2023, regular meeting to order at 2:01 p.m.

VOTE FOR VICE CHAIRPERSON

It was moved by B. Talbot that T. Foubert be elected as Vice Chairperson. Member T. Foubert accepted this nomination. There were no other nominations.

**MOTION CARRIED
UNANIMOUSLY**

ADOPTION OF AGENDA

Chair W. Smithers moved that the Agenda of the Canmore Planning Commission meeting of March 1, 2023, be adopted.

**MOTION CARRIED
UNANIMOUSLY**

ADOPTION OF MINUTES

Chair W. Smithers moved that the adoption of the October 26, 2022, minutes of the Canmore Planning Commission meeting be approved as amended with the correction to the spelling of Mr. Burch's name.

**MOTION CARRIED
UNANIMOUSLY**

DEVELOPMENT PERMIT APPLICATION

Development Permit Application:

PL20220305

1736 Bow Valley Trail

Block 1, Plan 941 0149

98 Unit Visitor Accommodation

Variances to building height, eaveline height and front yard setback

ADMINISTRATION'S PRESENTATION OF THE APPLICATION

Administration presented a visual and verbal presentation of the application.

APPLICATION QUESTIONS FROM THE BOARD

Administration answered questions from the Commission.

QUESTIONS/COMMENTS FROM THE APPLICANT AND APPLICANT SPOKESPERSON

The Applicant, Chris Sparrow with MTA Architecture, spoke to the application and answered any questions from the Commission.

QUESTIONS/COMMENTS FROM THE PUBLIC.

None.

DECISION

Chair W. Smithers moved that application PL20220305 be **APPROVED** with the conditions attached in the Schedule A.

**MOTION CARRIED
ANIMOUSLY**

OTHER PLANNING BUSINESS

None.

NEXT SCHEDULED MEETING

March 30, 2023

ADJOURNMENT

Chair W. Smithers moved that the meeting be adjourned.

**MOTION CARRIED
2:47 P.M.**

Whitney Smithers, CPC Chair

Jolene Noël, Secretary

CANMORE PLANNING COMMISSION

STAFF REPORT



DATE:	APRIL 26, 2023
PROPOSED DEVELOPMENT:	98-UNIT VISITOR ACCOMMODATION, ONE (1) RETAIL SALES UNIT AND ONE (1) EMPLOYEE HOUSING UNIT
APPLICATION NUMBER:	PL2020303
LEGAL DESCRIPTION:	BLOCK A, PLAN 6122JK
CIVIC ADDRESS:	1734 BOW VALLEY TRAIL
CURRENT USE(S):	RV PARK AND CAMPGROUND
APPLICANT:	MTA URBAN DESIGN AND ARCHITECTURE
REFERENCED DOCUMENTS:	LAND USE BYLAW 2018-22: <ul style="list-style-type: none">• SECTION 4.6 BOW VALLEY TRAIL GENERAL COMMERCIAL DISTRICT (BVT-G)• SECTION 11.0 COMMUNITY ARCHITECTURE & URBAN DESIGN STANDARDS

EXECUTIVE SUMMARY

This application proposes a 98-unit Visitor Accommodation building with one Retail Sales unit and an Employee Housing Unit at 1734 Bow Valley Trail. The following four variances are requested: increase maximum building height; increase maximum building area over building height; increase of maximum eave height; and decrease minimum percent of building elevation step-back above eave height. The building height and eave height variances are contemplated under Section 1.14 and the "Special Variance Provisions" of the Bow Valley Trail General Commercial District. Administration recommends approving the application, as the proposed development aligns with the policy direction of the Municipal Development Plan (MDP), Bow Valley Trail Area Redevelopment Plan (BVT ARP), and the Land Use Bylaw (LUB).

BACKGROUND

Municipal Development Plan

The Municipal Development Plan (MDP) divides Canmore into various conceptual land use areas (MDP, Map 2). This site is located in the Tourist Service area (MDP, Map 5). Relevant policy direction includes:

10.4.1 Tourist/Service commercial areas should provide commercial accommodation developments and a variety of limited size retail and service uses that serve visitors and residents.

Bow Valley Trail Area Redevelopment Plan

The site is also within the Bow Valley Trail Area Redevelopment Plan (BVT ARP), in the General BVT precinct. Relevant policies include the following:

6.2.1 Purpose

The General BVT Precinct is intended as a commercial area primarily focused on serving visitors. Historical development patterns have resulted in this area having a large number of visitor accommodation units. The main floor of buildings should be reserved for uses that require and facilitate strong interaction with people on the street, such as hotel entrances, small retail stores, or eating establishments. Particular emphasis should be placed on the BVT frontage of properties. Uses that are predominantly focused on serving visitors are encouraged in this precinct.

6.2.4.1 Efficient Use of Land

The efficient and aesthetic development of properties within the BVT area is important to maximize the use of limited commercial lands, to promote the desired streetscape and to generate a critical mass of residents, businesses and visitors that create synergies with each other.

7.1.1 Continuous Pedestrian and Multi-Use Pathways on Both Sides of the Road

Multi-use pathways are the preferred alternative to accommodate pedestrian and some cyclist movement throughout the Bow Valley Trail (BVT) area. These should be built on both sides of Bow Valley Trail. These pathways do not need to be straight, but can meander within the right-of-way, and need not be tied to the curb like a traditional sidewalk, unless adjacent to on street parking. Moving the paths away from the traffic flow will increase the sense of safety for pedestrians. These paths should be wide enough to allow two strollers/burleys to comfortably pass each other.

EXISTING SITE

1734 Bow Valley Trail is designated BVT-G Bow Valley Trail General Commercial District, which is intended to provide a variety of commercial uses including visitor-oriented services and activities, and small retail outlets that serve both residents and visitors.

Visitor Accommodation and Retail Sales are both listed as Permitted Uses, while Employee Housing is listed as a Discretionary Use; therefore this application aligns with the purpose and use provisions of this district. The site fronts onto Bow Valley Trail and is one of the few remaining lots that has not dedicated land for road widening. The front property line is currently located closer to the road and trail surfaces than the surrounding parcels.

Please see Attachment I for images of the site. As a result, a 12.8 m setback as per LUB Section 2.4.7.1 is provided to accommodate the future road widening for Bow Valley Trail.

Adjacent uses include (see Attachment I):

- Visitor Accommodation developments to the north, south and east; and
- The Canmore Recreation Center to the west, across the CP Rail Line.

BYLAW CONFORMANCE/VARIANCE DISCUSSION

There are 90 parkade and 10 surface vehicle stalls proposed, which is consistent with the minimum and maximum parking standards. In accordance with Section 2.7.3.1 a loading stall was not provided. Section 2.7.3.1 states the following:

A minimum of one (1) loading stall shall be required per multi-unit residential building or non-residential building, unless it can be otherwise demonstrated to the Development Authority that loading can reasonably take place without a dedicated stall on-site, or that it can be shared amongst multiple developments and/or tenants to minimize the number of loading stalls required.

The proposed Visitor Accommodation development will not experience frequent deliveries or loading/unloading of goods that other commercial businesses may require. Apart from the initial delivery of furniture and appliances upon construction, delivery/loading is expected to be infrequent for the Visitor Accommodation use on this site. If these deliveries do occur, it is reasonable to expect this parking demand can be managed onsite. The development also proposes a small commercial retail unit for retail sales. The demand for deliveries for this use is low and is expected to occur outside of business operating hours.

The retail unit requires two parking spaces, which are provided on the surface parking lot. Delivery vehicles, which are expected to be a passenger van or cube van, can utilize one of these stalls when making deliveries to the business outside of business hours. A condition requiring this has been added to Schedule A. The Development Authority was satisfied loading can reasonably take place without a dedicated on-site stall through low loading demand and onsite parking management. As a result, Section 2.7.3.1 was considered met. No variance was required due to development authority discretion being integrated within this section.

The application aligns with policies of the Bow Valley Trail Area Redevelopment Plan and Municipal Development Plan as it is a commercial hotel development that intensifies an existing site in the Bow Valley Trail area. The development meets the requirements of the Community Architectural & Urban Design Standards and contributes to the Rocky Mountain architectural character. Some of the key elements are steeply pitched roofs, stepping back the upper floor (partially), a distinct base, middle and top, appropriate detailing, a great use of natural materials, relief and texture in the building facades, and acceptable colour palate. The development proposes a retractable glass balcony enclosure system that allows outdoor balcony use in all seasons. Although this system increases the window glazing on the building, it allows for the architectural elements to be seen and therefore does not detract from Canmore's Traditional Rocky Mountain architectural character.

The development includes a small retail unit that will provide services to both residents and visitors. The retail unit and visitor accommodation have entrances that face the street but are setback interior to the site when compared to the front building façade. The entrances will be visible from the street and a pedestrian/bicycle pathway will lead directly from the trail to the entrances (see Attachment 4).

The proposed development includes improvements to the pedestrian and bicycle trail system by including a new trail/sidewalk adjacent to the existing Legacy Trail pathway. The trail/sidewalk will help reduce conflict between different users (e.g. pedestrians and cyclists). This also aligns with policy from the BVT ARP to create Continuous Pedestrian and Multi-Use Pathways.

Section 4.6.5.5 states *the ground floor of developments for sites lower than the crown of road should be raised to be closer to the elevation of the crown of the road.*

The building is located slightly lower than the crown of the road, and in this case, it is not practical to raise the building without significant re-grading. The purpose of this regulation is to align the building with the road surface elevation to allow for easy access to the businesses/shops that may be located adjacent Bow Valley Trail. Given this development is a Visitor Accommodation with one commercial retail unit, and is located over 13.0 m away from the pathway, the proposed grade is appropriate.

The development complies with the land use requirements and regulations, with the exception of the 16.0 m maximum building height, 7.0 m maximum eaveline height, and the requirement for 70% of the floor area above the eaveline to step back from the floor below.

The proposed development does not conform with the Town of Canmore Land Use Bylaw 2018-22 and requires the following variances:

I. Maximum Building Height and Eaveline Height

Variance Authority

Section 1.14 provides limitations and considerations for variances related to building height. Building Height variances up to 20% can be considered by CPC. When considering variances to building height and/or eavelines, the following impacts may be considered:

- Access to light for neighbouring properties;
- Protection of views of neighbouring properties;
- Privacy of neighbouring properties; and
- Site constraints.

The BVT-G District includes “Special Variance Considerations”, which include the following:

4.6.9.1 Notwithstanding the variance regulations set out in 1.14, the following variances may also be considered within this District:

- a. *Where the Development Authority is satisfied that the architectural integrity of a building would be enhanced, variances may be granted to allow 20% of the building to exceed the maximum height by up to 20%. No height variances shall be granted beyond the 20% relaxation.*
- b. *The required eaveline height of a proposed development may be raised to 9.0 m where the Development Authority is satisfied that the intent of the “Massing and Scale Guidelines” in Section 11: Community Architectural and Urban Design Standards, are still met.*

The application proposes the following:

- a. 19.2 m building height (3.2 m variance = 20%);
- b. 57.9% of building over 16.0 m building height (37.9% variance); and
- c. 9.0 m front façade eaveline height (2.0 m variance) and a 13.0 m eaveline height along the side and rear facades (6.0 m variance).

Section 1.14.1.1- Discretion of the Development Authority, allows the Canmore Planning Commission to grant a variance to maximum building height up to 20%. Section 1.14.1.2 provides the Development Authority the discretion to grant variances to other regulations and guidelines in the Land Use Bylaw with no limitation. Therefore, the Canmore Planning Commission has the authority to grant the variances requested.

Discussion

Section 4.6.9.1 allows for 20% of a building to exceed the maximum height requirement to a maximum of 20% where the architectural integrity of the building would be enhanced. The proposed 19.2 m building height at the peak is 20% over the 16.0 m maximum height requirement. Also, 57.9% of the roof area is located above the 16.0 m maximum height requirement (See Figure 1).

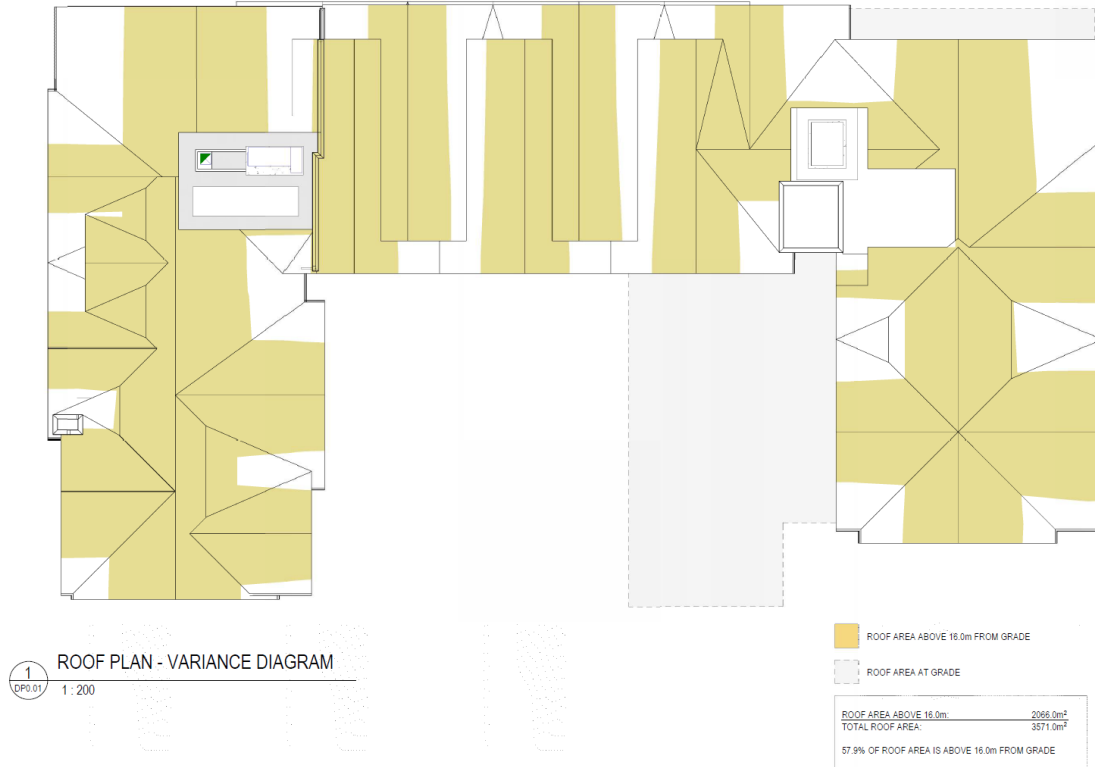


Figure 1: Roof Plan Showing Area Over 16.0m Height

The application requires a relaxation to allow 57.9% of the building to exceed the 16.0 m height requirement. The building roof design incorporates multiple steeply pitched roof peaks and lines that complements and reflects Canmore’s Rocky Mountains and architectural character. The roof incorporates a cascade of roofs along the front (north) façade as well as wood cross beams along all facades that breaks up the perception of building massing and adds visual interest.

Additionally, this regulation allows the eaveline to be raised to 9.0 m where the massing and scale guidelines are met. The application proposes to raise the eaveline to 9.0 m along the front building façade facing Bow Valley Trail (See Figure 2) and to 13.0 m along the other building facades (See Figure 3).

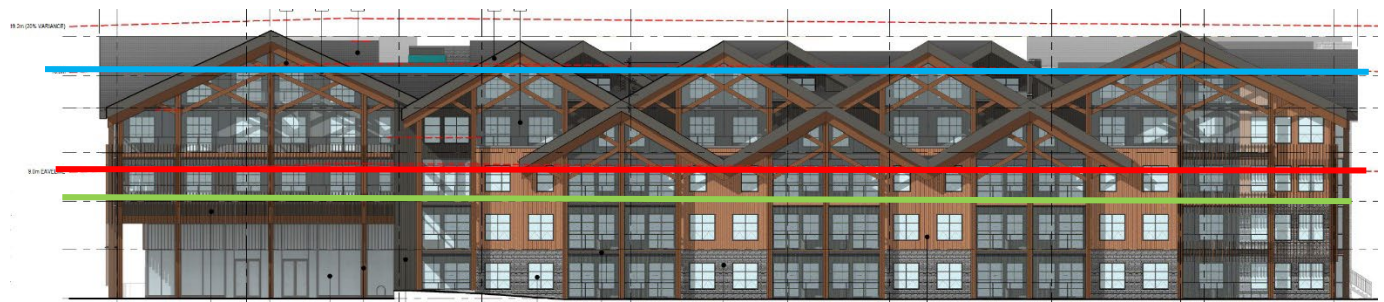


Figure 2: Front (North) Elevation Showing 7.0m Maximum Eaveline Height in Green, Proposed 9.0m Eaveline Height in Red, and 16.0m Maximum Building Height in Blue.



Figure 3: Side (West) Elevation Showing 7.0m Maximum Eaveline Height in Green, Proposed 13.0m Eaveline Height in Red and 16.0m Maximum Building Height in Blue

The 9.0 m eaveline is consistent with the parameters of this regulation, however the 13.0 m eaveline is beyond what the regulation contemplates. To address the massing and scale impacts of the additional eaveline height, the building incorporates a high degree of articulation and detailing, including a “distinct base, middle, and top”, vertical wooded ‘screens’, wooden beams, vertical siding and windows with mullions. The applicant also provided a view and sun/shadow study that show minimal impact to the neighbouring properties with respect to views, access to light and shadows (see Attachment 4).

The site contains the following limitations:

- a maximum of 84 m of building frontage can be located within 30 m of the CP Railway right-of-way;
- site access required to be offset from existing access across Bow Valley Trail;
- 12.8 m setback from existing property line.

Overall, the building utilizes natural materials and incorporates a high degree of detailing and articulation to provide a quality architectural design reflective of Canmore’s Rocky Mountain character. Although beyond the specific variance considerations, the site and building design does a good job of meeting the intent of the established criteria and minimizing the impacts on adjacent parcels. With these considerations in mind, Administration is supportive of the variances.

The applicant provided rationale for the proposed variances, which is included in Attachment 7.

2. Floor Area Above the Eaveline Step Back

Section 4.6.3.13 states that *floor area above the eaveline shall be incorporated into the roof structure and step back from the front building face for a minimum of 70% of the elevation.* The intent of this regulation is to minimize the impact of the massing and scale of the building and access to sunlight and views on pedestrians and bicyclists passing by along the pathway. The application proposes floor area above the eaveline with a step back of 30% of the building façade and therefore does not conform with this regulation.

The building is setback from the current road right-of-way by a little more than 12.8 m from the existing and future pathway/sidewalk. The pathway/sidewalk is not located near the building, minimizing the impact of the reduced building stepback. Also, portions of the building’s front façade are stepped back from grade to eaveline, adding to the high degree of articulation a detailing provided in the building design, and contributing to breaking up the overall perceived mass and scale of the building. The distance from the road right-of-way and high degree of building articulation and detailing contributes to mitigating the impact caused by the reduced front façade stepback of floor area above the eaveline and therefore, Planning supports this variance.

For variances to be approved, section 1.14.1.6 requires the development meet the Enhanced Green status:

1.14.1.6 Notwithstanding 1.14.1.1, in accordance with the Green Building Regulations of this Bylaw, neither a Development Officer nor the Canmore Planning Commission shall grant a variance to a commercial, industrial or institutional development, or a residential development to which the EnerGuide program cannot be applied, unless the development meets the Town's Enhanced Green Construction requirements.

The applicant has committed to achieve a minimum of 11% improvement better than the NECB - Current Version requirements for building energy design. This is consistent with the requirement for Enhanced Green Construction. To achieve this, the applicant's have included heat pumps within each unit, triple pane glazing with U-Value of 1.65, and will install conduit for 10 electric vehicle charging stations.

SUSTAINABILITY SCREENING REPORT (SSR)

The applicant's Sustainability Screening Report and score of 3.55, suggests that the development will have a positive impact on the community (see Attachment 5). The offsets include adding an employee housing unit, increase to density, proximity to services, waste diversion, and enhanced green construction.

OFF-SITE LEVIES

Offsite levies will be collected for this development at the current rate at the time of signing the Development Agreement.

LIGHTING

Details on exterior and site lighting have been provided with this application and conform to the requirements of the LUB.

SUBMITTED COMMENTS

A Notice of Application has been posted on the site pursuant to the Land Use Bylaw. No comments were received at the time of writing this report.

OPTIONS FOR CONSIDERATION

The CPC has three options:

1. Approve the application subject to the conditions or in addition to others than those contained in Schedule A.
2. Refuse the application, specifying reason(s) for refusal.
3. Postpone the application, pending submission of any additional details requested by CPC.

RECOMMENDATION

The Planning and Development Department recommends that the Canmore Planning Commission **APPROVE** PL20220303, subject to the conditions of approval set out in Schedule A attached to this report.

ATTACHMENTS:

1. Site Context
2. Zoning
3. Bylaw Conformance Review
4. Submitted Plans
5. Sustainability Screening Report (SSR)
6. Schedule A – Conditions of Approval
7. Variance Rationale



Riley Welden, RPP, MCIP
Development Planner



Lauren Miller, RPP, MCIP, AICP
Manager of Planning and Development



Figure 1: Overview of Site and Adjacent Uses (looking Northwest)



Figure 2: View of existing entrance from Bow Valley Trail

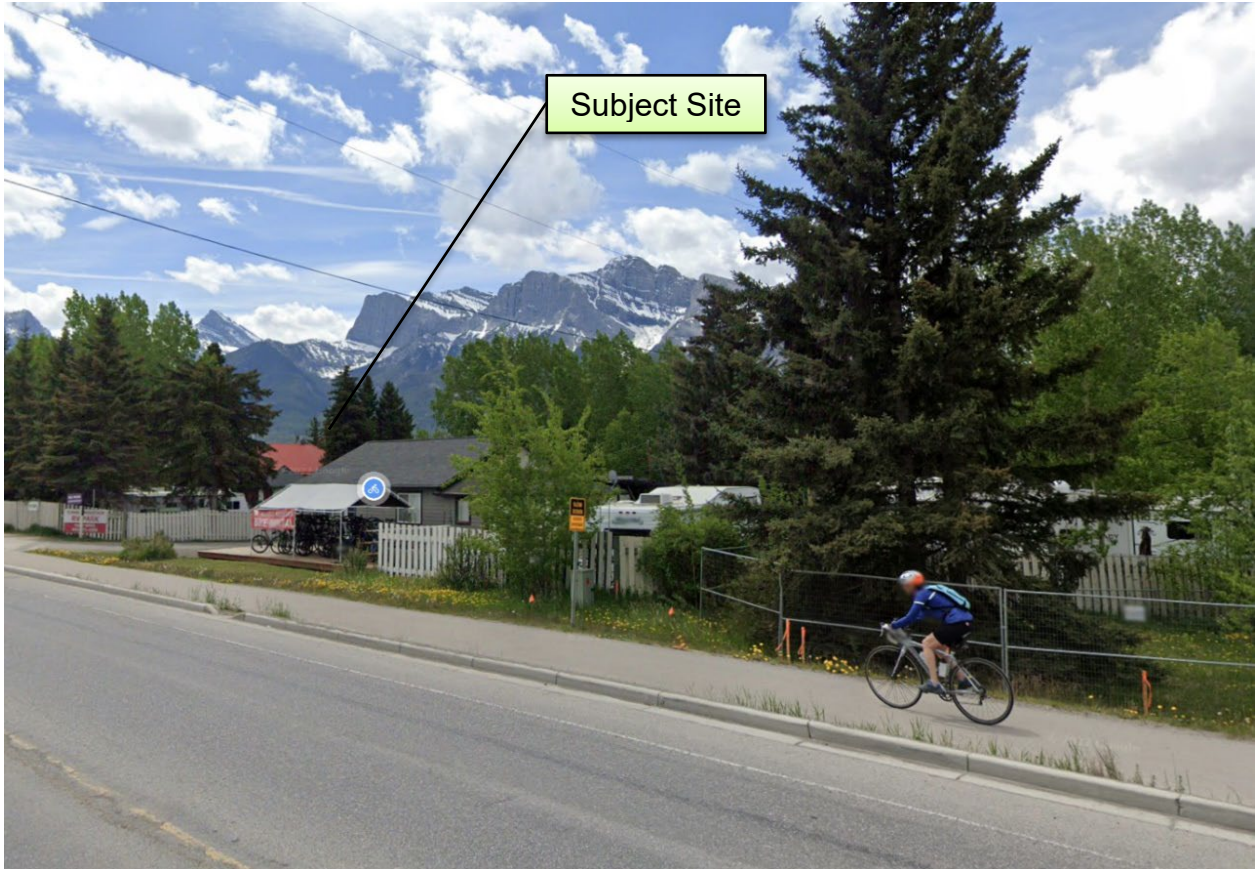
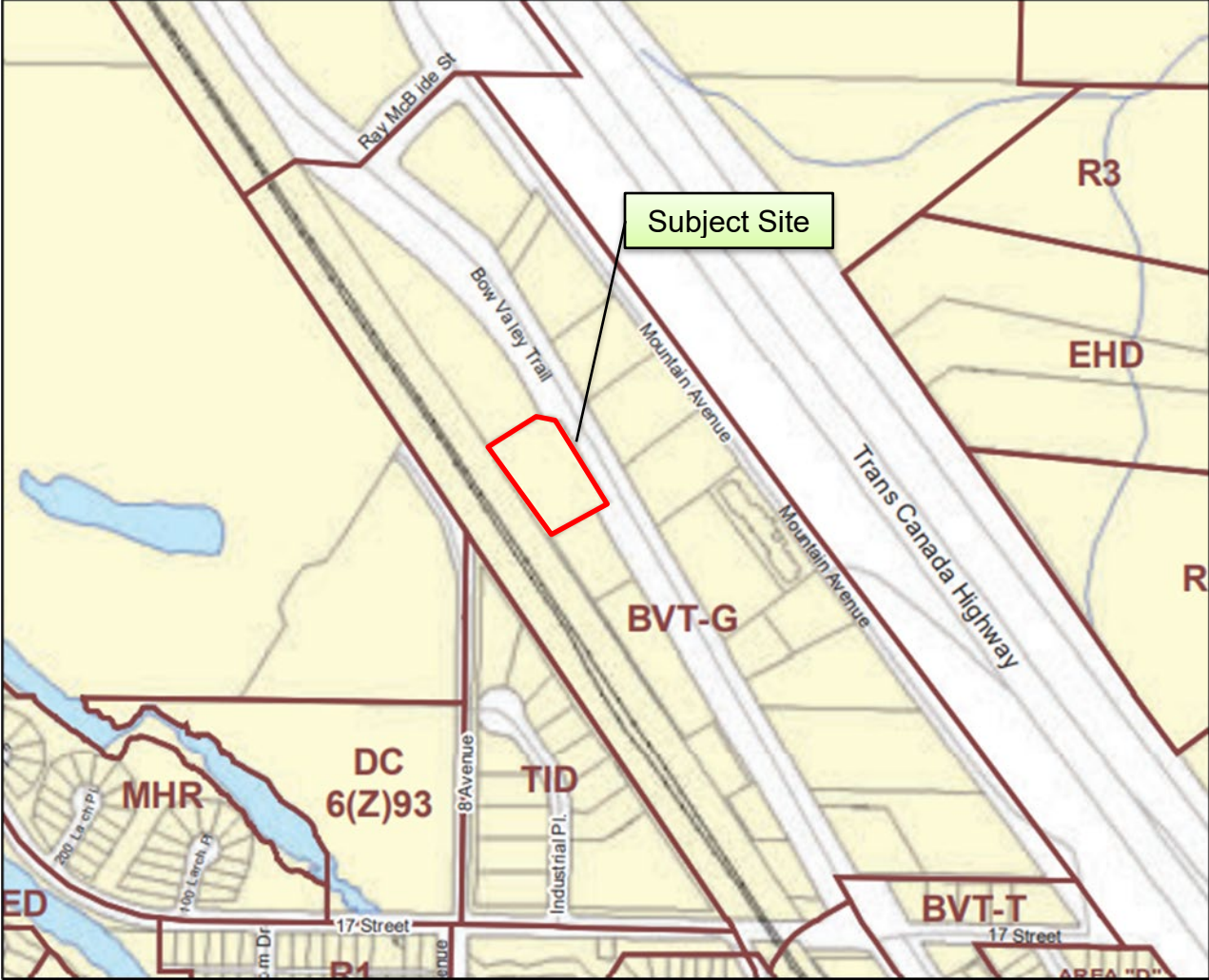


Figure 3: View looking southeast along Bow Valley Trail



Figure 4: View looking northwest along Bow Valley Trail



	BYLAW 2018-22	PROPOSED	VARIANCE
MAX FLOOR AREA RATIO (FAR)	MIN 0.75 MAX 1.5	1.5	NO
MAX BUILDING SITE COVERAGE	55%	48%	NO
FRONT YARD SETBACK (MEASURED FROM FUTURE ROAD WIDENING)	2.0 M WITH MIN 20% OF BUILDING AT 2.0M	2.0 M WITH 39% AT 2.0 M	NO
SIDE YARD SETBACK (SOUTHEAST)	3.0 M	> 3.0 M	No
SIDE YARD SETBACK (NORTHWEST)	3.0 M	3.0 M	No
REAR YARD SETBACK	6.0 M	6.0 M	No
MAX BUILDING HEIGHT	16 M	19.2 M	YES 3.2 M
MAX BUILDING EAVELINE HEIGHT	7.0 M	9.0 M AT FRONT FAÇADE 13.0 M OTHER FAÇADES	YES 2 M & 6 M
FLOOR AREA ABOVE EAVELINE SETBACK MIN OF 70% OF THE ELEVATION	REQUIRED	30% PROVIDED	YES 40%
LANDSCAPING			
AREA	25%	47%	No
TREES	134	139	No
SHRUBS	141	334	No
PARKING			
VISITOR ACCOMMODATION VEHICLE PARKING	98 UNITS = 98 VEHICLE PARKING STALLS	98 VEHICLE PARKING STALLS	No
VISITOR ACCOMMODATION BICYCLE PARKING	30 LONG TERM BICYCLE STALLS 15 SHORT TERM BICYCLE STALLS	30 LONG TERM BICYCLE STALLS 16 SHORT TERM BICYCLE STALLS	No No
COMMERCIAL RETAIL UNIT VEHICLE PARKING	2 STALLS/100 M2 = 2 VEHICLE PARKING STALLS	2 VEHICLE PARKING STALLS	No
COMMERCIAL RETAIL UNIT BICYCLE PARKING	1 LONG TERM BICYCLE STALLS 2 SHORT TERM BICYCLE STALLS	1 LONG TERM BICYCLE STALLS 2 SHORT TERM BICYCLE STALLS	No
EMPLOYEE HOUSING UNIT VEHICLE PARKING	NO REQUIREMENT	NONE	No
EMPLOYEE HOUSING UNIT BICYCLE PARKING	NO REQUIREMENT	1 LONG TERM BICYCLE STALL	No



Urban Design Architecture Interior Design

RE-ISSUED FOR DEVELOPMENT PERMIT

23/02/01



- DR0.00 COVER
- DR0.01 GENERAL INFORMATION
- DR0.02 SITE SURVEY
- DP1.01 SITE PLAN
- DP1.02 SITE DETAILS
- DP2.00 PARKADE PLAN
- DP2.01 MAIN FLOOR PLAN
- DP2.02 LEVEL 2 PLAN
- DP2.03 LEVEL 3 PLAN
- DP2.04 LEVEL 4 PLAN
- DP2.05 LEVEL 4 LOFT PLAN
- DP2.06 ROOF PLAN
- DP3.01 BUILDING ELEVATIONS
- DP3.02 BUILDING ELEVATIONS
- DP4.01 BUILDING SECTIONS
- DP5.01 PERSPECTIVE VIEWS
- DP6.01 SUN STUDY
- L0.00 LANDSCAPE STATS PLAN
- L0.01 PLANT LIST, PLANT LEGEND, LEGEND & GENERAL NOTES
- L1.00 LANDSCAPE PLAN
- L1.01 COURTYARD - LANDSCAPE PLAN
- L1.02 PUBLIC PLAZA - LANDSCAPE PLAN
- L2.00 LANDSCAPE DETAILS
- L2.01 LANDSCAPE DETAILS
- L2.02 LANDSCAPE DETAILS
- L2.03 LANDSCAPE DETAILS
- L2.04 LANDSCAPE INSPIRATION
- E000 ELECTRICAL COVER PAGE
- E001 SITE PLAN - ELECTRICAL
- E002 AGI SITE PLAN
- E003 AGI RENDERINGS & LUMINAIRE SCHEDULE
- C-100 SURFACEWORKS & GRADING
- C-200 UNDERGROUND UTILITY
- C-300 STORM DESIGN
- C-400 DETAILS - SHEET 1-5
- C-401 DETAILS - SHEET 2-5
- C-402 DETAILS - SHEET 3-5
- C-403 DETAILS - SHEET 4-5
- C-404 DETAILS - SHEET 5-5

RUNDLE PARK

1734 Bow Valley Trail

Architecture

MTA | Urban Design Architecture Interior Design
310-625 11th Ave SW
Calgary, AB T2R 0E1
t: 403.264.8700
www.mtalink.com

Structural

MMP Structural Engineering Ltd.
#205, 1170 Kensington Cr, NW
Calgary, AB, T2N 1X6
403-264-8495
mmpengineering.com/

Mechanical

Embe Consulting Engineers
Suite 204, 110 12th Avenue, S.W.
Calgary, AB, T2R 0G7
403-406-2277
www.embeconsulting.ca/

Electrical

Embe Consulting Engineers
Suite 204, 110 12th Avenue, S.W.
Calgary, AB, T2R 0G7
403-406-2277
www.embeconsulting.ca/

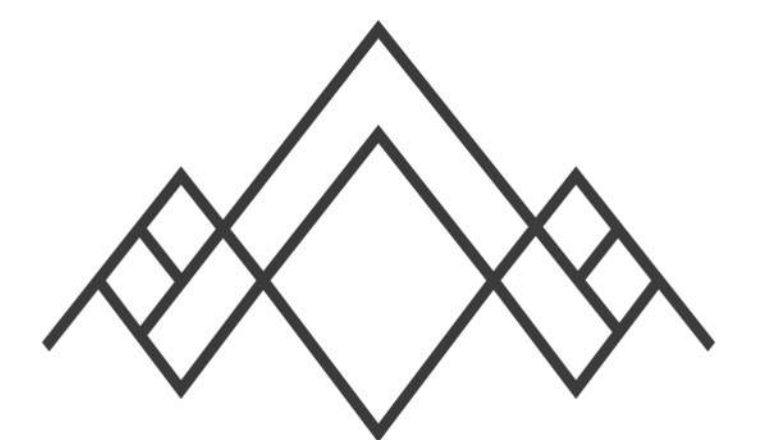
Civil

McElhanney
203-502 Bow Valley Trail
Canmore, AB, T1W 1N9
403-609-3992
www.mcelhanney.com/

Landscape

pLAnt Studio
1006, 26th Ave SW
Calgary, AB, T2S 0M3
403-454-8115

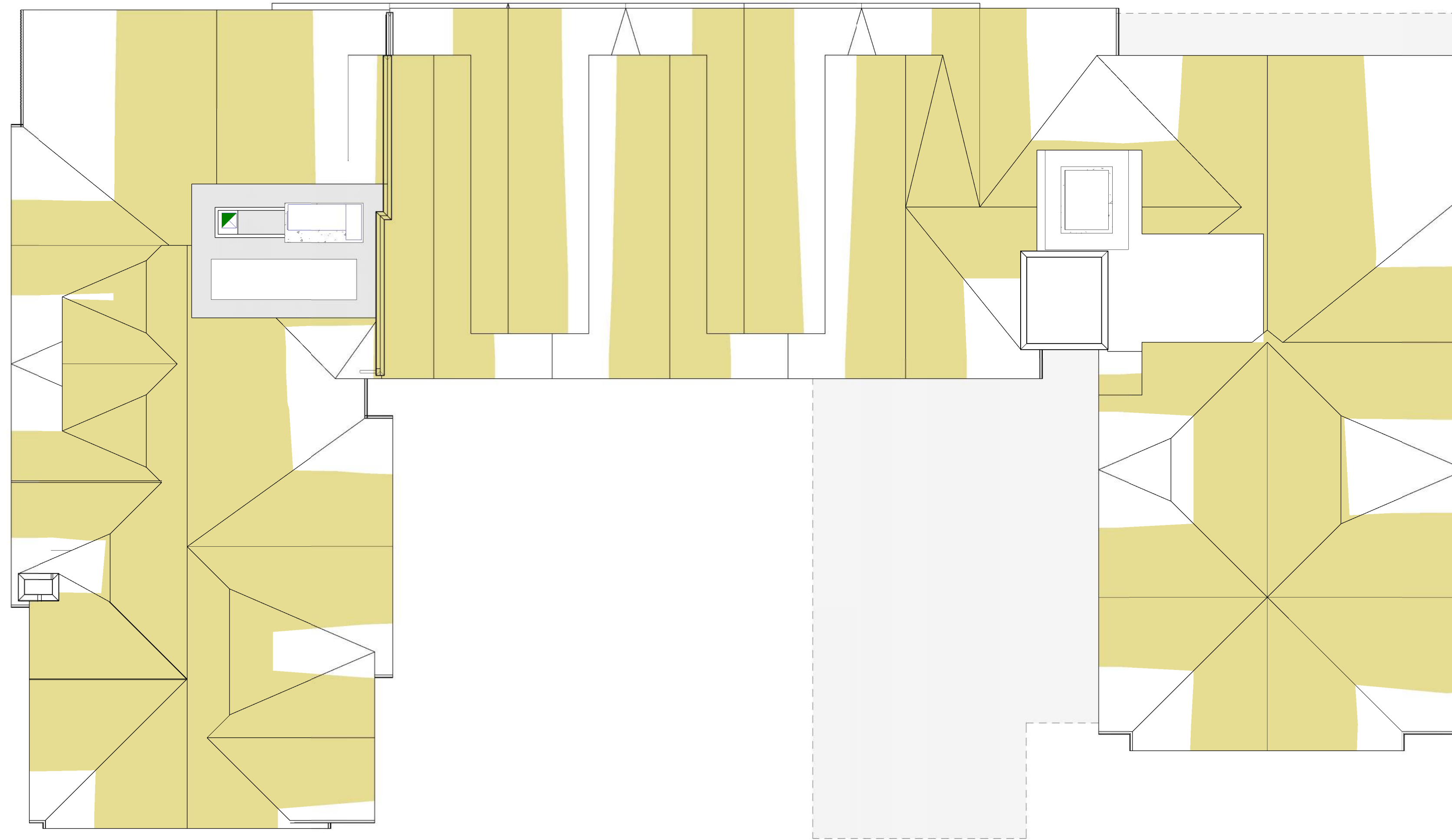
PREPARED FOR CIELLE PROPERTIES



RUNDLE PARK

PROJECT NO.

21.158



1
DP0.01
1 : 200
ROOF PLAN - VARIANCE DIAGRAM

ROOF AREA ABOVE 16.0m FROM GRADE
 ROOF AREA AT GRADE

ROOF AREA ABOVE 16.0m: 2066.0m²
 TOTAL ROOF AREA: 3571.0m²
 57.9% OF ROOF AREA IS ABOVE 16.0m FROM GRADE



2
DP0.01
1 : 200
SITE SECTION THROUGH BVT

PROJECT STATISTICS			
DEVELOPMENT DESCRIPTION			
Cielle Canmore is a 4.5 storey building with 98 visitor accommodation units and 1 CRU.			
MUNICIPAL ADDRESS			
1734 Bow Valley Trail, Canmore, AB			
LEGAL ADDRESS			
Block A, Plan 6122 JK			
ZONING DISTRICT			
BVT Bow Valley Trail General Commercial District (BVT-G)			
LAND USE REGULATIONS			
Front yard setback (facing BVT)	Max. 2m away from property line		
Side yard setback	Min. 3m		
Rear yard setback	Min. 6m		
EAVELINE HEIGHT			
Allowable height:	7.0m, may be raised to 9.0m if approved by Development Authority		
Proposed height:	9.0m		
1:100 YEAR GROUNDWATER FLOOD ELEVATION			
1311.6m			
BUILDING HEIGHT			
Allowable height:	16.0m, 20% relaxation permitted for architectural integrity		
Proposed height:	16.0m with portions of the building at 19.2m (20% height variance)		
SITE AREA			
6985m ²			
CONFIRMED POST-LAND TRANSFER SITE AREA:			
5759m ²			
SITE COVERAGE:			
Allowable site coverage:	55% maximum		
Proposed site coverage:	48%		
LANDSCAPE AREA:			
Required landscape area:	1440m ² (25% minimum)		
Proposed landscape area:	2700m ² (47%)		
UNIT DEPLOYMENT MATRIX			
UNIT TYPE	BEDROOMS	FLOOR NO.	TOTAL NO. OF UNITS
UNIT A	0	01	07
UNIT B	1	01	04
UNIT D1	2	01	06
UNIT D2	2	01	01
UNIT F	3	01	01
UNIT G	2	01	02
UNIT H	3	01	01
UNIT A	0	02	07
UNIT B	1	02	06
UNIT D1	2	02	04
UNIT D2	2	02	01
UNIT F	3	02	01
UNIT G	2	02	02
UNIT H	3	02	01
UNIT A	0	03	09
UNIT B	1	03	06
UNIT C2	3	03	01
UNIT C3	3	03	01
UNIT D1	2	03	06
UNIT D2	2	03	01
UNIT E	2	03	01
UNIT F	3	03	02
UNIT G	2	03	03
UNIT H	3	03	01
UNIT A	0	04	03
UNIT C1 (LOFT)	3	04	03
UNIT C2 (LOFT)	3	04	01
UNIT C3 (LOFT)	3	04	01
UNIT E (LOFT)	3	04	01
UNIT F (LOFT)	4	04	02
UNIT G (LOFT)	3	04	03
UNIT H	3	04	01
UNIT J	2	04	01
UNIT J (LOFT)	3	04	03
UNIT K (LOFT)	3	04	02
UNIT L1 (LOFT)	3	04	01
UNIT L2 (LOFT)	3	04	01
TOTAL NUMBER OF STUDIOS			26
TOTAL NUMBER OF 1 BEDROOMS			14
TOTAL NUMBER OF 2 BEDROOMS			30
TOTAL NUMBER OF 3 BEDROOMS			21
TOTAL NUMBER OF 4 BEDROOMS			07
TOTAL NUMBER OF UNITS			98
(PLUS ONE STAFF ACCOMMODATION UNIT)			
GROSS FLOOR AREA			
Main Floor:	BUILDING A 1112m ²	BUILDING B 883m ²	
Second Floor:	852m ²	883m ²	
Third Floor:	1484m ²	883m ²	
Fourth Floor:	1372m ²	841m ²	
Loft:	184m ²	124m ²	
Total:	5914m ²	3614m ²	
Parkade Area:	3468m ²		
FLOOR AREA RATIO			
1.50			
PARKING REQUIREMENTS			
Parking stall requirement for non-residential developments (2.7.2):			
Visitor accommodation - 1 stall per accommodation unit			
Retail - 2 stalls per 100m ² of GFA			
Number of Parking stalls required:	100		
Number of parking stalls provided:	100		
Bike parking stalls provided:			
Short Term:	Required: 17	Provided: 18	
Long Term:	Required: 31	Provided: 31	

RUNDLE PARK
1734 Bow Valley Trail



issued/ revision schedule

no.	description	date
1	ISSUED FOR DEVELOPMENT PERMIT	22/09/20
2	RE-ISSUED FOR DEVELOPMENT PERMIT	23/02/01
3	RE-ISSUED FOR DEVELOPMENT PERMIT	23/03/15

seal
 permit
 consultant

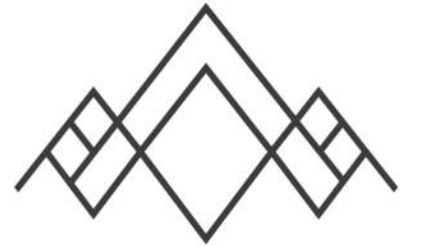


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 drawing title

GENERAL INFORMATION
 sheet
 DP0.01
 scale
 drawn JC 1 : 200
 checked CM
 project no. 21.158

RUNDLE PARK

1734 Bow Valley Trail



RUNDLE PARK

issued/ revision schedule

no.	description	date
1	ISSUED FOR DEVELOPMENT PERMIT	2209/30
2	RE-ISSUED FOR DEVELOPMENT PERMIT	2302/01
3	RE-ISSUED FOR DEVELOPMENT PERMIT	2303/15

seal
 permit
 consultant
 drawing title
 sheet
 scale
 project no.



1
DP5.01
PERSPECTIVE FROM ACROSS THE STREET



2
DP5.01
PERSPECTIVE FROM THE PLAZA



3
DP5.01
PERSPECTIVE FROM THE NORTH



4
DP5.01
PERSPECTIVE FROM THE CPR LAND

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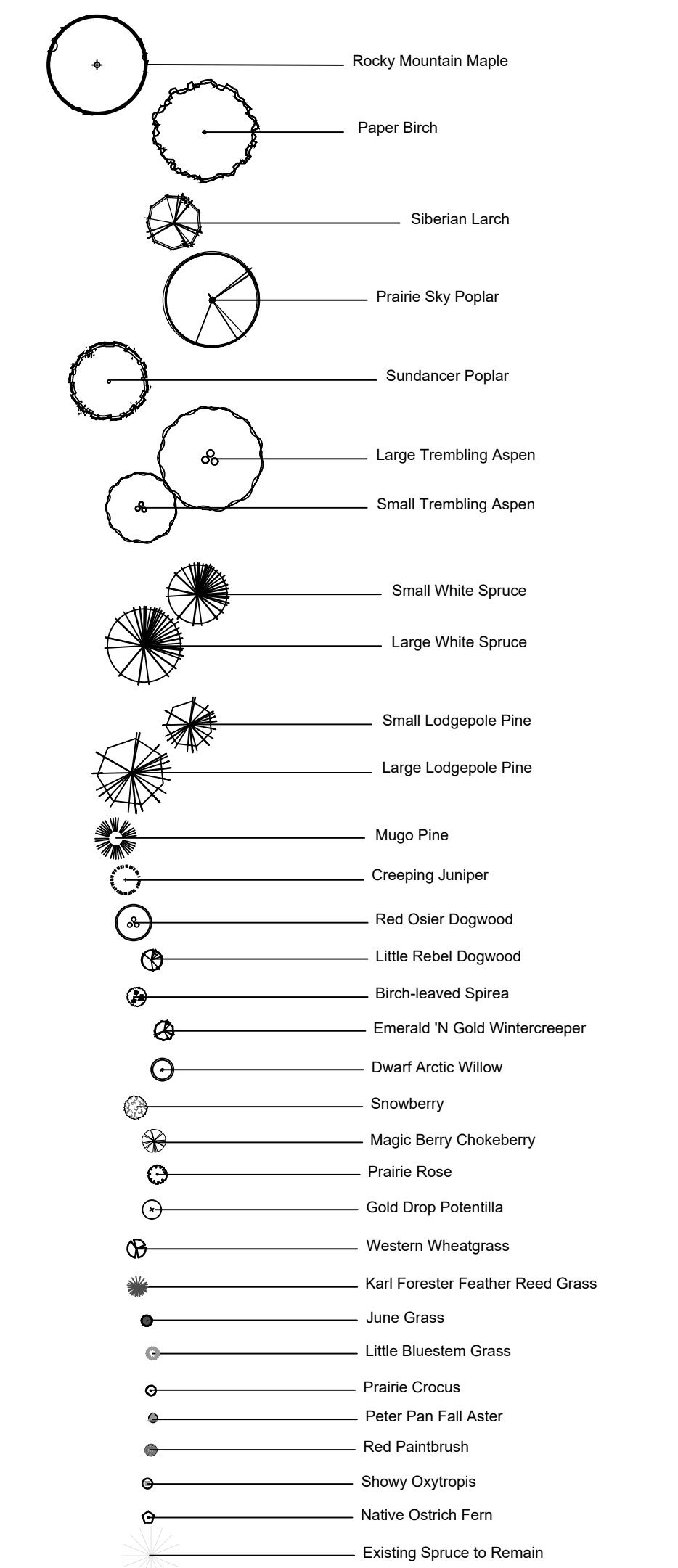
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PERSPECTIVE VIEWS

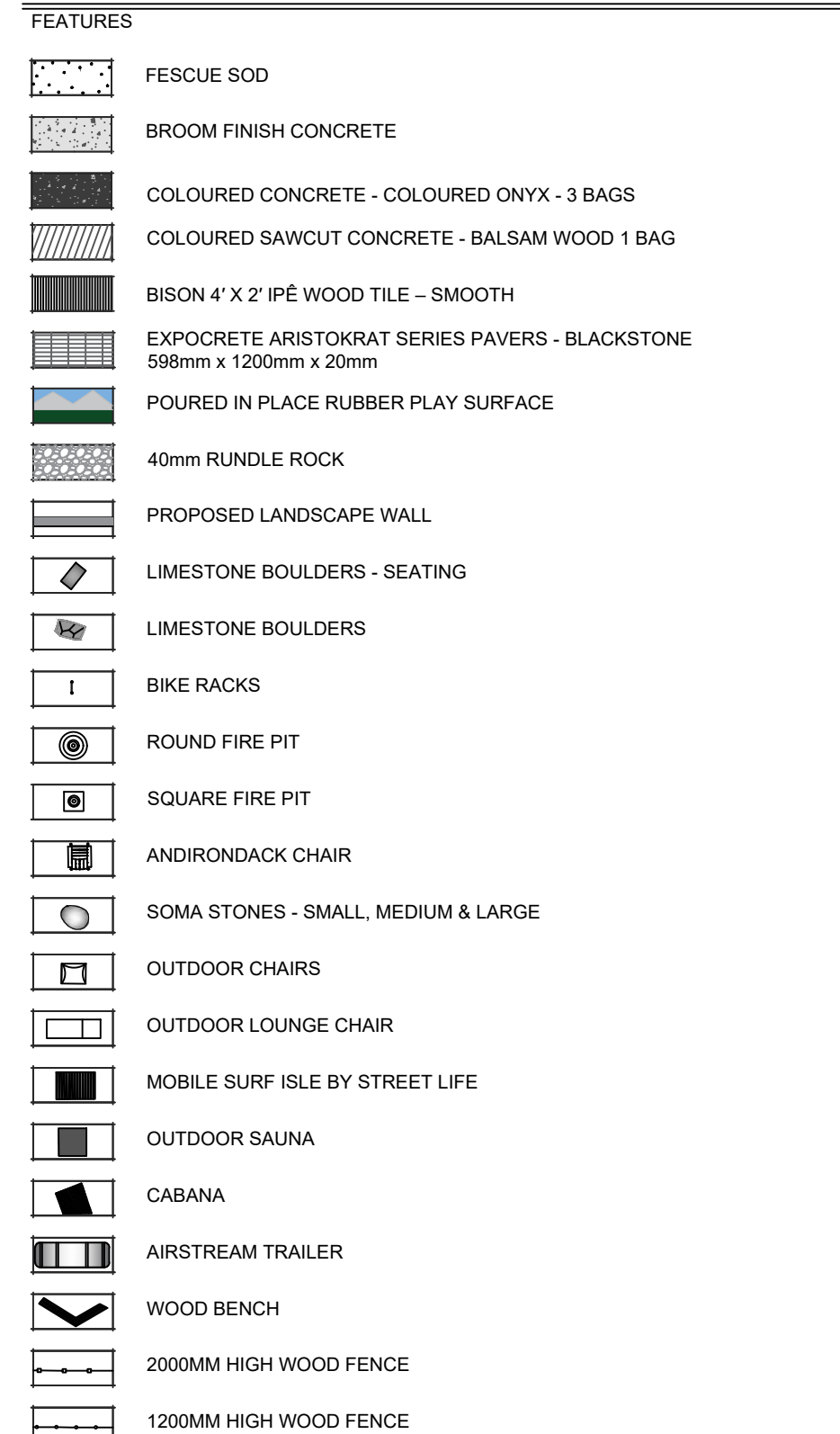
DP5.01

JS drawn
 CM checked
 scale
 project no. 21.158

PLANT LEGEND:



LEGEND:



PLANT LIST:

CONIFEROUS TREES

QTY	SCIENTIFIC NAME	COMMON NAME	PLANT SIZE
09	Picea glauca	White Spruce	4.0m Ht. B&B, 1220mm root ball dia.
02	Picea glauca	White Spruce	3.0m Ht. B&B, 800mm root ball dia.
02	Picea glauca	White Spruce	2.0m Ht. B&B, 700mm root ball dia.
07	Pinus contorta 'Latifolia'	Lodgepole Pine	4.0m Ht. B&B, 1220mm root ball dia.
12	Pinus contorta 'Latifolia'	Ponderosa Pine	3.0m Ht. B&B, 800mm root ball dia.
07	Pinus contorta 'Latifolia'	Ponderosa Pine	2.0m Ht. B&B, 700mm root ball dia.

DECIDUOUS TREES

QTY	SCIENTIFIC NAME	COMMON NAME	PLANT SIZE
05	Acer glabrum	Rocky Mountain Maple	50mm cal. (B&B), min 850mm wide x 550mm depth root ball
13	Betula papyrifera	Paper Birch	80 mm cal. (B&B), min 950mm wide x 650mm depth root ball
08	Larix sibirica	Siberian Larch	3.0 m height, (B&B), min 850mm wide x 550mm depth root ball
05	Populus x canadensis 'Prairie Sky'	Prairie Sky Poplar	80mm cal. (B&B), min 850mm wide x 550mm depth root ball
17	Populus 'ACWS151'	Sundancer Poplar	70mm cal. (B&B), min 850mm wide x 550mm depth root ball
38	Populus tremuloides	Trembling Aspen - small	50mm cal. (B&B), min 850mm wide x 550mm depth root ball
14	Populus tremuloides	Trembling Aspen - Large	80mm cal. (B&B), min 900mm wide x 750mm depth root ball

CONIFEROUS SHRUBS

QTY	SCIENTIFIC NAME	COMMON NAME	PLANT SIZE
30	Juniperus sabinia 'Calgary Carpet'	Calgary Carpet Juniper	Min. 600 spr, root ball dia. 350mm, #5 container size
11	Pinus mugo pumila	Dwarf Mugo Pine	Min. 600 spr, root ball dia. 350mm, #5 container size

DECIDUOUS SHRUBS

QTY	SCIENTIFIC NAME	COMMON NAME	PLANT SIZE
41	Aronia melanocarpa 'Autumn Magic'	Autumn Magic Chokeberry	Min. 600mm Ht., Min. 4 major basal stems, Root Spread 300mm. - #2 cont.
100	Cornus alba 'Jefreib'	Little Rebel Dogwood	Min. 600mm Ht., Min. 4 major basal stems, Root Spread 300mm. - #2 cont.
13	Cornus stolonifera	Red Osier Dogwood	Min. 600mm Ht., Min. 4 major basal stems, Root Spread 300mm. - #2 cont.
60	Euonymus fortunei 'Emerald 'N Gold'	Emerald 'N Gold Wintercreeper	Min. 600mm Ht., Min. 4 major basal stems, Root Spread 300mm. - #2 cont.
22	Potentilla fruticosa 'Gold Drop'	Gold Drop Potentilla	Min. 600mm Ht., Min. 4 major basal stems, Root Spread 300mm. - #2 cont.
12	Rosa arkansas	Prairie Rose	Min. 600mm Ht., Min. 4 major basal stems, Root Spread 300mm. - #2 cont.
34	Salix purpurea 'Nana'	Dwarf Arctic Willow	Min. 600mm Ht., Min. 4 major basal stems, Root Spread 300mm. - #2 cont.
04	Spiraea betulifolia	Birch-leaved Spiraea	Min. 600mm Ht., Min. 4 major basal stems, Root Spread 300mm. - #2 cont.
07	Symphoricarpos albus	Snowberry	Min. 600mm Ht., Min. 4 major basal stems, Root Spread 300mm. - #2 cont.

ORNAMENTAL GRASSES

QTY	SCIENTIFIC NAME	COMMON NAME	PLANT SIZE
280	Calamagrostis acutiflora 'Karl Foerster'	Karl Foerster Feather Reed Grass	Min. 200mm Ht., #2 cont.
104	Koeleria macrantha	June Grass	Min. 200mm Ht., #2 cont.
164	Paspopyrum smithii	Western Wheatgrass	Min. 200mm Ht., #2 cont.
164	Schizachyrium scoparium	Little Bluestem Grass	Min. 200mm Ht., #2 cont.

PERENNIALS

QTY	SCIENTIFIC NAME	COMMON NAME	PLANT SIZE
30	Anemone patens	Prairie Crocus	Min. 150mm Ht., 150mm pot.
124	Aster dumosus 'Peter Pan'	Peter Pan Aster	Min. 150mm Ht., 150mm pot.
169	Castilleja miniata	Red Paintbrush	Min. 150mm Ht., 150mm pot.
39	Matteuccia struthiopteris	Ostrich Fern	Min. 150mm Ht., #1 cont.
139	Oxytropis splendens	Showy Oxytropis	Min. 200mm Ht. 150mm pot.

NOTES:

1. ALL TREE AND SHRUB BEDS TO BE MULCHED WITH 75mm BARK MULCH.
2. TREES NOT PLANTED IN BEDS SHALL BE SURROUNDED BY A TREE WELL TO A DEPTH OF 100MM AFTER PLANTING AND WATERING. ALL TREES TO BE MULCHED STARTING 50MM FROM THE ROOT FLARE (TRUNK) AND EXTENDING THE LENGTH OF HOLE.
3. ALL SOFT LANDSCAPE AREAS TO BE IRRIGATED WITH AN UNDERGROUND AUTOMATIC IRRIGATION SYSTEM.

PLANT IMAGES:

TREES



White Spruce, Lodgepole Pine, Rocky Mountain Maple, Paper Birch, Siberian Larch, Prairie Sky Poplar, Sundance Poplar, Trembling Aspen

SHRUBS



Creeping Juniper, Dwarf Mugo Pine, Red Osier Dogwood, Birch-Leaved Birch, Autumn Magic Chokeberry, Dwarf Arctic Willow

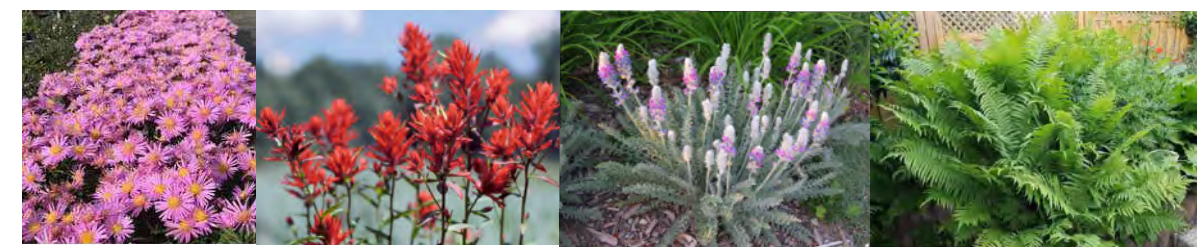


Snowberry, Little Rebel Dogwood, Prairie Rose, Gold Drop Potentilla, Emerald 'N Gold Wintercreeper

GRASSES/PERENNIALS



Blue Gramma Grass, Karl Foerster Reed Grass, June Grass, Little Bluestem Grass, Prairie Crocus



Peter Pan Aster, Red Paintbrush, Showy Oxytropis, Native Ostrich Fern

GENERAL NOTES:

1. ALL PLANT MATERIAL SHALL CONFORM TO THE STANDARDS OF THE CANADIAN NURSERY TRADES ASSOCIATION FOR NURSERY STOCK.
2. WARRANTY ON ALL LANDSCAPE TO BE TWO YEARS FROM DATE OF ACCEPTANCE.
3. CONTRACTOR TO CO-ORDINATE WORK WITH ALL OTHER CONTRACTORS ON SITE.
4. CONTRACTOR SHALL VERIFY ALL SITE CONDITIONS AT TIME OF CONSTRUCTION.
5. CONTRACTOR TO VERIFY ALL UNDERGROUND SERVICES PRIOR TO ANY WORK.
6. DURING CONSTRUCTION, ANY DAMAGE OCCURRING TO PUBLIC AND PRIVATE LANDS, RESULTING FROM ENCROACHMENT WILL BE MADE GOOD AT TIME OF CONSTRUCTION.
7. ALL EXISTING TREES WITHIN 6.0m OF CONSTRUCTION TO BE PROTECTED WITH A TREE PROTECTION FENCE ALONG THE DRIFLINE.
8. NEW TREE LOCATIONS TO BE STAKED BY THE CONTRACTOR AND APPROVED BY THE LANDSCAPE ARCHITECT PRIOR TO ANY INSTALLATION.
9. TREES SHALL BE WATERED IN FOR THE WINTER TO ENSURE THAT THE ROOTS ARE FROZEN. TREES SHALL BE REGULARLY CHECKED IN WINTER TO ENSURE THAT ROOTS ARE STILL FROZEN. IF THE WINTER WEATHER VARIES TO PLUS TEMPERATURES WATERING OF THE TREES SHALL BE DONE BY WATER TRUCK TO ENSURE HEALTHY PLANT MATERIAL.
10. CONTRACTOR TO ENSURE ADEQUATE SUPPORT FOR ALL PLANT MATERIAL NOTED ON DRAWINGS.
11. ANY SUBSTITUTIONS OF PLANT MATERIAL TO BE APPROVED BY THE LANDSCAPE ARCHITECT PRIOR TO THE SUBSTITUTIONS BEING MADE.
12. ANY DAMAGE TO EXISTING HARD AND SOFT SURFACING TO BE MADE GOOD BY THE CONTRACTOR.
13. ALL TREE AND SHRUB BEDS TO BE MULCHED WITH 75mm BARK MULCH.
14. TREES NOT PLANTED IN BEDS SHALL BE SURROUNDED BY A TREE WELL TO A DEPTH OF 100MM AFTER PLANTING AND WATERING. ALL TREES TO BE MULCHED STARTING 50MM FROM THE ROOT FLARE (TRUNK) AND EXTENDING THE LENGTH OF HOLE.
15. ANY DAMAGE OCCURRING TO THE EXISTING CONSTRUCTION, BUILDINGS, VEGETATION, LANDSCAPING, PATHWAYS AND/OR OTHER SITE AMENITIES ON THE SITE, ADJACENT PRIVATE PROPERTY OR PUBLIC LANDS SHALL BE REHABILITATED, RESTORED AND/OR MADE GOOD, BY THE CONTRACTOR TO THE OWNERS AND/OR THE CITY OR TOWN AUTHORITY'S REQUIREMENTS.
16. ALL SITE GRADES SHALL MATCH EXISTING GRADES OF ADJACENT LANDS, WITH GRADING CONFINED TO THE SITE. SEE CIVIL PLAN FOR GRADING, CURB AND SERVICING INFORMATION.
17. THE CONTRACTOR SHALL OBTAIN, AT THE CONTRACTOR'S SOLE EXPENSE, ALL UTILITY LOCATES PRIOR TO COMMENCING WORK. ANY AND ALL DAMAGES TO UTILITIES, REGARDLESS OF WHETHER LOCATES WERE COMPLETED, SHALL BE REHABILITATED, RESTORED AND/OR MADE GOOD BY THE CONTRACTOR, TO THE OWNERS AND/OR THE CITY OR TOWN AUTHORITY'S REQUIREMENTS.
18. ANY DISCREPANCIES BETWEEN SITE CONDITION AND THOSE ON THE DRAWING TO BE REPORTED IMMEDIATELY TO THE CONSULTANT AT TIME OF CONSTRUCTION.

PLANTING NOTES

1. ALL PLANT MATERIAL SHALL BE INSTALLED IN ACCORDANCE WITH THE CURRENT TOWN OF CANMORE STANDARDS AND GUIDELINES (CURRENT EDITION).
2. THE OWNER'S REPRESENTATIVE WILL INSPECT PLANTS AT THE FOLLOWING STAGES: AT TIME OF DELIVERY PRIOR TO INSTALLATION, INSTALLED PLANTS BEFORE COMMENCEMENT OF MAINTENANCE PERIOD AND AT THE END OF MAINTENANCE PERIOD. NOTIFY OWNERS REPRESENTATIVE 72 HOURS IN ADVANCE OF EACH REQUIRED INSPECTION. CONTRACTOR SHALL ENSURE REQUIRED PLANTS WILL BE AVAILABLE OR MEET SPECIFICATIONS BEFORE ARRANGING INSPECTION FOR SELECTION AND TAGGING OF PLANTS.
3. ALL PLANT MATERIAL TO BE DISEASE FREE AND TRUE TO FORM. SPECIMEN GRADE SINGLE STEM STOCK - UNLESS NOTED OTHERWISE, PLANTS WILL BE REJECTED IF: SOIL BALLS HAVE BEEN CRACKED OR BROKEN BEFORE OR DURING PLANTING OR WHEN BURLAP ROPES REQUIRED IN CONNECTION WITH THEIR TRANSPLANTING HAVE BEEN REMOVED. DO NOT USE PLANTS DAMAGED DURING CONTACT WITH EQUIPMENT, OR PLANTS THAT ARE WILTED, WIND BURNED OR STRESSED. CONTRACTOR TO REPLACE DAMAGED OR REJECTED PLANTS AT NO ADDITIONAL COST TO OWNER.
4. STAKE OUT LOCATIONS OF TREES, SHRUBS AND PLANTING BEDS. LOCATIONS OF ALL PLANTS SHALL BE REVIEWED AND APPROVED BY OWNERS REPRESENTATIVE ON SITE BEFORE PLANTING. PLANTS INSTALLED WITHOUT REVIEW AND APPROVAL MAY REQUIRE TRANSPLANTING OR RELOCATION AS DIRECTED BY OWNERS REPRESENTATIVE AT NO ADDITIONAL COST.
5. CONTRACTOR SHALL VERIFY THAT ALL MINIMUM CLEARANCES ARE PROVIDED BETWEEN PROPOSED TREES AND UNDERGROUND AND/OR OVERHEAD UTILITIES AS DEFINED BY THE LOCAL PARKS AND PLANNING DEPARTMENT.
6. PART OF PLANT MATERIAL INSTALLATION SHALL INCLUDE SUFFICIENT PROTECTION TO PREVENT BARK AND TREE DAMAGE DUE TO FORAGING ANIMALS AND ANCHORING SYSTEM TO PREVENT ROOT BREAKAGE DUE TO STRONG WINDS. LANDSCAPE ARCHITECT NOT RESPONSIBLE FOR ANY DAMAGES THAT INCUR DUE TO IMPROPER INSTALLATION.
7. ALL DEVELOPMENT AND CONSTRUCTION ACTIVITIES WITHIN 5 METERS OF EXISTING VEGETATION MUST BE SUPERVISED BY AN ISA (INTERNATIONAL SOCIETY OF ARBORICULTURE) CERTIFIED ARBORIST.
8. ALL PRUNING OF EXISTING VEGETATION MUST BE SUPERVISED BY AN ISA CERTIFIED ARBORIST.

LANDSCAPE REMEDIATION

1. ANY TURF AREAS DISTURBED DURING THE CONSTRUCTION PROCESS ARE TO BE AMENDED WITH A MINIMUM OF 150 MM DEPTH TOPSOIL AND SOD, AS PER THE TOWN OF CANMORE DESIGN STANDARDS.
2. ANY PLANTING BEDS DISTURBED DURING CONSTRUCTION ARE TO BE AMENDED WITH A MINIMUM OF 450 MM DEPTH TOPSOIL AND A MINIMUM OF 150 MM DEPTH WOOD CHIP MULCH, AS PER THE CITY OF CALGARY DESIGN STANDARDS.
3. ANY PLANT MATERIAL DAMAGED DURING CONSTRUCTION IS TO BE REMOVED, DISPOSED OF, AND REPLACED BY THE CONTRACTOR. ANY REPLACEMENT PLANT MATERIAL MUST BE THE SAME SPECIES OF THE REMOVED ITEM.

EXISTING LANDSCAPE

1. LOCATIONS AND SIZES OF EXISTING PLANT MATERIAL ARE APPROXIMATE.

GRADING NOTES:

1. REFER TO CIVIL ENGINEERING DRAWINGS FOR GRADING INFORMATION.

SOD:

1. CERTIFIED NO. 1 CULTIVATED TURF FESCUE SOD, WITH STRONG FIBROUS ROOT SYSTEM, THICK AND HEALTHY GROWTH AND DELIVERED 24 HOURS FROM THE TIME OF CUTTING. SOD SHOWING SIGNS OF DETERIORATION DUE TO AGE OR LACK OF MOISTURE WILL BE REJECTED. SOD MUST BE FREE OF STONES, BURNS, DRY OR BARE SPOTS, TEARS AND DELIVERED MOIST, CUT IN STRIPS OF UNIFORM WIDTH AND THICKNESS.

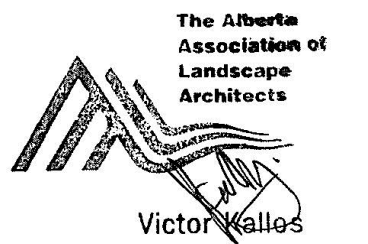
CIELLE

1734 Bow Valley Trail
Canmore, AB

issued/ revision schedule

no.	description	date
1	DP SUBMISSION	09/30/22

seal



permit

consultant

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landscape architecture + planning



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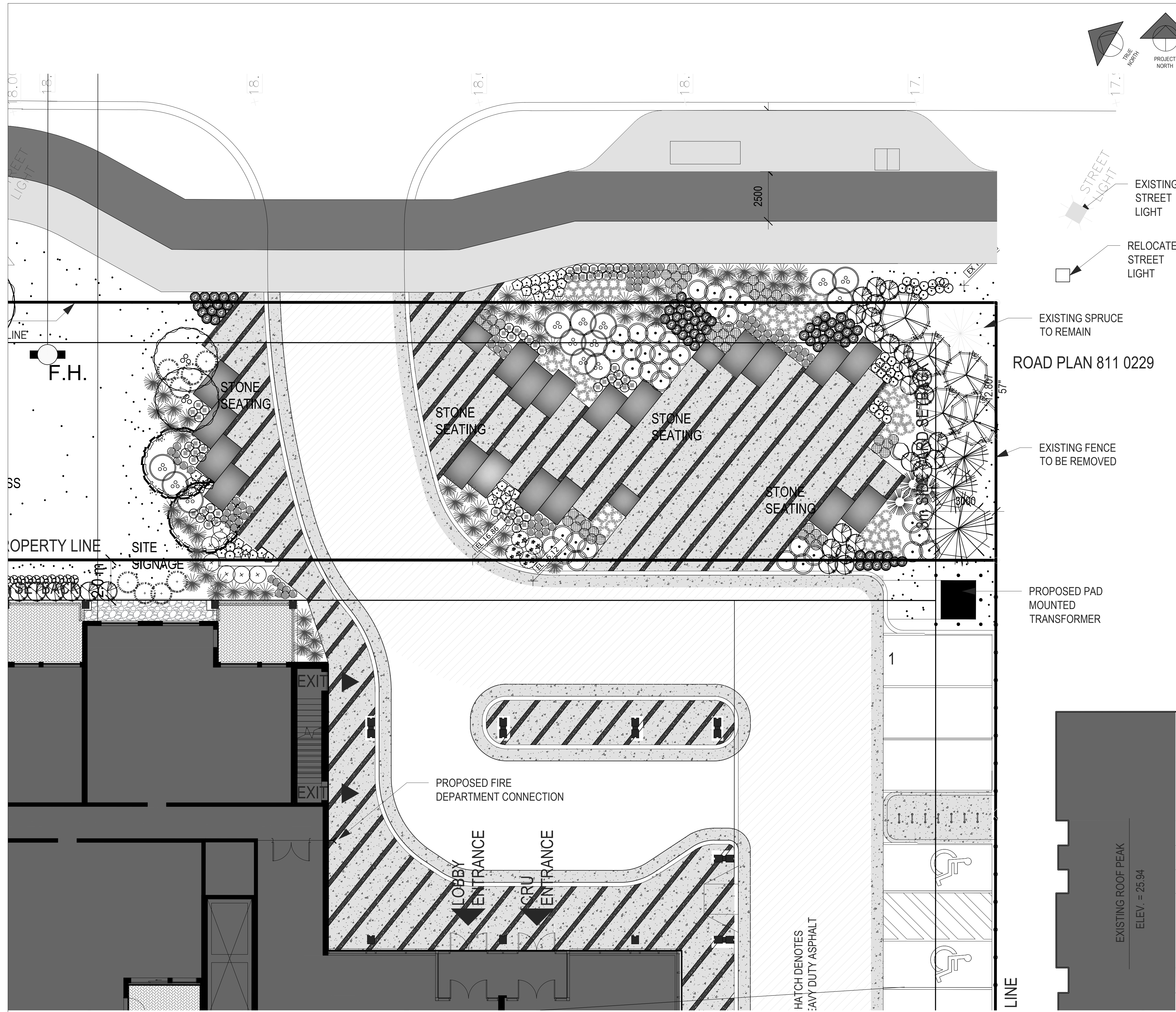
drawing title

PLANT LIST, PLANT LEGEND
LEGEND & GENERAL NOTES

sheet

L0.01

CK	drawn	NTS	scale
VK	checked	21.158	project no.



- PLANT LEGEND:**
- Rocky Mountain Maple
 - Paper Birch
 - Siberian Larch
 - Prairie Sky Poplar
 - Sundancer Poplar
 - Large Trembling Aspen
 - Small Trembling Aspen
 - Small White Spruce
 - Large White Spruce
 - Small Lodgepole Pine
 - Large Lodgepole Pine
 - Mugo Pine
 - Creeping Juniper
 - Red Osier Dogwood
 - Little Rebel Dogwood
 - Birch-leaved Spirea
 - Dwarf Arctic Willow
 - Emerald 'N Gold Wintercreeper
 - Snowberry
 - Magic Berry Chokeberry
 - Prairie Rose
 - Gold Drop Potentilla
 - Western Wheatgrass
 - Karl Forester Feather Reed Grass
 - June Grass
 - Little Bluestem Grass
 - Prairie Crocus
 - Peter Pan Fall Aster
 - Red Paintbrush
 - Showy Oxytropis

- SITE LEGEND:**
- FEATURES**
- FESCUE SOD
 - BROOM FINISH CONCRETE
 - COLOURED CONCRETE - COLOURED ONYX - 3 BAGS
 - COLOURED SAWCUT CONCRETE - BALSAM WOOD 1 BAG
 - BISON 4' x 2' IPÉ WOOD TILE - SMOOTH
 - EXPROCRETE ARISTOKRAT SERIES PAVERS - BLACKSTONE 598mm x 1200mm x 20mm
 - POURED IN PLACE RUBBER PLAY SURFACE
 - 40mm RUNDLE ROCK
 - PROPOSED LANDSCAPE WALL
 - LIMESTONE BOULDERS - SEATING
 - LIMESTONE BOULDERS
 - BIKE RACKS
 - ROUND FIRE PIT
 - SQUARE FIRE PIT
 - ANDIRONACK CHAIR
 - SOMA STONES - SMALL, MEDIUM & LARGE
 - OUTDOOR CHAIRS
 - OUTDOOR LOUNGE CHAIR
 - MOBILE SURF ISLE BY STREET LIFE
 - OUTDOOR SAUNA
 - CABANA
 - AIRSTREAM TRAILER
 - WOOD BENCH
 - 2000MM HIGH WOOD FENCE
 - 1200MM HIGH WOOD FENCE

01 PUBLIC PLAZA LANDSCAPE PLAN
L1.02 1:100

CIELLE
1734 Bow Valley Trail
Canmore, AB

issued/ revision schedule

no.	description	date
1	DP SUBMISSION	09/30/22



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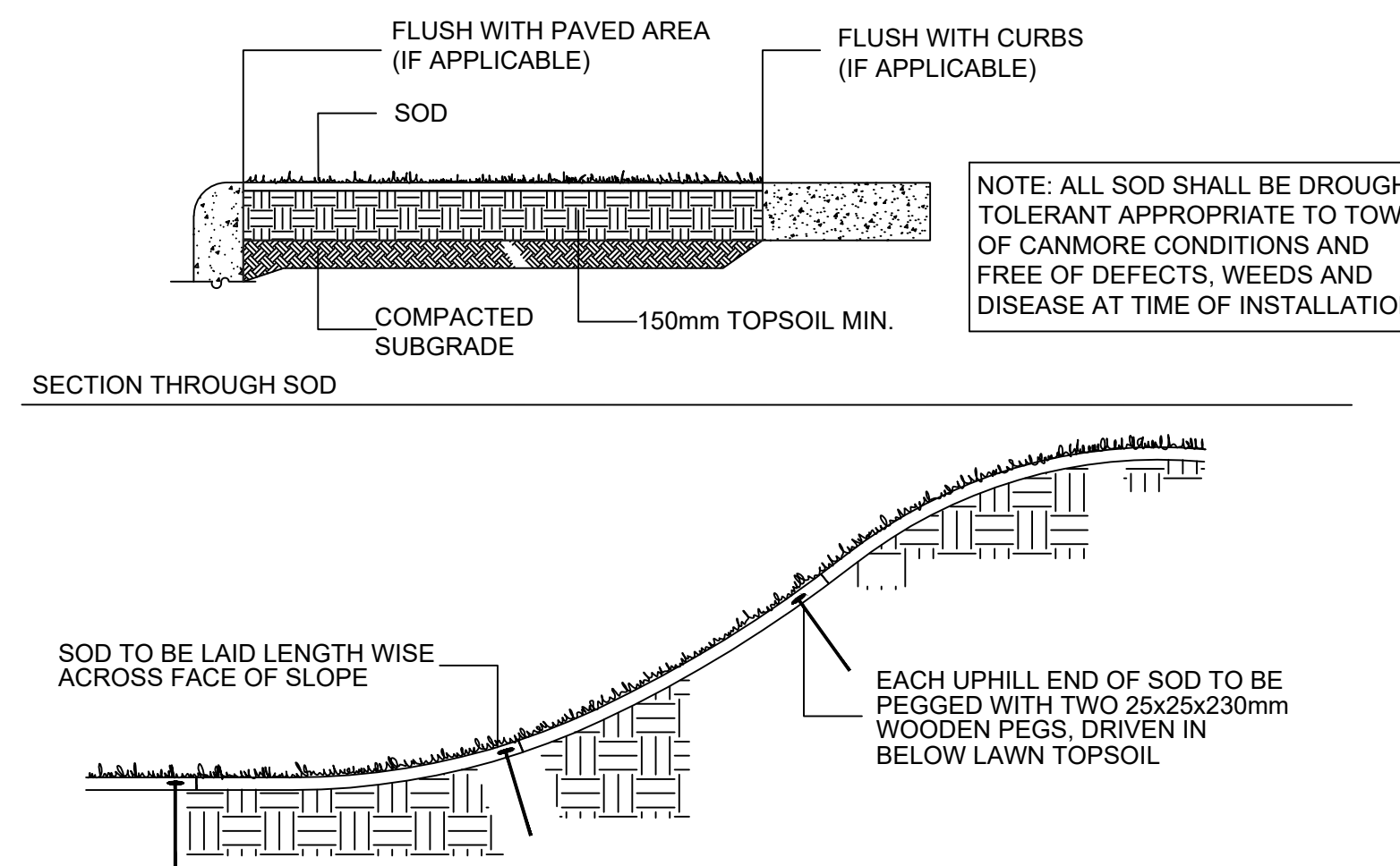
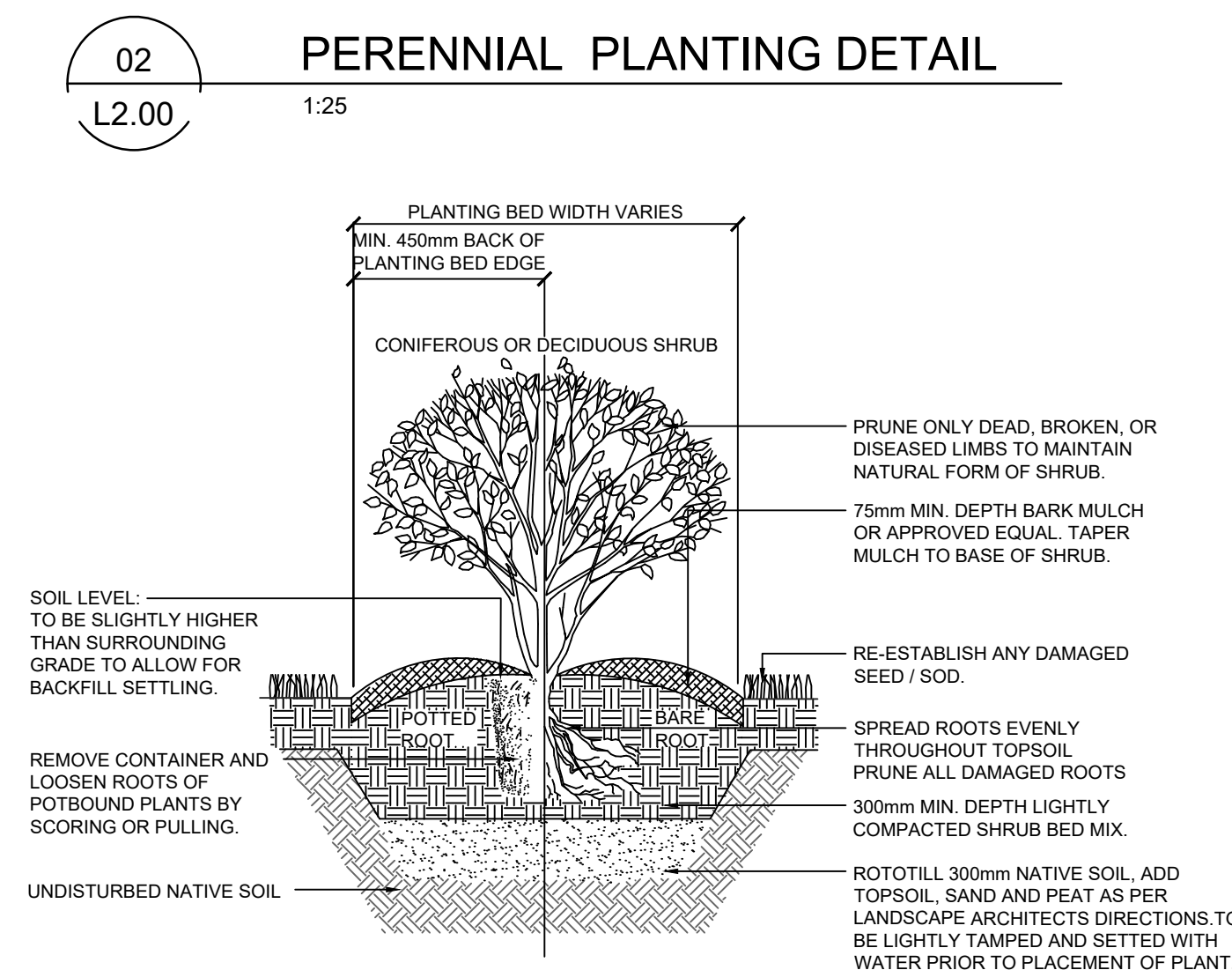
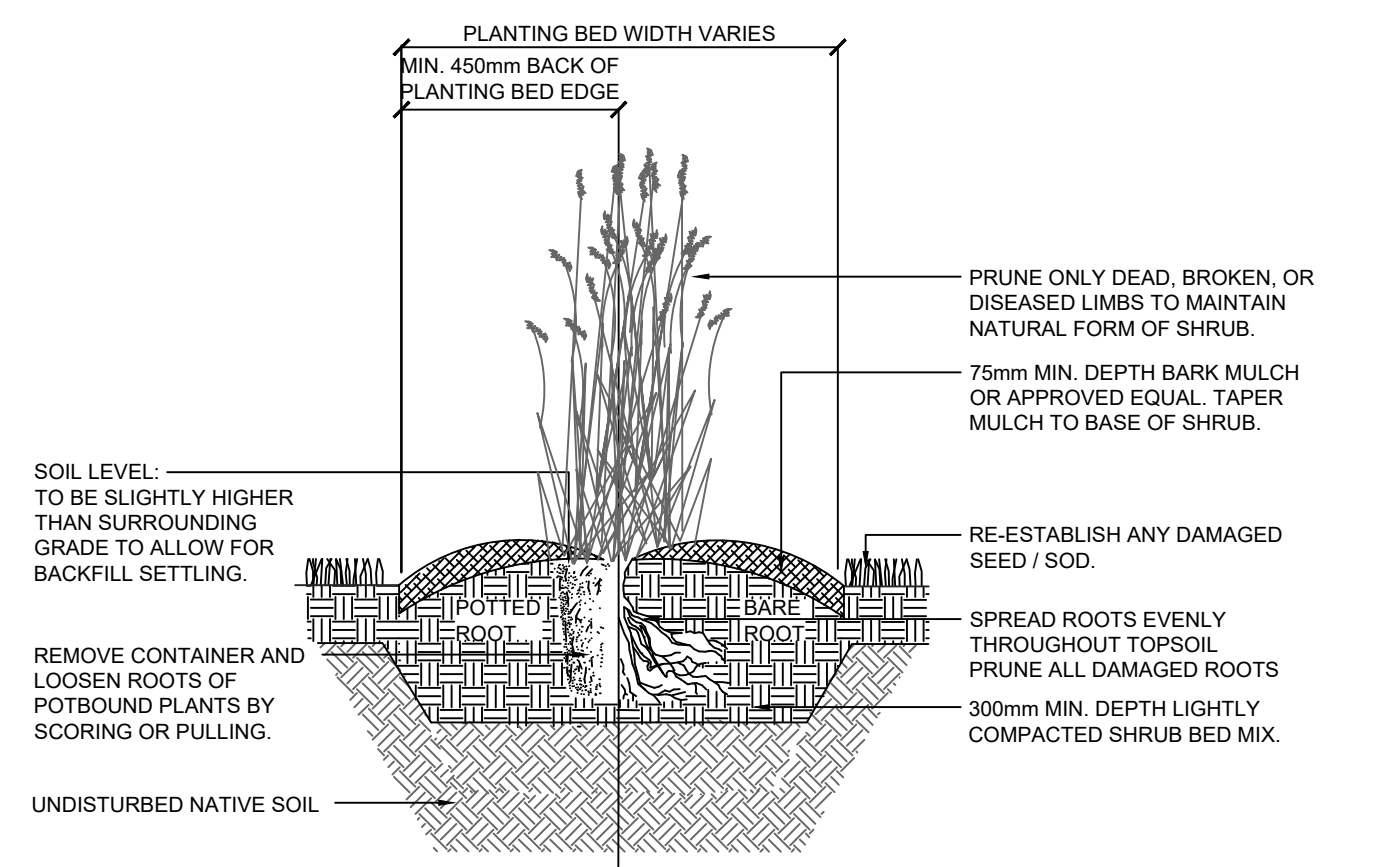
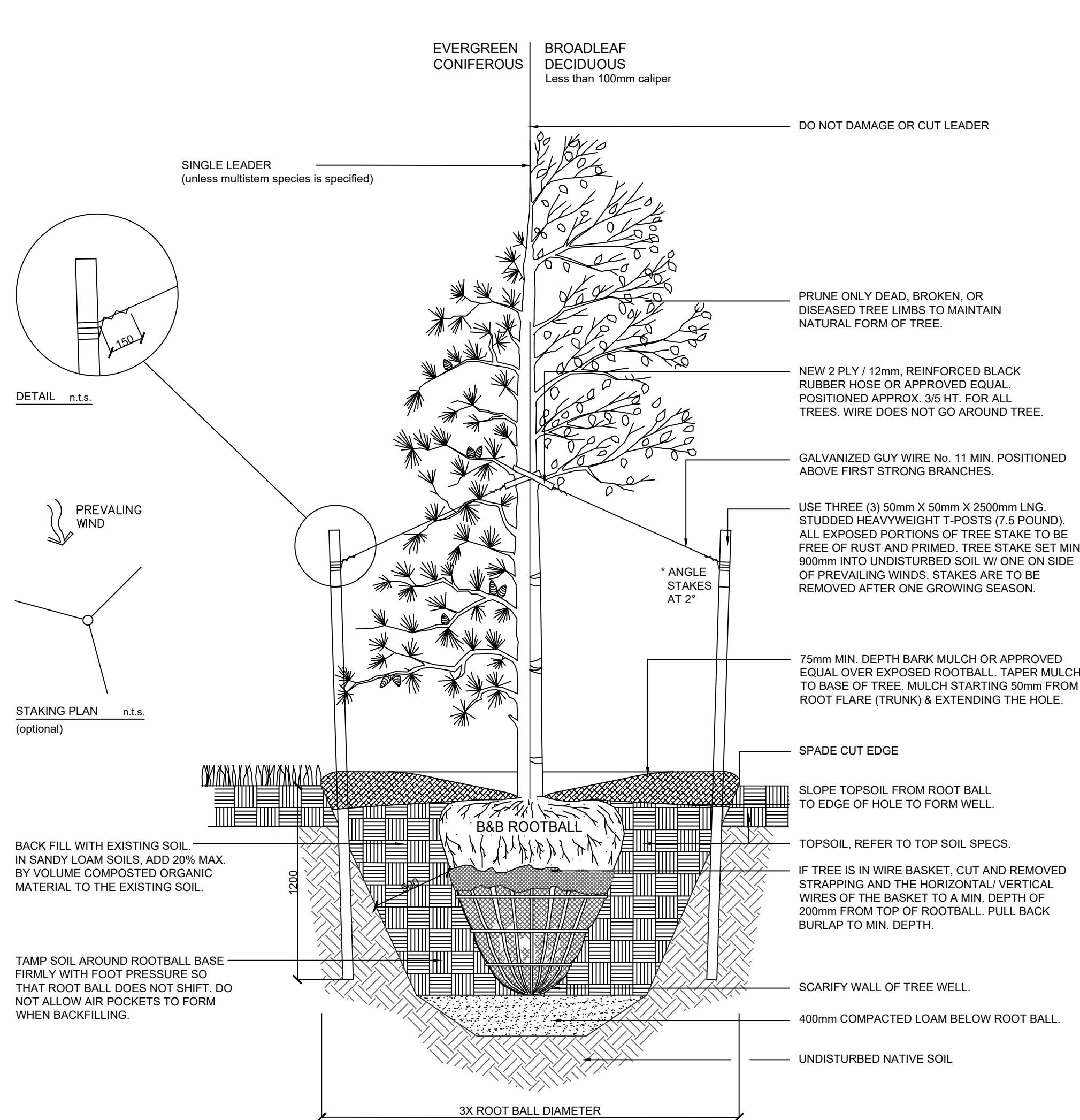


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PUBLIC PLAZA
LANDSCAPE PLAN

L1.02

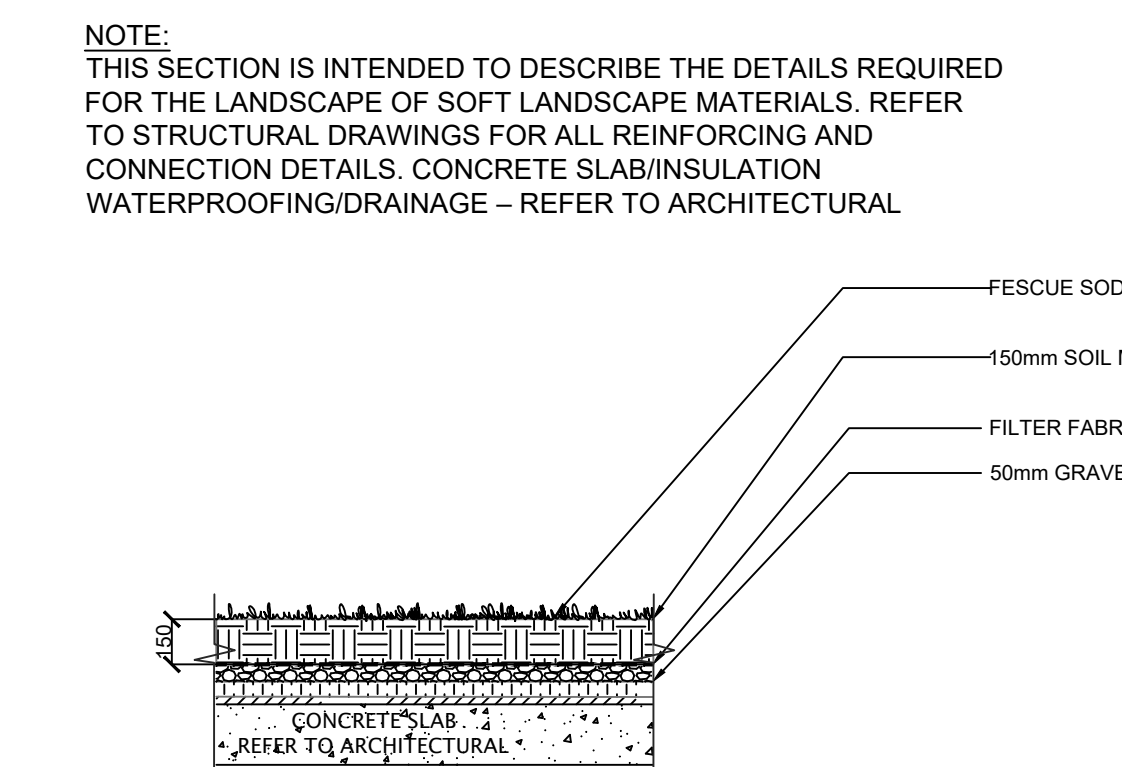
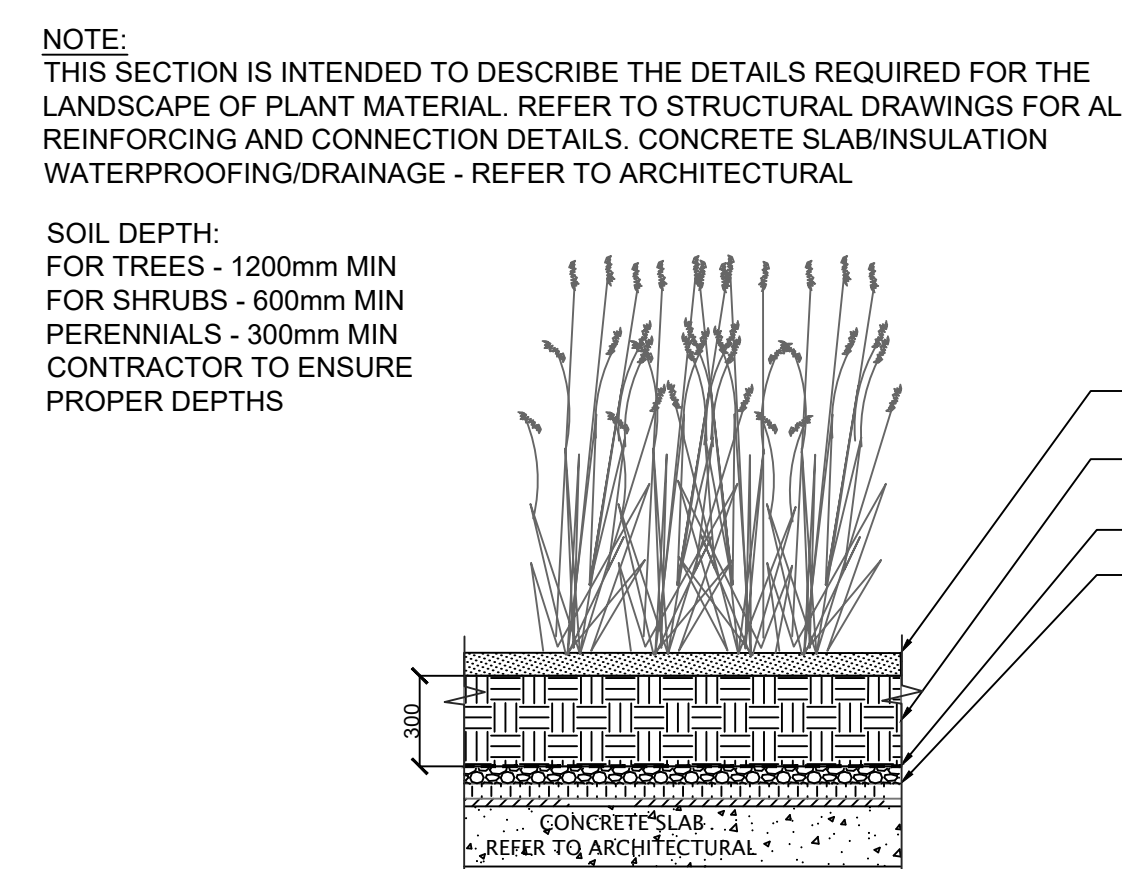
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VK	checked	21.158	project no.



01 TREE PLANTING DETAIL
1:25
L2.00

03 SHRUB PLANTING DETAIL
1:25
L2.00

04 SOD INSTALLATION DETAIL
1:25
L2.00



05 PERENNIAL PLANTING ON SLAB
1:25
L2.00

06 SOD INSTALLATION ON SLAB
1:25
L2.00

LANDSCAPE NOTES:

- ALL LANDSCAPE CONSTRUCTION MATERIALS AND TECHNIQUES SHALL MEET THE MINIMUM REQUIREMENTS OF THE CNLA STANDARDS
- NO SUBSTITUTIONS OF PLANT MATERIAL WILL BE PERMITTED BY THE LANDSCAPE CONSULTANT UNLESS APPROVED BY THE LANDSCAPE CONSULTANT IN WRITING PRIOR TO SHIPMENT TO SITE. ALL PLANT MATERIAL SHALL BE INSTALLED AS SHOWN ON THE DRAWINGS. ALL GROUND COVER SHALL BE PLANTED AT THE SPACING INDICATED. ANY PLANT MATERIAL INSTALLED THAT DOES NOT MEET WITH THE REQUIREMENTS OF THESE DRAWINGS AND ASSOCIATED SPECIFICATIONS SHALL BE REMOVED AND REPLACED WITH PLANT MATERIAL THAT MEETS THE AFOREMENTIONED REQUIREMENTS AT THE CONTRACTOR'S SOLE EXPENSE.
- ALL PLANT MATERIAL SHALL BE APPROVED AT THE SOURCE NURSERY PRIOR TO SHIPMENT TO SITE. THE CONTRACTORS BEARS THE SOLE RESPONSIBILITY OF NOTIFYING THE LANDSCAPE CONSULTANT A MINIMUM OF TWO WEEKS PRIOR TO SHIPMENT OF PLANT MATERIAL TO COORDINATE THE REVIEW OF THE PLANT MATERIAL. THE LANDSCAPE CONSULTANT RESERVES THE RIGHT TO WAIVE THIS REQUIREMENT BASED ON THE NURSERY SOURCE PROPOSED FOR USE BY THE CONTRACTOR.
- ALL PLANTING BEDS SHALL BE MULCHED WITH AN ALL WOOD MULCH TO A DEPTH OF 75MM
- ANY DAMAGE OCCURRING TO THE EXISTING CONSTRUCTION, BUILDINGS, VEGETATION, LANDSCAPING, PATHWAYS AND/OR OTHER SITE AMENITIES ON THE SITE, ADJACENT PRIVATE PROPERTY OR PUBLIC LANDS SHALL BE REHABILITATED, RESTORED AND/OR MADE GOOD, BY THE CONTRACTOR TO THE OWNERS AND/OR THE CITY OR TOWN AUTHORITY'S REQUIREMENTS.
- ALL SITE GRADES SHALL MATCH EXISTING GRADES OF ADJACENT LANDS, WITH GRADING CONFINED TO THE SITE.
- THE CONTRACTOR SHALL OBTAIN, AT THE CONTRACTOR'S SOLE EXPENSE, ALL UTILITY LOCATES PRIOR TO COMMENCING WORK. ANY AND ALL DAMAGES TO UTILITIES, REGARDLESS OF WHETHER LOCATES WERE COMPLETED, SHALL BE REHABILITATED, RESTORED AND/OR MADE GOOD BY THE CONTRACTOR, TO THE OWNERS AND/OR THE CITY OR TOWN AUTHORITY'S REQUIREMENTS.

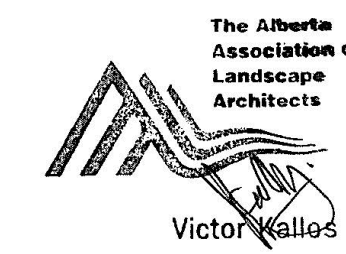
PLANT MATERIAL WARRANTY:

- THE CONTRACTOR SHALL WARRANT THAT THE PLANT MATERIAL WILL REMAIN FREE OF DEFECTS FOR A PERIOD OF ONE (1) YEAR FROM THE DATE OF SUBSTANTIAL PERFORMANCE. THE CONSULTANT ACCOMPANIED BY THE CONTRACTOR WILL UNDERTAKE AN END-OF-WARRANTY INSPECTION TO IDENTIFY PLANT MATERIAL THAT WILL BE REPLACED BY THE CONTRACTOR.
- THE CONSULTANT RESERVES THE RIGHT TO EXTEND THE CONTRACTOR'S WARRANTY AND RESPONSIBILITY FOR AN ADDITIONAL YEAR IF, AT THE END OF THE WARRANTY PERIOD, THE LEAF DEVELOPMENT AND GROWTH OF THE PLANT MATERIAL IS NOT SUFFICIENT TO ENSURE FUTURE SURVIVAL.
- THE CONTRACTOR SHALL EXTEND THE WARRANTY ON ALL REPLACEMENT PLANT MATERIAL FOR ONE(1) YEAR. REPLACEMENT PLANTS SHALL BE PLANTS OF SAME SIZE AND SPECIES SPECIFIED, PLANTED IN THE NEXT GROWING SEASON, WITH A NEW WARRANTY COMMENCING ON DATE OF REPLACEMENT
- THE CONTRACTOR SHALL CONTINUE SUCH REPLACEMENT AND WARRANTY OF PLANT MATERIAL TO THE SATISFACTION OF THE CONSULTANT.

IRRIGATION SYSTEMS:

- THE CONTRACTOR SHALL SUPPLY AND INSTALL A FULLY FUNCTIONAL AUTOMATIC PROGRAMMABLE IRRIGATION SYSTEM. IRRIGATION SYSTEMS WILL BE DESIGNED IN SUCH A WAY THAT ALL AREAS, INCLUDING LAWNS, BEDS, PLANTERS, GARDENS, AND GREEN ROOFS, WHETHER EXISTING OR NEW, ARE WATERED IN THE MOST EFFICIENT MANNER WHILE PROVIDING FULL COVERAGE. ONLY LANDSCAPE AREAS ARE TO RECEIVE WATER.
- SYSTEM CONFIGURATION WILL VARY DEPENDING ON PLANT VARIETY AND LANDSCAPE AREA. ANY OVERLAPPING OR UNNECESSARY SPRAY PATTERNS ARE TO BE KEPT TO A MINIMUM. SYSTEM INSPECTIONS AND NOZZLE ADJUSTMENTS ARE TO BE MADE AFTER THE INSTALL IS COMPLETE AND AT THE START OF EVERY GROWING SEASON. NOZZLES SHOULD BE REPLACED EVERY FIVE YEARS. WATERING TIMES ARE TO BE SET FOR THE EARLY MORNING TO REDUCE EVAPORATION.
- ONCE ESTABLISHED, ONE INCH OF WATER PER WEEK SHALL BE IRRIGATED ON ALL LAWN AREAS. TREES, SHRUBS, PERENNIALS, AND ORNAMENTAL GRASSES WILL BE WATERED ACCORDING TO THEIR NEEDS. DRIPS SYSTEMS ARE RECOMMENDED FOR TREES. CONTRACTOR WILL BE RESPONSIBLE FOR ANY DAMAGE TO PLANTS CAUSED BY OVER OR UNDER WATERING.
- IRRIGATION PLANS SHALL BE PREPARED BY A QUALIFIED IRRIGATION CONSULTANT AND INSTALLED BY A PROFESSIONALLY CERTIFIED IRRIGATION INSTALLER. CONTRACTOR SHALL PROVIDE MANUALS AND TOOLS, REQUIRED FOR SYSTEM OPERATION, TO THE OWNER UPON SYSTEM COMPLETION. AS-BUILT DRAWING SHALL BE PROVIDED BY THE CONTRACTOR TO THE OWNER.
- ALL UNDERGROUND IRRIGATION SYSTEMS ARE TO BE EQUIPPED WITH A HIGH FLOW SHUTOFF VALVE AND BACK FLOW PREVENTION VALVE. ALL DOWN SPOUTS ARE TO BE DIRECTED AWAY FROM FOUNDATIONS AND INTO SHRUB BEDS OR TREE WELLS WHERE PRACTICAL.
- CONTRACTOR TO REVIEW EXISTING IRRIGATION AND PROVIDE DESIGN TO INTEGRATE INTO EXISTING.

issued/ revision schedule		
no.	description	date
1	DP SUBMISSION	09/30/22



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LANDSCAPE DETAILS

L2.00

CK	drawn	1 : 25	scale
VK	checked	21.158	project no.



NOTES:
1. HOT TUB AREA INSPIRATION

01 HOT TUB INSPIRATION
L2.04 NTS



NOTES:
1. PLAYGROUND AREA INSPIRATION

02 PLAYGROUND INSPIRATION
L2.04 NTS

CIELLE

1734 Bow Valley Trail
Canmore, AB

issued/ revision schedule

no.	description	date
1	DP SUBMISSION	09/30/22

seal

The Alberta Association of Landscape Architects
Victor Gallos

permit

consultant

pLAnt studio
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drawing title
LANDSCAPE INSPIRATION

sheet
L2.04

drawn	scale
CK	NTS
checked	project no.
VK	21.158

Summary Page

Overall Results	Impact	%	Offset	%
Economic Sustainability	-0.11	0.54	10.40	44.39
Environmental Stewardship	-10.88	54.77	9.20	39.28
Social Fabric	-8.88	44.69	3.82	16.33
Total Impact	-19.87		Total Offset 23.42	Net Score 3.55

Economic Sustainability	
Income and Wages	-0.11
Non-Residential Tax Assessment	0.00

Economic Sustainability	
0.00	InfraCycle Assessment
5.02	Increasing commercial assessment
0.00	New employment above median salary
0.00	New employment outside of 4 significant sectors
0.00	Floor space for Economic Development & Tourism
5.37	Percentage of local construction labour value
0.00	Economic leadership or innovation

Environmental Stewardship	
Residential Water Consumption	0.00
Commercial Water Consumption	-2.44
Residential Solid Waste Generation	0.00
Commercial Solid Waste Generation	-4.87
All Building Energy use and GHG emissions	-2.79
Transportation	0.00
Infrastructure (sanitary-gravity)	0.00
Infrastructure (sanitary-pressure)	0.00
Environmentally Sensitive Lands	0.00
Land Consumption	-0.11
Efficient Residential Land Use	0.00
Efficient Commercial Land Use	0.00
Efficient Industrial Land Use	0.00
Efficient Mixed Use Residential Land Use	0.00
Efficient Mixed Use Commercial Land Use	0.00
Metres of trails / capita	-0.57
Metres of new roads to service development	-0.11

Environmental Stewardship	
0.00	Residential / commercial mix of uses
2.54	Higher density than current levels
0.00	Access to community services from residences
2.00	Access to services from the commercial site
1.00	Water saving measures
0.00	Rain water harvesting system or infiltration
0.00	Construction waste diversion rate
1.00	Long-term, operating waste diversion
0.66	Parking stalls are un-assigned
1.00	Bike parking of adequate quality
0.00	Average size of the dwellings
0.00	LEED Certified
0.00	Built Green Certified
0.00	Other green building certification programs
0.00	Commercial energy consumption reduction
0.00	Residential energy consumption reduction
1.00	Environmentally sensitive land protection
0.00	Minimize density adjacent to sensitive lands
0.00	Reuse an existing contaminated site
0.00	Environmental leadership or innovation

Social Fabric	
Affordability of Market housing (in relation to median income)	0.00
PAH Housing	-1.74
Seniors Housing	-1.40
Employee Housing	-1.05
Childcare spaces	-1.39
Library	-0.55
Food Bank Usage	-1.11
Social Assistance Payments	-1.09
Crimes Against Persons and Property	-0.55

Social Fabric	
0.00	Units of perpetually affordable housing
0.00	Cash contribution towards PAH
2.32	Bedrooms of employee housing
0.00	Bedrooms for employees earning < median income
0.00	Cash contribution towards employee housing
0.00	Units of seniors housing
0.00	Percentage of the employees housed
0.00	Employees rental assistance 10% below market levels
1.50	Percentage of site ares for social interaction
0.00	Reuse an existing historic property or building
0.00	Exceed minimum municipal reserve requirements
0.00	Accessible recreation or cultural facilities or programs
0.00	Contribution to recreation facilities
0.00	Support school enrollment
0.00	Support for current childcare facilities
0.00	Support for cultural establishments
0.00	Support for other non-profit community organizations
0.00	Unique supports for community programming
0.00	Support for special events
0.00	Public art component
0.00	Public consultation program
0.00	Social leadership or innovation

SCHEDULE A

CONDITIONS OF APPROVAL

DEVELOPMENT PERMIT No.: PL20220303

LAND USE DISTRICT: BVT-G District

APPROVED USE(S): VISITOR ACCOMMODATION
RETAIL SALES
EMPLOYEE HOUSING

APPROVED VARIANCE(S): 1. MAXIMUM BUILDING HEIGHT
2. MAXIMUM PERCENTAGE OF BUILDING OVER BUILDING HEIGHT
3. MAXIMUM EAVELINE HEIGHT
4. REDUCE MINIMUM PERCENT OF BUILDING ELEVATION STEPBACK ABOVE EAVELINE

LEGAL ADDRESS: BLOCK A; PLAN 6122JK

APPROVED VARIANCES TO LAND USE BYLAW 2018-22

1. Section 4.6.3.11 - Maximum Building Height: Increase maximum building height from 16.0 m to 19.2 m.
2. Section 4.6.9.1 a – 20% of the building may exceed maximum building height: Increase 20% of building exceeding building height to 57.9% of building exceeding building height.
3. Section 4.6.3.12 - Maximum Eaveline Height: Increase maximum eaveline height from 7.0 m to 9.0 m and 13.0 m.
4. Section 4.6.3.13 – Floor area above eaveline shall be incorporated into the roof structure and step back from the front building face for a minimum of 70% of the elevation. Decrease minimum step back from building face from 70% of elevation to 30% of elevation.

STANDARD CONDITIONS:

1. Prior to the release of the Development Permit, the applicant shall enter into a Development Agreement with the Town of Canmore to do the following:
 - a. construct or pay for the construction of the municipal improvements, infrastructure and services required by the development, which may include but shall not be limited to:
 - Transportation;
 - Water;
 - Sanitary;
 - Storm; and
 - Fire
 - b. pay the off-site levies imposed by the Off-Site Levy Bylaw; and
 - c. provide security in accordance with the Engineering Design and Construction Guidelines (EDCG) to ensure the terms of the Development Agreement are carried out.
2. All construction associated with the approval of this Development Permit shall comply with the regulations of the Land Use Bylaw (LUB) 2018-22, unless otherwise stated under the approved variances section of this document.
3. All construction associated with the approval of this Development Permit shall comply with the Town of Canmore Engineering requirements as outlined in the Engineering Design and Construction Guidelines (EDCG).
4. All construction associated with the approval of this Development Permit shall comply with the Tree Protection Bylaw and ensure all tree protection measure are appropriately put in place prior to the development of the site, where determined necessary by the Town of

Canmore Parks Department.

5. All construction, landscaping and exterior finishing materials are to be as shown on the approved plans and other supporting material submitted with the application.
6. Any trees, shrubs or other plant material installed as part of the landscaping plan which may die or are blown over, shall be replaced on an ongoing basis, prior to receipt by the developer of a Development Completion Certificate.
7. Any roof top mechanical apparatus, including chimneys and vents, shall be screened to the satisfaction of the Development Officer.
8. Access to the site for emergency vehicles shall be to the satisfaction of the Manager of Emergency Services.
9. All signs shall require a separate development permit.
10. **No occupancy** shall be permitted until an Occupancy Certificate has been issued by the Town of Canmore.

SPECIFIC CONDITIONS:

1. The applicant shall provide security to the Town of Canmore to ensure the completion of the project, in the form of cash or an irrevocable Letter of Credit. The amount should be equal to or no less than 1.25 (125%) of the estimated project costs for the project for landscaping and all hard surfacing, paving; and, site servicing; both to the satisfaction of the Town. The Letter of Credit shall be supplied at the time of the signing of the Development Agreement, and shall be in a format acceptable to the Town of Canmore.
2. The Developer shall pay off site levies according to the approved bylaw adopted by Council at the time of the signing of the Development Agreement. The Development Agreement shall specify the manner of the payment of these monies and all other relevant fees and contributions as determined by approved Town of Canmore policy(ies).
3. The Developer shall submit and follow their approved Construction Management Plan. The construction management plan submitted shall be followed through all stages of construction. If any problems arise where the Town Bylaws are being violated, a Stop Work Order will be delivered without warning and all construction shall cease until all problems have been rectified to the satisfaction of the Town of Canmore.
4. The Developer is required to provide **100** vehicle parking stalls, **32** long term bicycle stalls and **18** short term bicycle stalls as shown more or less in the approved plans to the satisfaction of the Development Officer. All on-site parking stalls shall be graded and paved to dispose of drainage to the satisfaction of the Development Officer.
5. The Developer shall ensure deliveries to the retail sales unit occur outside of business operating hours and that one surface vehicle parking stall is available for delivery vehicles.
6. The Developer shall provide landscaping generally in accordance with the approved landscaping plan.
7. The Developer agrees to comply with the requirements for enhanced green construction, and that the development will be a minimum 11% better than the current NECB in place at the time of development as outlined in Section 11 Green Building Regulations of the Land Use Bylaw.
8. Commitments expressed in the Developer's Sustainability Screening Report become conditions of approval upon the signing of this Schedule A and will be included in the development agreement.
9. No plant material is permitted between 0.0m and 1.5m from the building.
10. The development is approved for Visitor Accommodation use, which is defined in the Town of Canmore Land Use Bylaw 2018-22 as "a building or group of buildings not intended for residential use where sleeping facilities are provided for persons for periods of up to 30 days and which may also contain a variety of services and amenities for the benefit of guest". A

visitor accommodation development shall:

- a. Provide a central management and reservation service;
 - b. Have a single address for mail (not for individual units);
 - c. Not serve as a residential address for utility billing or electoral enumeration purposes; and
 - d. Provide signage designating the development as a “hotel” or similar visitor use.
11. Unless permission is granted by the Town of Canmore, snow clearing shall be handled on-site. No snow shall be pushed onto public land.
12. The Developer shall screen any mechanical equipment or vents to the satisfaction of the Development Officer.

Prior to the Release of the Development Permit Conditions

13. **Prior to the release of the Development Permit**, the Developer shall pay the following variance fees:
Four (4) approved variances:

Discretion limited in Land Use Bylaw 1 @ \$370.00 = \$370.00
Discretion not limited in Land Use Bylaw 3 @ \$200.00 = \$600.00
TOTAL FEES PAYABLE: \$970.00

Prior to the Release of the Building Permit and Commencement of Construction Conditions

14. **Prior to the release of the Building Permit**, the Developer shall provide a pre-construction energy report estimating that the building will achieve a minimum of 11% better than the current NECB in place at the time of development, to the satisfaction of the Development Officer.
15. **Prior to the release of the Building Permit**, the Developer shall submit addressing in accordance with the Town’s Civic Addressing Protocol.
16. **Prior to the release of the Building Permit**, the applicant shall submit and have approved a maintenance agreement satisfactory to the Town regarding the proposed landscaping and public plaza to be located within the future municipal Road Right-of-Way. This agreement shall include construction/installation, maintenance, replacement, and any other matters the Town deems necessary.
17. **Prior to the release of the Building Permit**, the applicant shall confirm street lighting will meet standards to the satisfaction of the Town of Canmore.
18. **Prior to Commencement of Construction**, the Developer shall protect all natural vegetation in the road right of way with fencing unless such vegetation has been approved for removal as indicated on approved plans, including any approved Tree Protection Plan. Any unnecessary vegetation removal may result in a Stop Order, fines, and compensation.

Prior to Occupancy and Operation Conditions

19. **Prior to occupancy of the building**, the Developer shall provide evidence of compliance with the Town’s Enhanced Green Construction requirements.
20. **Prior to occupancy of the building**, the Developer shall apply for, receive approval for, and install signage for the development that designates the development as a hotel or similar visitor accommodation use, to the satisfaction Development Officer.

Signature
CHAIR, CANMORE PLANNING COMMISSION

Date

IS A NOTICE POSTING REQUIRED: YES

Rundle Park (PL 20220303) Building & Eaveline Variance Rationale

March 14, 2023

1. VARIANCES REQUESTED AND WHY

a) BUILDING HEIGHT VARIANCE OF 20% (19.2M)

- **APPLICABLE BYLAWS:**

4.6.3.11 THE MAXIMUM BUILDING HEIGHT IS 16.0M.

4.6.9 SPECIAL VARIANCE CONSIDERATIONS

4.6.9.1 NOTWITHSTANDING THE VARIANCE REGULATIONS SET OUT IN 1.14, THE FOLLOWING VARIANCES MAY ALSO BE CONSIDERED WITHIN THE DISTRICT:

WHERE THE DEVELOPMENT AUTHORITY IS SATISFIED THAT THE ARCHITECTURAL INTEGRITY OF A BUILDING WOULD BE ENHANCED, VARIANCES MAY BE GRANTED TO ALLOW 20% OF THE BUILDING TO EXCEED THE MAXIMUM HEIGHT BY UP TO 20%. NO HEIGHT VARIANCES SHALL BE GRANTED BEYOND THE 20% RELAXATION.

1.14.3.1 VARIANCES IN EXCESS OF THE LIMITS DESCRIBED IN SUBSECTION 1.14.1.1 SHALL ONLY BE CONSIDERED AND MAY ONLY BE GRANTED BY THE DEVELOPMENT AUTHORITY WHERE THE DEVELOPMENT AUTHORITY IS SATISFIED THAT THE PROPOSED DEVELOPMENT PROVIDES AN EXTRAORDINARY NET BENEFIT TO THE COMMUNITY PURSUANT TO ONE OR MORE OF THE FOLLOWING:

c. ENHANCED GREEN CONSTRUCTION (SECTION 10)

- **VARIANCE REQUIREMENT RATIONALE:**

THIS VARIANCE SUPPORTS THE EXCEPTIONAL LEVEL OF QUALITY, ALPINE MASSING AND DETAILED ARTICULATION IN THE DESIGN OF THIS BUILDING. NO PART OF THE BUILDING EXCEEDS 20% ABOVE THE MAXIMUM BUILDING HEIGHT (16M), HOWEVER THE BUILDING AREA THAT EXCEEDS 16M DOES EXCEED 20%, MEASURING 57.9%. THIS IS IN LARGE PART DUE TO THE MOUNTAIN-PEAK INSPIRED ROOF-FORM WHICH OVERPOPULATES THE AREA ABOVE THE 16M WITH A DESIGN THAT THE BYLAW DOES NOT ANTICIPATE, BUT OFFERS A VERY COMPELLING AND RECOGNIZABLE ROOFLINE THAT REFLECTS THE MOUNTAIN CONTEXT. THE MTA TEAM DID REVIEW IF MAKING THE BUILDING FLOOR TO FLOOR HEIGHT WOULD BE WORTHWHILE, BUT THIS ULTIMATELY WOULD NOT RESULT IN A BENEFIT FOR THE COMMUNITY. A CHANGE OF REDUCING THE FLOOR TO FLOOR HEIGHT WOULD RESULT IN AN OVERALL 3' DIFFERENCE TO BUILDING HEIGHT, WHICH WOULD NOT SUBSTANTIALLY; CHANGE THE BUILDING AREA PERCENTAGE, IMPROVE THE EXPERIENCE OF NEIGHBOURING PROPERTIES, NOR WOULD IT LESSEN THE PERCEIVED MASS OF THE BUILDING OVERALL. HOWEVER THIS CHANGE WOULD COMPROMISE CLEARANCE FOR FIRE-TRUCK ACCESS BELOW THE PORTE-COCHERE, LIMIT SPACE FOR UNIT VENTILATION VIA THE FAÇADE, AND PREVENT THE ADDITION OF 1 VISITOR ACCOMMODATION UNIT. IT IS ALSO IMPORTANT TO RECOGNIZE THAT THE DEVELOPMENT REPRESENTS TOTAL FLOOR AREA AS CLOSE TO THE MAXIMUM ALLOWABLE GFA, WHILE THE OWNER COULD ACHIEVE THE SAME FLOOR AREA WITH A BUILDING THAT COMPLIES WITH BYLAWS BUT IS MUCH MORE DENSE, WITH LESS

ARTICULATION IN THE MASSING, AND WITH A LESS STRIKING DESIGN. INSTEAD, THE OWNER OF THIS DEVELOPMENT HAS ELECTED TO INVEST IN A DESIGN THAT STRIKES A BALANCE BETWEEN THE TRADITIONAL MOUNTAIN AESTHETIC ESTABLISHED IN CANMORE AND A MORE MODERN ALPINE LOOK WITH HIGH QUALITY GLU-LAM TIMBER STRUCTURE.

b) EAVELINE VARIANCE OF 29% (9M)

- **APPLICABLE BYLAWS:**

4.6.3.12 THE MAX EAVELINE HEIGHT IS 7.0M

4.6.9.1 NOTWITHSTANDING THE VARIANCE REGULATIONS SET OUT IN 1.14, THE FOLLOWING VARIANCES MAY ALSO BE CONSIDERED WITHIN THE DISTRICT:

- b. THE REQUIRED EAVE LINE HEIGHT OF A PROPOSED DEVELOPMENT MAY BE RAISED TO 9.0M WHERE THE DEVELOPMENT AUTHORITY IS SATISFIED THAT THE INTENT OF THE “MASSING AND SCALE GUIDELINES” IN SECTION 11: COMMUNITY ARCHITECTURAL AND URBAN DESIGN STANDARDS

- **VARIANCE REQUIREMENTS RATIONALE:**

THIS DEVELOPMENT COMPLIES WITH THE 9M MAXIMUM ALONG THE STREET FACING FAÇADE, ALL OTHER FACADES REQUIRE A VARIANCE FOR A 13M EAVELINE. WE FEEL IT IS REASONABLE THAT THE FACADES WITH SUBSTANTIALLY LESS VISIBILITY FROM THE PUBLIC REALM DO NOT REQUIRE COMPLIANCE WITH THE EAVELINE RESTRICTION AS IT WOULD OVERCOMPLICATE ROOF GEOMETRY AT INNER CORNERS AND OBSCURE VIEWS OF THE MOUNTAINS FROM. THE DESIGN TEAM HAS MAINTAINED THE COMPOSITION OF EACH FAÇADE BY INTRODUCING A SCREEN ELEMENT WHICH PROVIDES THE SAME HORIZONTAL COUNTER TO THE VERTICAL BALCONIES THAT A LOWERED EAVELINE WOULD. FURTHER TO THE VISUAL ASPECT OF THE EAVELINE, THE MASSING AND IT'S RESULT ON THE NEIGHBOURING PROPERTIES' VIEWSHED AND ACCESS TO LIGHT IS NOT COMPROMISED BY THIS DESIGN. SEE DIAGRAMS ON SHEET DP6.01.

2. THE PROPOSED DEVELOPMENT (AND VARIANCES) PROVIDE A COMMUNITY AND NEIGHBOURHOOD BENEFITS EXCEEDING WHAT IS REQUIRED PER BYLAW

a) AESTHETIC VALUE & HIGH LEVEL OF QUALITY

Raising the level of quality for one development can have a significant positive impact on the community. This is particularly true when it comes to the aesthetic value and use of high-end materials and amenities.

First and foremost, a development that is designed with aesthetics in mind can have a significant impact on the community. People are naturally drawn to beautiful buildings and spaces, and a well-designed development can create a sense of pride and ownership within the community. This can lead to increased foot traffic and tourism, as well as a boost in local businesses.

In addition, the use of high-end materials and amenities can have a direct impact on the quality of life for residents and visitors. High-end materials are typically more durable and require less maintenance over time, which can save money and resources in the long run. Amenities such as the spa pools, children's park, public plaza and future cafe can also provide residents with much-needed resources and a sense of community.

Furthermore, a development that raises the level of quality can serve as a model for future projects in the area. By setting a high standard for quality and design, other developers may be inspired to follow suit, leading to a domino effect of improved development projects throughout the community.

Overall, it is clear that raising the level of quality for one development can have a significant positive impact on the community. By prioritizing aesthetics and the use of high-end materials and amenities, developers can create a space that is not only beautiful and functional, but also sustainable and beneficial for the community as a whole.

b) SUSTAINABLE - EXCEED ENHANCED GREEN CONSTRUCTION

This development goes beyond the Green Building targets for the Town of Canmore and offers a minimum of 11% improvement over NECB standards, far exceeding the minimum requirements for Enhanced Green Construction in Canmore. Specific investments that add to enhanced green performance include but are not limited to; the Lumon Balcony system, triple pane glazing, glazing with a U-Value of 1.65 for lower head gain, Insulation along foundation walls and below slab, heat pump system. In addition to this 10 EV ready stalls have been included within the parkade to ensure the community and visitors can transition away from fossil fuels in time.

Energy efficiency has significant value for both individual developments and the broader community. At the level of a single development, energy-efficient

practices can help to reduce energy consumption and lower energy bills, which can save money and make the development more financially sustainable over time. Additionally, energy-efficient buildings can be more comfortable for occupants, as they are less likely to experience temperature fluctuations and other discomforts associated with poor building performance.

However, the benefits of energy efficiency extend far beyond the boundaries of a single development. By reducing energy consumption, energy-efficient buildings help to reduce the overall demand for energy in a community, which can help to lower energy prices and reduce the need for new energy infrastructure. This, in turn, can have a positive impact on the environment, as less energy consumption means less greenhouse gas emissions and less strain on natural resources.

Furthermore, energy-efficient buildings can serve as models for other developments in the community, demonstrating the economic and environmental benefits of energy efficiency and inspiring others to follow suit. This can create a positive feedback loop, as more and more developments adopt energy-efficient practices and contribute to a more sustainable, resilient, and prosperous community.

Overall, the value of energy efficiency for a single development lies not only in the cost savings and occupant comfort it can provide, but also in its potential to contribute to a broader community-wide shift towards a more sustainable and resilient future.

c) PLACEMAKING PUBLIC AMENITY

This development includes a high-end public plaza connecting the main entry, pedestrian/cycle route, and bus stop. A plaza can have significant value to the public as a space for placemaking and community building, especially in a tourist destination in the mountains and along a popular bike trail.

A plaza can serve as a central gathering space for locals and visitors alike. It can be a place to relax, socialize, and enjoy the natural beauty of the surrounding area. This can help to create a sense of community and belonging among residents, as well as provide a welcoming atmosphere for tourists. This will help enrich the experience for passersby and elevate the experience along Bow Valley Trail as an entry point for the town.

This gathering space can also offer some relief to infrastructure serving this bike trail which is already over-taxed by users such as the Canmore Visitor Information Centre.

Third, a plaza can serve as a hub for outdoor recreation. In a mountain tourist destination, it is likely that visitors will be drawn to the area for its natural beauty and opportunities for outdoor activities. A plaza located directly along a popular bike trail can provide a convenient place for cyclists to rest and recharge, as well as

a starting or ending point for rides. This can also help to promote sustainable transportation and support healthy lifestyles.

Overall, a plaza can have a range of benefits for the public in terms of placemaking and community building. In a tourist destination in the mountains and along a popular bike trail, it can serve as a vital hub for both residents and visitors, promoting social interaction, cultural activities, and outdoor recreation.

d) ROAD WIDENING SETBACK

When this development sells a portion of the property to the town for road-widening, it has the potential to benefit the community in several ways. Here are some of the most significant benefits:

Improved Traffic Flow: In the future, should the town desire to pursue former plans for road widening, widening roads can help alleviate traffic congestion, making it easier and faster for people to get around along perimeter roads. This can improve overall mobility, reduce travel time, and make commutes more efficient.

Improved Traffic Calming: Based on the current ARP for Bow Valley Trail the intention for this road setback is to develop ample planting and support the multi-model pathway system. Heavily treed roadways allow drivers to recognize their speed more readily and markedly reduce the speed at which cars travel. The pathways flanking the roadway on Bow Valley Trail will greatly benefit from more cautious drivers who move at a reduced speed.

Reduced Heat Island Effect: The increase to planting, including trees along the setback of Bow Valley Trail will offer shade in the summer months and can naturally lower the temperature in the surrounding environment by several degrees, particularly when shade is offered to asphalt roads and pathways, as is the case here.

Increased Safety: Wider roads allow for generous space for varied modes of transportation, each safely isolated from the other with dedicated lanes serving vehicle drivers, pedestrians, and bicyclists, reducing the risk of accidents and making the area safer for everyone.

Consistent Streetscape: When this road setback is honoured as part of this development it creates a more consistent streetscape, enhancing the aesthetic appeal of the community allowing the vision in the ARP for Bow Valley Trail to be actualized. This can help attract new residents and businesses to the area, boosting the local economy.

Enhanced Property Values: A consistent streetscape can also help enhance property values, making homes and businesses more attractive to buyers and investors.

Overall, selling a portion of the development's property to the town for road-widening can have many benefits for the community. It can help improve traffic flow, increase safety, create a consistent streetscape, and enhance property values, all of which can contribute to a more prosperous and livable community.

e) STAFF HOUSING – EMPLOYEE HOUSING

This development proposes an employee housing unit adjacent to the CRU. This unit could be accessed via the CRU or from the side of the building, with a dedicated entry point on the main floor which is distinct from all visitor accommodation circulation. The inclusion of employee housing this development offers several benefits to the community. This employee housing unit is a space that pairs residential and commercial use in one location, allowing residents to both live and work in the same location. Here are some potential benefits to consider:

Addressing the Staff Housing Deficit: By including an employee unit in the development, the town has more potential to provide an affordable housing option for employees who work in the tourism industry. This can be especially helpful with the rising cost of living and housing supply.

Supporting Small Business Owners: Employee housing units paired with a retail space can be a great option for entrepreneurs who want to start a small business but cannot afford the high costs of commercial rent. By providing a space for both living and working, the town can encourage small business development, which can be a boost to the local economy.

Promoting Sustainable Development: By combining residential and commercial spaces, live-work units can reduce the need for commuting, which can help reduce carbon emissions and promote sustainable development.

In conclusion, the inclusion of a live-work unit in a development can provide several benefits to Canmore by addressing the housing shortage, supporting small businesses, fostering a sense of community, and promoting sustainable development, live-work units can be a valuable addition to the town's planning strategy.

f) STREET ACTIVATION/SECURITY – STREET ORIENTED UNITS AND DYNAMIC FAÇADE

In addition to the CRU frontage and main accommodation entry, this development offers street-oriented visitor accommodation units that are directly facing the street, with entrances and windows oriented towards the sidewalk. Such units provide a number of benefits to the community, including improved safety and a more dynamic streetscape.

One of the main benefits of street-oriented units is improved safety on the street. When there are people living or working on the street, there are more eyes on the

street and a greater sense of community ownership of the space. This can deter criminal activity and increase the sense of safety for everyone who uses the street, including pedestrians and cyclists.

In addition to improved safety, street-oriented units also contribute to a more dynamic and vibrant streetscape. When there are businesses and residences directly facing the street, there is more activity and interaction between people on the street. This can lead to more foot traffic, more socializing, and a greater sense of community on the street.

Overall, street-oriented units offer numerous benefits for the community, including improved safety on the street and a more dynamic streetscape. By encouraging more street-oriented development, cities and developers can create more livable and engaging communities for everyone who lives, works, and visits the area.

3. THESE VARIANCES WOULD NOT UNDULY INTERFERE WITH THE AMENITIES OF THE AREA OR MATERIALLY INTERFERE WITH, OR AFFECT THE USE, ENJOYMENT, SAFETY, AESTHETICS, OR VALUE OF NEIGHBOURING PROPERTIES

a) ACCESS TO LIGHT

Access to light is an essential aspect of the livability and comfort of a property. Natural light not only illuminates a space, but also has significant health benefits, such as regulating the body's circadian rhythm and improving mood. Additionally, a well-lit property can enhance safety and security, as well as the overall aesthetic appeal of the space.

Because of considered siting, this building does not unduly interfere with the amount of natural light that neighboring properties receive. By locating the building as near to the NW property line as possible, this design and the neighbouring property benefit from a natural gap in development created by the URW along that boundary. A similar gap is likewise created by the at grade parking located on the SE boundary of the site. Given the substantial road setback along the length of the development on the NE side, there is no issue with access to light for neighbouring properties on the other side of Bow Valley Trail. This design ensures that the new development is respectful of neighboring properties and does not detract from their livability or value, and this design

b) ACCESS TO VIEWS

Access to mountain views is a highly valued feature for visitor accommodation properties in Canmore. The stunning natural scenery of the Canadian Rockies is a major draw for visitors, and the ability to enjoy mountain views from their accommodations can greatly enhance their experience.

Having mountain views can provide several benefits for visitor accommodation properties. Firstly, it can increase the attractiveness of the property, leading to higher occupancy rates and the ability to charge higher prices. This is particularly true in Canmore, where visitors come specifically to experience the beauty of the mountains.

Additionally, mountain views can enhance the overall experience of guests, creating a sense of relaxation and tranquility. This can help visitors feel more connected to nature and their surroundings, which can make their stay more memorable and enjoyable.

It is important to ensure that any development complies with Canmore bylaws and does not unduly interfere with access to views for neighbouring properties. As described in the previous section for access to light, the siting of the building, combined with surrounding setbacks and features of the town, such as the rail, the building does not obscure existing views or access to light.

c) ACCESS TO SITE FOR NEIGHBOURING PROPERTIES

Respecting a neighboring property's access to site as a new development is essential for several reasons. It promotes good relations between neighbors and helps to build trust and respect in the community. When property owners feel that their interests and concerns are being considered, they are more likely to be supportive of new developments in their area.

The Owner of this development elected to accommodate the preference of the town's transportation department by aligning the site access point with the driveway on the other side of Bow Valley Trail can help to improve traffic flow and safety in the area. This can have significant benefits for both the new development and the surrounding community by reducing congestion and the risk of accidents.

d) PRIVACY FOR UNITS ON NEIGHBOURING PROPERTIES

This development does not interfere with the privacy of neighboring properties, this will promote harmonious living and minimizes potential conflicts between neighbours. Privacy is a fundamental human right and a critical aspect of comfortable living. Therefore, respecting the privacy of neighboring properties is essential for this development project.

This respect for privacy is achieved in the building design through proper orientation, setbacks, and screening with plants to minimize any potential negative impacts on the neighboring properties. The SE property line is the only interface shared directly with an alternate building facade. This is the backside of the Chateau Canmore low rise visitor accommodation. This condition is first addressed by minimizing the number of units on the Rundle Park building which face this direction, orienting balconies towards the view of Mt. Rundle or Bow Valley Trail where possible. Second, the setback between the two buildings is maximized to allow visitors in both buildings to appropriately feel they are in their own space. Finally, this boundary is reinforced with a planting screen which offers privacy while not obscuring access to light and views as mentioned earlier in this document.

e) AESTHETICS

The design of Rundle Park is inspired by detailed timber architecture development. Developments that share a similar aesthetic can have several values for a community, especially when it comes to a mountain or alpine aesthetic. Here are a few potential benefits:

Sense of Community: When buildings in a community share a similar aesthetic, it can create a sense of belonging and community. This is because the aesthetic creates a shared identity and can serve as a unifying factor. This can lead to stronger community connections, a greater sense of pride in the community, and increased social interaction among community members.

Attractiveness: A community that shares a mountain or alpine aesthetic can be visually stunning, with buildings that blend in with the natural surroundings. This can make the community more attractive to residents, visitors, and potential investors. The aesthetic can also help to preserve the natural beauty of the surrounding landscape.

Economic Benefits: An aesthetically pleasing community can be a draw for tourism and recreation, which can bring economic benefits to the area. This can lead to job creation, increased tax revenue, and increased economic activity in the community.

Environmental Benefits: A community with a mountain or alpine aesthetic may be more likely to prioritize environmentally sustainable practices, such as energy-efficient buildings, green spaces, and recycling programs. This can lead to a cleaner, healthier environment for residents, visitors, and wildlife.

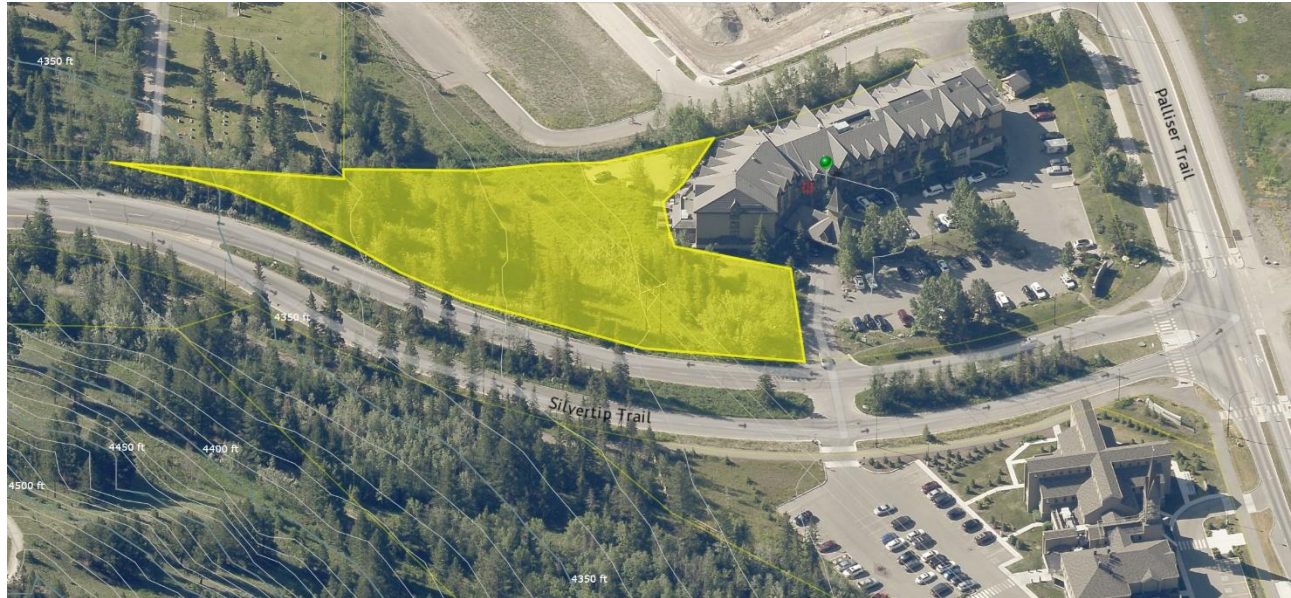
Overall, this development shares a strong alpine aesthetic with its neighbours and this can have numerous benefits Canmore, including a stronger sense of community, increased attractiveness for the Town, economic benefits, and environmental benefits.

f) VALUE OF NEIGHBOURING PROPERTIES IMPROVED

Overall, the development offers only an increase in value to surrounding properties, especially if the existing use on and quality of development for this property is considered.

CANMORE PLANNING COMMISSION

STAFF REPORT



DATE:	APRIL 26, 2023
PROPOSED DEVELOPMENT:	EXCAVATION, STRIPPING AND GRADING
APPLICATION NUMBER:	PL20230005
LEGAL DESCRIPTION:	LOT I, BLOCK 71, PLAN 971 1512
CIVIC ADDRESS:	I SILVERTIP TRAIL
CURRENT USE(S):	VISITOR ACCOMMODATION
APPLICANT:	MCELHANNEY LTD.
REFERENCED DOCUMENTS:	TOWN OF CANMORE MUNICIPAL DEVELOPMENT PLAN LAND USE BYLAW 2018-22: <ul style="list-style-type: none">• SECTION 1.15 USES NOT LISTED BUT ALLOWED IN ALL DISTRICT• SECTION 14.11 SILVERTIP TRAIL DIRECT CONTROL DISTRICT

EXECUTIVE SUMMARY

This application proposes to prepare the site for the potential future expansion of the existing Visitor Accommodation to include outdoor pool areas, saunas, and spa amenities. Site preparation will include tree clearing, stripping and grading of the development area. The proposed development is consistent with the definition of Excavation, Stripping and Grading, which is considered a Discretionary Use in all land use districts. An Erosion and Sediment Control Plan and Construction Management Plan were submitted with the application and deemed consistent with the Town of Canmore standards. No variances are proposed for the development and Administration recommends the application be approved.

BACKGROUND

The Town of Canmore received an application for Excavation, Stripping and Grading on Lot 1, Block 17, Plan 971 1517 (see Attachment 1). The parcel is located at 1 Silvertip Trail and is designated Silvertip Trail Direct Control District (DC District) (see Attachment 5). The purpose of the application is to prepare the site for the potential future expansion of the existing visitor accommodation development to include outdoor pools, saunas, and spa amenities.

EXISTING SITE

The site is located at Lot 1, Block 17, Plan 971 1521 and consists of approximately 1.44 hectares. The site is currently developed with visitor accommodation (hotel). The site area subject to the proposed development is approximately 0.7 hectares and contains an AltaLink transmission line. The site is bound by Silvertip Trail to the west and north, Palliser Trail to the south, the future Canmore Emergency Services Building to the east, and the Canmore Cemetery to the northeast. The Shrine Church of Our Lady of the Rockies is located west of the subject property across Silvertip Trail (see Attachment 1).

BYLAW CONFORMANCE/VARIANCE DISCUSSION

Although not explicitly listed as a use in the DC District, the proposed Excavation, Stripping and Grading development is considered a discretionary use as per Town of Canmore Land Use Bylaw (LUB) Section 1.15 - Uses Not Listed But Allowed in All Districts, included below:

1.15 USES NOT LISTED BUT ALLOWED IN ALL DISTRICTS

- 1.15.0.1 The following uses are discretionary in all districts, regardless of whether they are listed in the district:
- a. Excavation, Stripping and Grading. Where permitted, if the site is not subject to additional development within nine (9) months after the commencement of excavation, stripping or grading, remediation of the site to control sediment and erosion, dust, and aesthetics shall be completed to the satisfaction of the Town. This may include planting of vegetation of a type and quantity similar to adjacent areas.
 - b. Signs, subject to the regulations of Section 9: Signage of this Bylaw.
 - c. Public Utility, when not listed as a Permitted Use. [2020-16]
 - d. Administrative/Sales Office. [2021-24]

In accordance with this section, Excavation, Stripping and Grading is considered a discretionary use in all districts, including the DC District.

Excavation, Stripping and Grading is defined in the LUB as follows:

Excavation, Stripping and Grading means development on a site that may include removal of vegetation, re-grading, or the excavation of material in isolation of that authorized as part of a development permit or subdivision.

The proposed development includes the clearing of trees, stripping and grading of the northern portion of the site (see Attachment 3). The proposed development is consistent with the 'Excavation, Stripping, and Grading' definition and therefore can be considered on this site.

Development Authority:

The Development Authority for development permit applications within the DC District is established under Section 14.11.9 as outlined below:

14.11.9 Development Authority

The Canmore Planning Commission shall be the development authority for this District.

The application proposes to clear trees, strip and grade the site. Details of the proposed development and operation are outlined within the submitted Erosion and Sediment Control Plan, which is included in Attachment 4. The Land Use Bylaw does not contain specific regulations for Excavation, Stripping and Grading. The DC District requires each application to submit a visual impact assessment due to the highly visible location of the site from the Trans-Canada Highway and Palliser Trail. The applicant's submitted a visual impact assessment showing renderings of viewsheds from the Trans-Canada Highway and Palliser Trail. The development area is partially screened by the existing visitor accommodation building and existing landscaping at the southern portion of the site. Also, the excavation, stripping and grading development will be temporary until the final visitor accommodation expansion is complete.

The parcel and proposed development area is adjacent to the Lower Silvertip Wildlife Corridor. The Town of Canmore Municipal Development Plan provides the following direction regarding development adjacent to wildlife corridors:

New Development Within or Adjacent to Wildlife Corridors and Habitat Patches

- 4.2.11 No new development shall be allowed within a habitat patch or corridor, excepting:
 - a. infrastructure and utilities may be allowed to be located within or to cross a habitat patch or corridor in the least intrusive manner possible, and
 - b. non-intensive, trail-based recreational uses may be allowed.
- 4.2.12 Where new development is considered pursuant to 4.2.11, an EIS shall be required by the Town where it has the authority.
- 4.2.13 Development proposals within or adjacent to a wildlife corridor or habitat patch shall have regard for the *BCEAG Wildlife Corridor and Habitat Patch Guidelines for the Bow Valley (2012)* and most recent principles of wildlife conservation to ensure the values and function of the corridor or habitat patch are not compromised.
- 4.2.14 For the purposes of this section, the Town will determine adjacency consistent with the *BCEAG Wildlife Corridor and Habitat Patch Guidelines for the Bow Valley (2012)*. Not all proposals deemed adjacent will require an EIS – the requirement for an EIS will be evaluated based on the nature and scope of the proposed development, including the type of land use and the intensity of the development, as well the potential for adverse environmental impacts.

The applicants are proposing to retain trees at the northeast corner of the site and provide screening along the Silvertip Trail landscaped median, which is the area of the site adjacent to the Wildlife Corridor. As a result, Administration determined an Environmental Impact Study (EIS) was not required for the excavation, stripping and grading development. Further analysis regarding the requirement for an EIS will be provided through the visitor accommodation expansion development permit application.

The work required to facilitate the excavation, stripping and grading on site may create impacts such as noise, dust, mud tracking, odour and litter. The erosion and sediment control plan outlines measures to mitigate these potential impacts. Further, the applicants will be required to submit a construction management plan in accordance with Town standards to further ensure these potential impacts are addressed mitigated. The construction management plan will include the following:

- Noise, litter, dust and mud control and mitigation;
- Parking for contractors and employee vehicles;
- Access for construction and emergency vehicles;
- Truck haul routes;
- Signage including contractor contact information;
- Waste management.

Specific conditions regarding the submission of an Erosion and Sediment Control Plan, Construction Management Plan have been added to the Schedule A, included in Attachment 4.

SUSTAINABILITY SCREENING REPORT (SSR)

A Sustainability Screening Report will be provided with the Visitor Accommodation expansion development permit application.

SUBMITTED COMMENTS

A Notice of Application has been posted on the site pursuant to the Land Use Bylaw. No comments were received at the time of writing this report.

OPTIONS FOR CONSIDERATION

The CPC has three options:

1. Approve the application subject to the conditions or in addition to others than those contained in Schedule A.
2. Refuse the application, specifying reason(s) for refusal.
3. Postpone the application, pending submission of any additional details requested by CPC.

RECOMMENDATION

The Planning and Development Department recommends that the Canmore Planning Commission **APPROVE** PL20230005, subject to the conditions of approval set out in Schedule A attached to this report.

ATTACHMENTS:

1. Site Context
2. Zoning – DC District
3. Proposed Development Area
4. Schedule A – Conditions of Approval



Riley Welden, MCIP, RPP
Development Planner



Lauren Miller, MCIP, RPP, AICP
Manager, Planning and Development

ATTACHMENT I – SITE CONTEXT



Figure 1: Aerial View Location of Subject Site (looking east)



Figure 2: Overview of Site and Adjacent Uses

ATTACHMENT I – SITE CONTEXT

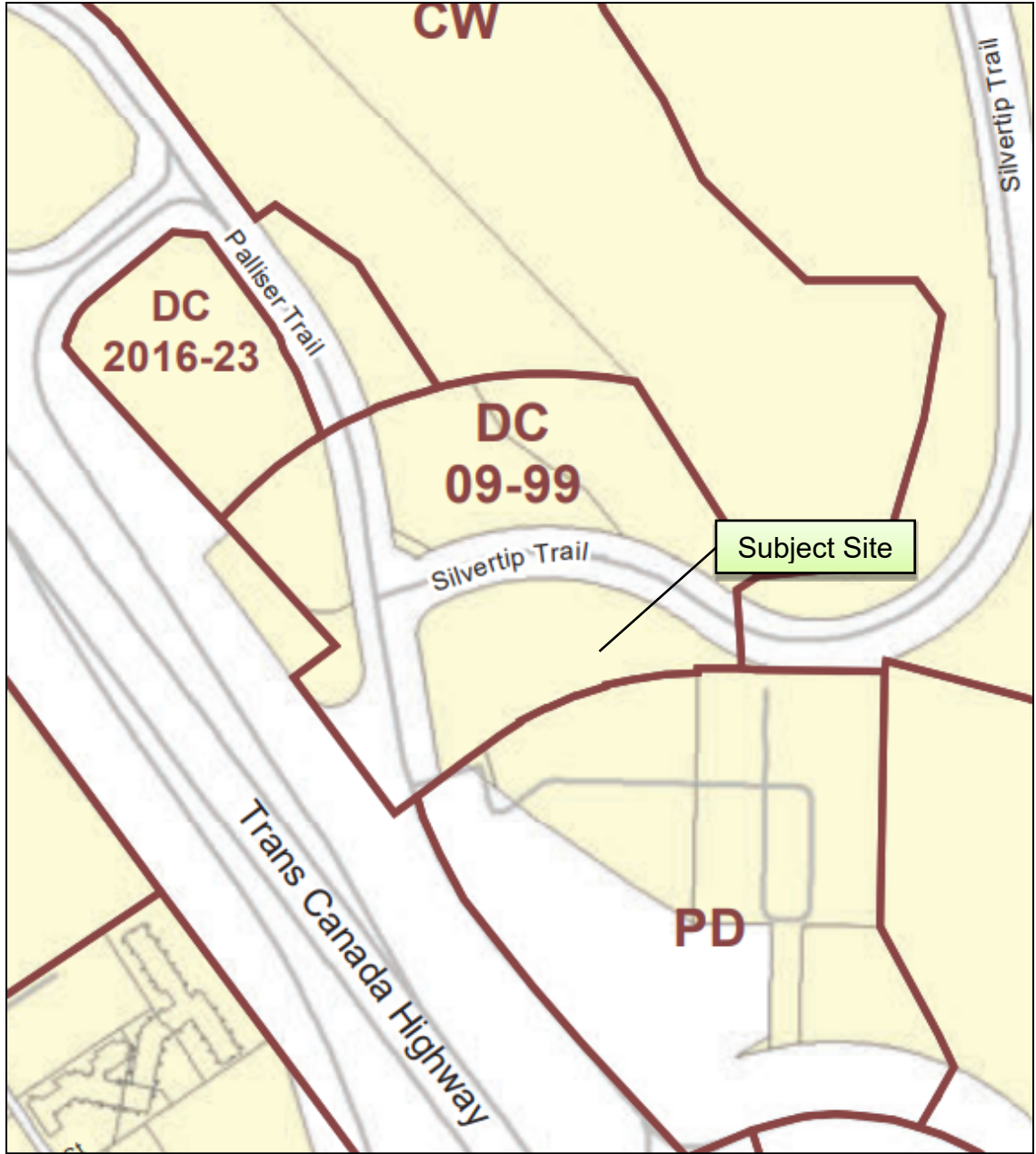


Figure 3: View looking east from Silvertip Trail

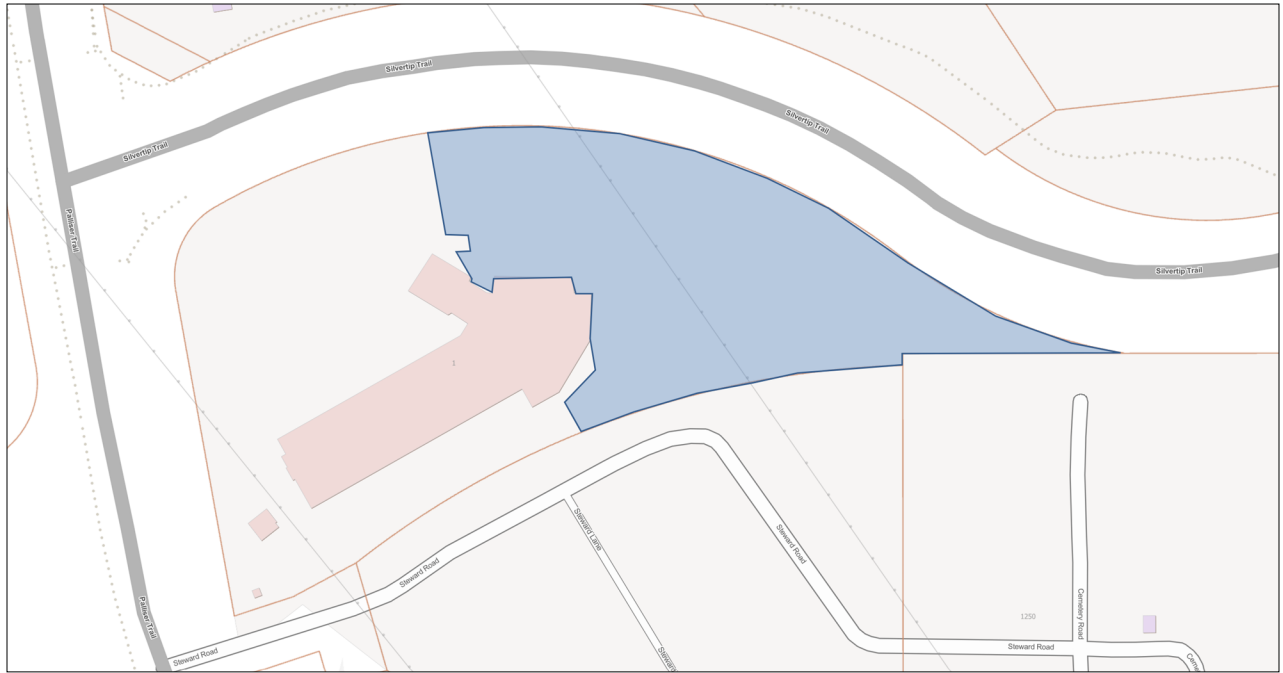


Figure 3: View looking east from Silvertip Trail

ATTACHMENT 2 – ZONING



ATTACHMENT 3 – PROPOSED DEVELOPMENT AREA



ATTACHMENT 4 – SCHEDULE A – CONDITIONS OF APPROVAL

SCHEDULE A
CONDITIONS OF APPROVAL

DEVELOPMENT PERMIT No.: PL20230005

LAND USE DISTRICT: Silvertip Trail Direct Control District

APPROVED USE(S): Excavation, Stripping and Grading

APPROVED VARIANCE(S): None

MUNICIPAL ADDRESS: 1 Silvertip Trail

LEGAL ADDRESS: Lot 1 Block 17 Plan 971 1512

APPROVED VARIANCES

1. None

STANDARD CONDITIONS:

1. All construction associated with the approval of this Development Permit shall comply with the regulations of the Land Use Bylaw (LUB) 2018-22, unless otherwise stated under the approved variances section of this document.
2. All construction associated with the approval of this Development Permit shall comply with the Town of Canmore Engineering requirements as outlined in the Engineering Design and Construction Guidelines (EDCG).
3. All construction associated with the approval of this Development Permit shall comply with the Tree Protection Bylaw and ensure all tree protection measure are appropriately put in place prior to the development of the site, where determined necessary by the Town of Canmore Parks Department.
4. All construction, landscaping and exterior finishing materials are to be as shown on the approved plans and other supporting material submitted with the application.
5. Any trees, shrubs or other plant material installed as part of the landscaping plan which may die or are blown over, shall be replaced on an ongoing basis, prior to receipt by the developer of a Development Completion Certificate.
6. Access to the site for emergency vehicles shall be to the satisfaction of the Manager of Emergency Services.
7. **No occupancy** shall be permitted until an Occupancy Certificate has been issued by the Town of Canmore.

SPECIFIC CONDITIONS:

1. **Prior to the Commencement of Construction**, the Developer shall submit an Erosion and Sediment Control Plan to the satisfaction of the Town of Canmore.

ATTACHMENT 4 – SCHEDULE A – CONDITIONS OF APPROVAL

- 2. **Prior to the Commencement of Construction**, the Developer shall submit a Construction Management Plan to the satisfaction of the Town of Canmore.
- 3. **Prior to the Commencement of Construction**, the Developer shall submit correspondence from the owners of any rights-of-way within the development area acknowledging the excavation, stripping and grading development to the satisfaction of the Town of Canmore.
- 4. The Developer shall comply with the approved Erosion and Sediment Control Plan and Construction Management Plan.
- 5. The hours of operation shall be limited to daylight hours and in accordance with the Town of Canmore Noise Bylaw.
- 6. The Developer shall erect fencing or similar screening along the northwest and east property boundaries adjacent to Silvertip Trail to the satisfaction of the Town of Canmore.
- 7. The Developer shall retain existing trees along the northeast boundary of the site to the satisfaction of the Town of Canmore.
- 8. The development permit shall be in effect for nine months from the date of issuance.
- 9. The Developer shall remediate the approved development area to the satisfaction of the Town of Canmore should additional development of the approved development area does not commence within nine months of the commencement of the excavation, stripping and grading. Remediation, may include sediment, erosion and dust control, and landscaping and other aesthetic measures.

Signature

Date

Chair, Canmore Planning Commission

IS A NOTICE POSTING REQUIRED: **YES** **NO**