

ADOPTED BY COUNCIL  
APRIL 23, 2024



# **THREE SISTERS VILLAGE | PHASE 1**

## **CONCEPTUAL SCHEME**

### Planning Rationale and Support Report

The report has been prepared for submission to the Town of Canmore by QuantumPlace Consulting on behalf of Three Sisters Mountain Village Properties Ltd.

April 17, 2024



## Supporting Reports Have Been Provided By:



### WSP

**(including the companies formerly known as Golder,  
and Wood Engineering)**

Civil Engineering, Transportation, Undermining,  
Wildlife Fence, and  
Monitoring and Adaptive Management Reports



### Englobe

Pre and Post Grading Slope Stability



### BGC

Steep Creek Engineering and Mitigation Design



### Landmark

Traditional Ecological Knowledge Assessment:  
Desktop Study



### NAK

Landscape Architecture

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Note: For full scale technical drawings please refer to the technical drawing package submitted under separate cover





## Land Acknowledgment

Three Sisters Mountain Village is located within the Treaty 7 region of Southern Alberta. In the spirit of respect, reciprocity and truth, we honour and acknowledge the Canmore area, known as "Chuwapchipchiyan Kudi Bi" (translated in Stoney Nakoda as "shooting at the willows") and the traditions and cultures of the signatories to Treaty 7, the Îyârhe Nakoda (Stoney Nakoda Nations) – comprised of the Bearspaw First Nation, Chiniki First Nation, and Goodstoney First Nation, – Tsuut'ina First Nation, and the Blackfoot Confederacy – comprised of the Siksika Nation, Piikani Nation, and Kainaiwa Blood Tribe. We acknowledge that this territory is home to the Rocky View Métis District 4 within the Battle River Territory. We acknowledge all Nations who live, work, honour, and celebrate this territory.



# Application Overview

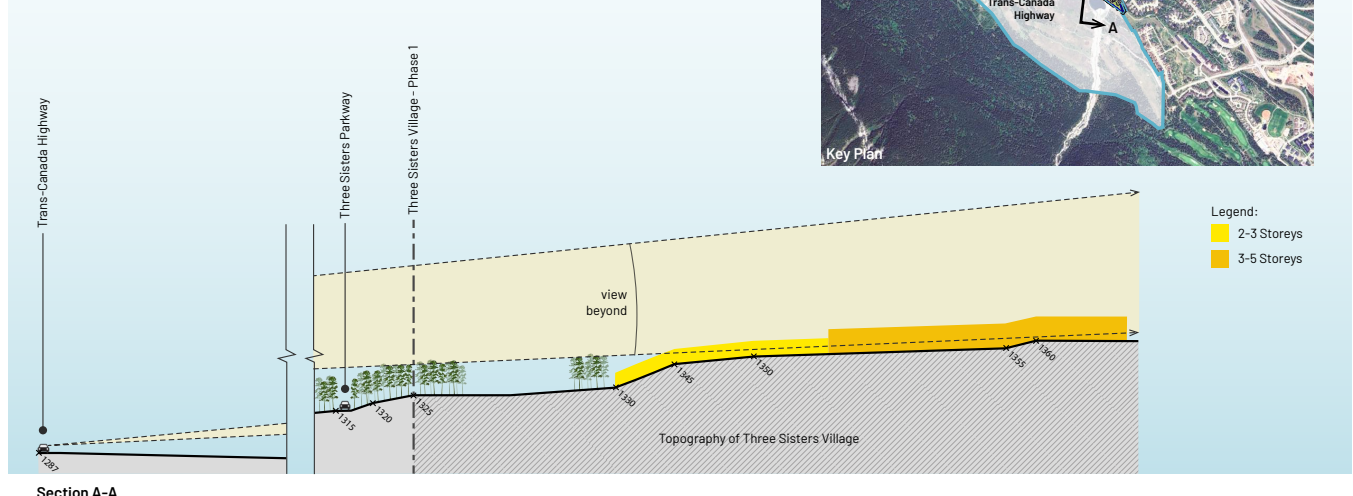
## Accompanying Reports & Supporting Information

In accordance with the approved ASP and Corporate Directive (#2021-02), on “Conceptual Scheme (CS) Preparation and Amendment”, Three Sisters Mountain Village Properties Limited (“TSMVPL”) submits this Conceptual Scheme rationale and supporting report in addition to the following materials:

- Provincially Approved Historical Resources Act Application;
- Subdivision Mining Impact Assessment Report;
- Pre-Development Slope Stability Assessment;
- Post-Grading Geotechnical Report
- Three Sisters Village Phase 1 Geotechnical Investigation with Phase 1 Geotechnical Report Addendum;
- Three Sisters Village Phase 1 Conceptual Scheme Mobility Assessment;
- Wildfire Risk Assessment;
- Traditional Ecological Knowledge (TEK) Assessment: Desktop Study;
- Three Sisters Creek Phase 1 Detailed Mitigation Design;
- Wildlife Exclusion Fence Design Plan (Accepted by the Province);
- BGC Wildlife Fence Commentary Technical Memorandum;
- Provincial Fence Design Letter;
- 2020 Environmental Impact Statement (EIS): Three Sisters Village Area Structure Plan
- 2024 Servicing Report;

- 2020 Master Drainage Plan;
- 2024 Site Master Drainage Plan
- Monitoring and Adaptive Management Plan (Accepted by the Province);
- Provincial MAMP & WEFPD Acceptance Letter
- Vegetation and Wetlands Technical Data Report;
- Wildlife and Biodiversity Technical Data Report; and
- TSMV DRAFT Design Guidelines.
- Alta-Link Bridge Specifications

A Visual Impact Assessment has not been included within this submission in alignment with Policy 4.5.1 and 4.5.4. Dependent on tree cover rooftops of parcels 29, 30b, 31, 32a and 32b may be visible from the Trans-Canada Highway adjacent to the Plan Area (See **FIGURE 1**). A Visual Impact Assessment would be submitted at Development Permit for parcels 33a, and 33b if the buildings are proposed at or above 5 storeys as per ASP policy. The VIA along with building form, placement, and proposed building materials analysis would determine if any mitigation should be undertaken.



**FIG 1. - VISUAL IMPACT ASSESSMENT CROSS-SECTION DIAGRAM**



## Application Processing Requirements

The Land and Property Rights Tribunal (LPRT) determined that the Three Sisters Village and Smith Creek ASPs are consistent with the 1992 Natural Resources Conservation Board (NRCB) decision and directed the Town to approve them as statutory plans. A joint Land Use and Conceptual Scheme application for Three Sisters Village Phase 1 is contained in this document and within appended documents, reports, technical drawings and figures .

The ASP requires that a joint Conceptual Scheme and land use redesignation application be submitted (ASP Policy 11.2.3). In accordance with Section 619 of the Municipal Government Act (MGA), the Town is required to approve the application within 90 days of submission. The application for land use redesignation would have to be approved, through three Readings of Council. If a municipality that is considering an application under 619 holds

a Public Hearing, the hearing may not address matters already decided by the NRCB except as necessary to determine whether an amendment to the Land Use Bylaw is required. As the Conceptual Scheme adheres to the Three Sisters Village ASP, deemed wholly compliant with the NRCB decision and adopted on October 24, 2023, no Public Hearing is required

Given the legal proceedings at the LPRT and Court of Appeal, TSMVPL has submitted this application in alignment with all requests for studies outlined in the Corporate Directive. A pre-application meeting occurred on October 31, 2023 to provide Administration with an overview of the proposed submission. A pre-application response document is included with the submission.





## NRCB, ASP, Conceptual Scheme & Land Use Alignment

Acknowledging the amount of time elapsed between this and previous governing document applications, **TABLE 1** has been prepared that summarizes the alignment between this application, the NRCB Decision and the Three Sisters Village ASP.

Table 1: NRCB, ASP and CS Alignment Summary

	<b>NRCB</b>	<b>ASP</b>	<b>Conceptual Scheme/ Land Use</b>
Gross Developable Area (GDA)	680 ha	The ASP Plan Area is a maximum of ±169 ha.	The Conceptual Scheme and land use application is for a single phase within the ASP area (29.07 ha). <i>The LPRT determined that the Three Sisters Village ASP is wholly consistent with the NRCB approval.</i>
Visitor Accommodation/ Hotel Rooms/ Tourist Homes	2,425	1,460-2,210 within Three Sisters Village ASP area. There are no hotels proposed within Phase 1. Tourist Homes are allowed on two parcels within Phase 1. Tourist Homes are counted within the 3,000 residential units.	Tourist Homes are enabled on two parcels per ASP policy via the proposed Overlay for Three Sisters Village. <i>The LPRT determined that the Three Sisters Village ASP is wholly consistent with the NRCB approval.</i>
Base Level Residential Units Approved without Density Bonusing	6,085 residential units	Allows 3,000 residential units within Three Sisters Village ASP area.	Allows up to 716 residential units via a maximum units per hectare (UPH) using the proposed Three Sisters Village standard districts. <i>The LPRT determined that the Three Sisters Village ASP is wholly consistent with the NRCB approval.</i>
Maximum # Of Units With Bonusing	Population densities (beyond what was approved by the NRCB) may be changed with the approval of the Town of Canmore.	An additional 2,000 units can be achieved through Density Bonusing above the baseline of 3,000 residential units.	Strategic objectives of Council linked to increasing Perpetually Affordable Housing (PAH) units and climate change objectives are enabled via a Density Bonusing mechanism due to limits within the MGA as to what a municipality can regulate or mandate. Density Bonusing is proposed via a Density Bonusing Overlay within the Canmore Land Use Bylaw that specifically implements the policies within the Three Sisters Village ASP. <i>The LPRT determined that the Three Sisters Village ASP is wholly consistent with the NRCB approval.</i>
Employee Housing	700	Employee housing (EH) is implemented in two ways within the Three Sisters Village ASP. When hotels are proposed, the ASP has established requirements for EH in policy 8.3.1. Prior to Phase 3, 200 EH beds will be constructed and prior to Phase 5, an additional 150 beds will be constructed (S. 11.4.2).	EH is not required to be constructed at this time. It is noted that a future builder may choose to build EH within Phase 1. 200 beds of EH are proposed within the Gateway at Three Sisters Village of which 110 beds will be attributed to the Phase 3 EH requirements. Visitor Accommodation is not being proposed within Phase 1. Policy 8.3.1 requires EH beds to support any hotels that are proposed. This detail would be provided at the Development Permit stage if a hotel development was being proposed. <i>The LPRT determined that the Three Sisters Village ASP is wholly consistent with the NRCB approval.</i>



	<b>NRCB</b>	<b>ASP</b>	<b>Conceptual Scheme/ Land Use</b>
Transportation	General transportation alignments were outlined.	Transportation network and layout is in alignment with the NRCB.  ASP and Three Sisters Mountain Village's Global TIA aligns to the Town's Integrated Transportation Plan (ITP) objectives.	The Conceptual Scheme and proposed road network is in alignment with the ASP and will be formally designated as road right-of-way as a part of a future subdivision in alignment with this application.  <i>The LPRT determined that the Three Sisters Village ASP is wholly consistent with the NRCB approval.</i>
Open Space (Municipal Reserve, Environmental Reserve and Resort Recreation Amenity Area (RRAA))		Open space areas and the amenities proposed within them, including Municipal and Environmental Reserve areas, have been proposed in alignment with Map 13. Open space is in alignment with the policies identified within the ASP and proposes approximately 40% of the Plan Area to open space.	Open space in Phase 1 represents about 42% of the Phase 1 application area.  <i>The LPRT determined that the Three Sisters Village ASP is wholly consistent with the NRCB approval.</i>
Undermining	Undermining to be addressed per Condition #6 of the NRCB approval. To satisfy this requirement, the Province adopted a specific regulation to be adhered to.	Proposed development must be in accordance with the Canmore Undermining Regulation (AR 34/2020) of the Municipal Government Act. Mitigation (if required), review and reporting as per the Guidelines under Section 7 approved by Ministerial Order (AR 34/2020).	An Undermining Study for the Phase 1 Area was conducted per the Canmore Undermining Regulation (AR 34/2020) of the Municipal Government Act. Mitigation (if required), review and reporting will be conducted as per the Guidelines under Section 7 approved by Ministerial Order (AR 34/2020).  <i>The LPRT determined that the Three Sisters Village ASP is wholly consistent with the NRCB approval.</i>
Wildlife Corridors & Adaptive Management	Required corridors to be approved by the Province	The wildlife corridor system around TSMV has been completed via the Alberta Environment and Parks (AEP) 2020 Wildlife Corridor Decision. A Monitoring and Adaptive Management Plan (MAMP) must be submitted with the first Conceptual scheme.  A Wildlife Fence Report must be submitted with the first Conceptual Scheme.	The Province has reviewed and accepted the MAMP and determined that it falls within provincial jurisdiction. This report is submitted along with this application.  The Province has reviewed and accepted the Wildlife Exclusion Fence Design Plan. This document is submitted along with this application.  The Province will be taking on the management and operation of the wildlife fence.  <i>The LPRT has determined that the Three Sisters Village ASP is wholly consistent with the NRCB approval.</i>
Housing Affordability	Outlines that affordability would be provided by the housing form proposed.	Requires that a minimum of 10% of all residential units will be affordable housing.  Three Sisters Village ASP proposes smaller, more compact housing forms.	The general Three Sisters Village Overlay implements the ASP policy (8.1.1) that requires a minimum of 10% of all residential units (exclusive of tourist homes, visitor accommodation and employee housing) to be Affordable Housing. Due to the MGA not allowing municipalities to undertake any inclusionary housing measures the Bonus Overlay addresses compensation for the additional Affordable Housing units provided to meet the ASP requirements, at the Development Permit stage.  <i>The LPRT has determined that the Three Sisters Village ASP is wholly consistent with the NRCB approval.</i>



# Administration

## Purpose – Conceptual Scheme & Land Use Application

On behalf of TSMVPL, QuantumPlace Consulting prepared an application for Conceptual Scheme and land use redesignation for Phase 1 of the proposed development of the Three Sisters Village resort area. Phase 1 is located in the northeast corner of the Three Sisters Village ASP area legally described as Plan 2010793, Block 1, Lot 1.

This Conceptual Scheme and Land Use application is in conformity with the Three Sisters Village ASP as indicated within this planning rationale and support report and provided in summary within **APPENDIX III** within the Three Sisters Village Phase 1 Application Consistency Table. Land use redesignation will be done concurrently, and that the area covered by a Conceptual Scheme will generally align to the phases identified in Map 22 of the ASP and on **FIGURE 2** within this document. Standard land use districts contained within Canmore's Land Use Bylaw should be used where appropriate (Policy 11.2.5) and provide for an approximate land supply of five years.

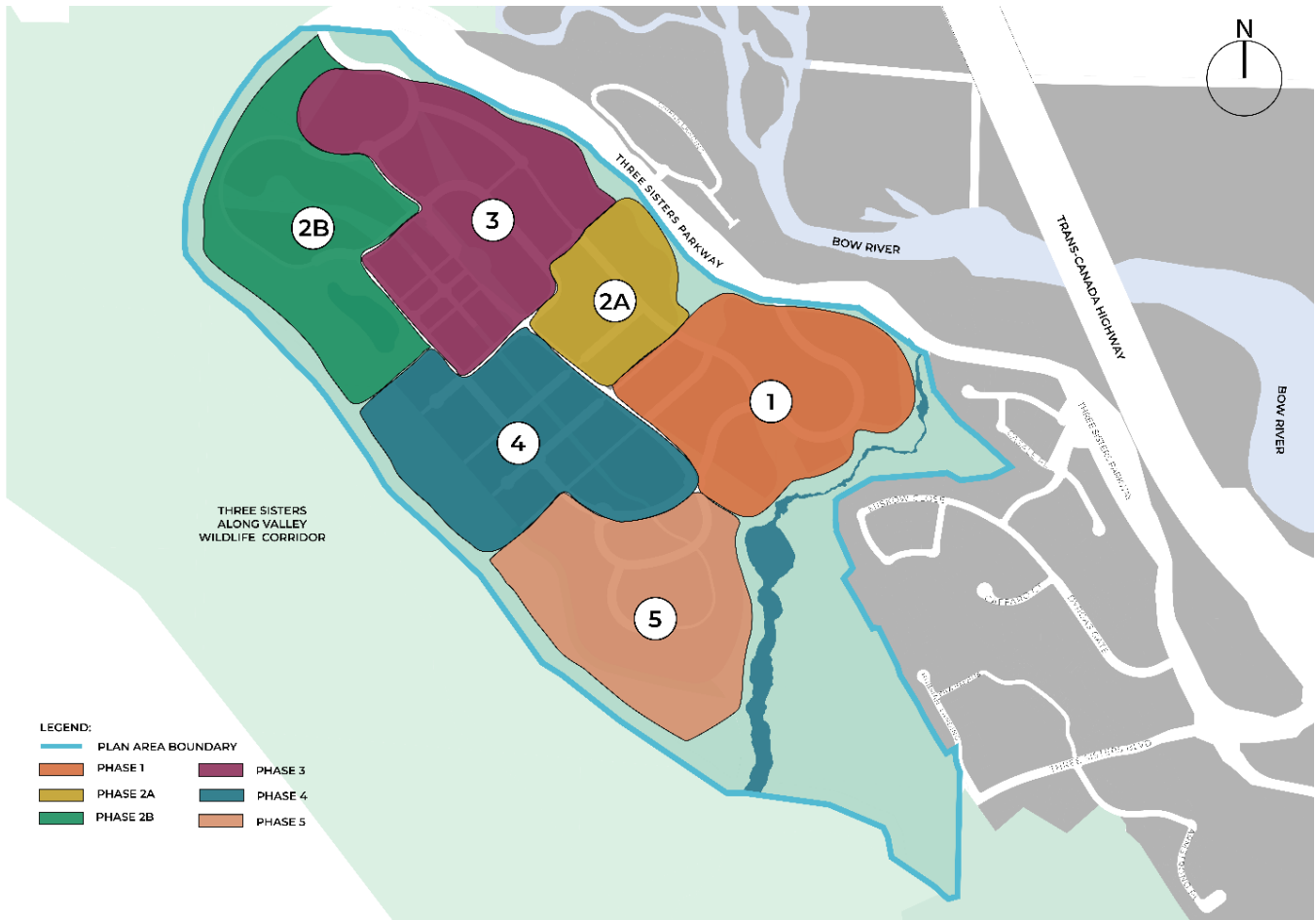
This Conceptual Scheme is a comprehensive mechanism to ensure appropriate planning considerations are addressed prior to any future subdivision development application(s)

being submitted. A Conceptual Scheme aligns land use redesignations (or zoning) to a future plan of subdivision. While land use changes are adopted by bylaw, a Conceptual Scheme is adopted by resolution of Council. The Conceptual Scheme can provide the municipality, the developer, and adjacent landowners with reasonable assurance of the future subdivision while respecting the overall form and function of future development on the subject land.

The Town of Canmore has adopted a Corporate Directive governing Conceptual Schemes (#2021-02) and identifies its purpose "... to ensure the orderly subdivision and development of an area of land." and goes on to describe that a "...CS is a non-statutory plan that provides a framework, based on an approved ASP, to inform land use and subdivision applications as they relate to the future phases of subdivision and development of adjacent lands." This application is intended to amend the existing land use districts within Phase 1 to align with ASP policy and provide ways in which to guide the future development and implementation of the Three Sisters Village resort neighbourhood.



## Timeframe



**FIG 2. - THREE SISTERS VILLAGE PHASING MAP (ASP MAP 22)**

Following the Conceptual Scheme and land use approval of this phase, further municipal approvals will be required including subdivision, development permits and building permits. As a result, this application represents an approximate 3-year supply of residential land that will only become available to build upon in the year 2026 at the earliest. It could take three to four years from the date of the approval of this Conceptual Scheme and land use redesignation application before residents are able to occupy Phase 1.

There are currently no lands available for sale in TSMV. Development permits in Stewart Creek represent less than a 1-year land supply. In the Gateway at Three Sisters development, there are approximately 200 beds of EH proposed within the Gateway at Three Sisters Village of which 110 beds will be attributed to the Phase 3 EH requirements. Development permit timing is anticipated in the winter 2024 and represents a 1-year supply of residential housing. Given the timing of the application and current market absorption rates, there will be no residential land supply available within TSMV in 2026 without the approval of this development.



## Context

### Location

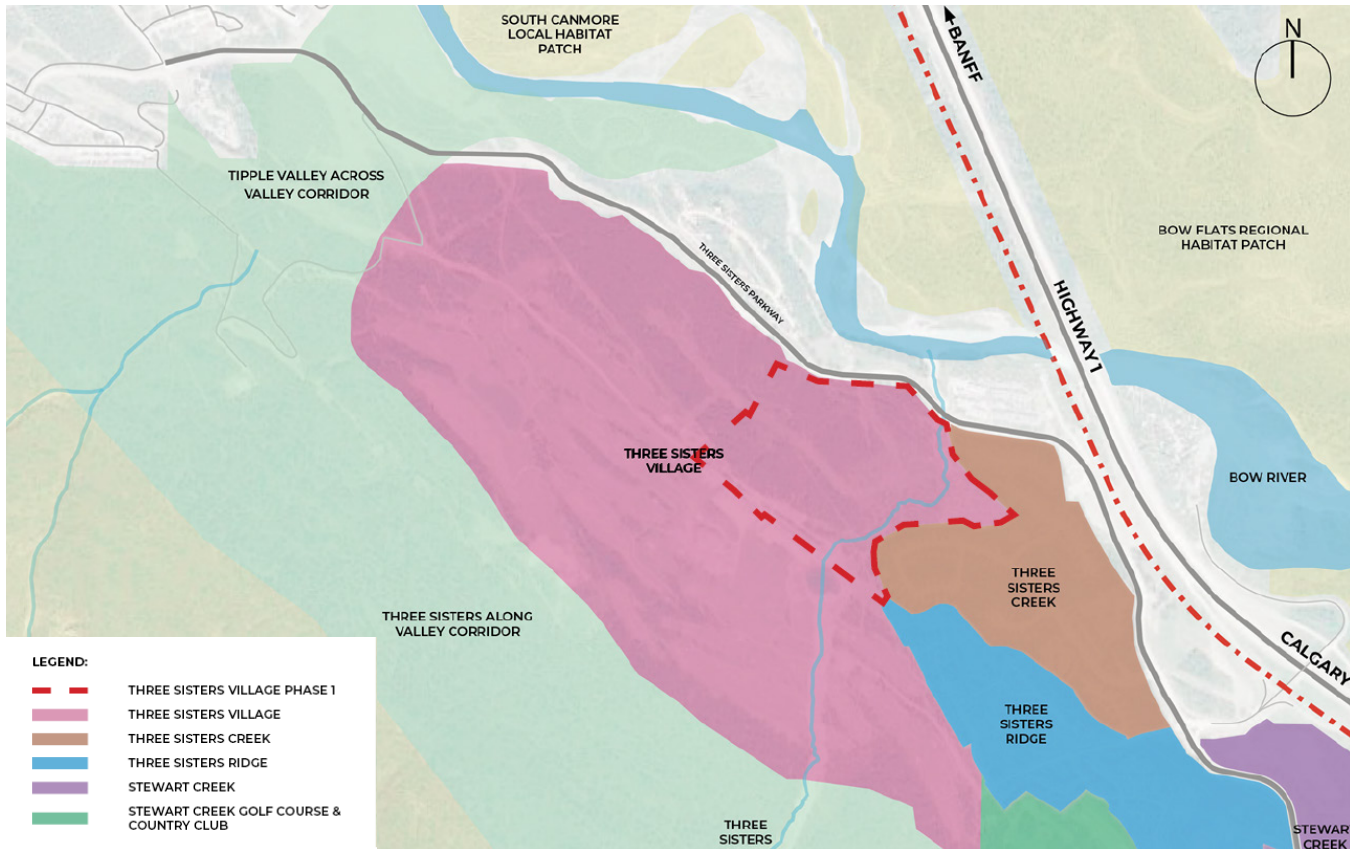


FIG 3. - CONTEXT MAP

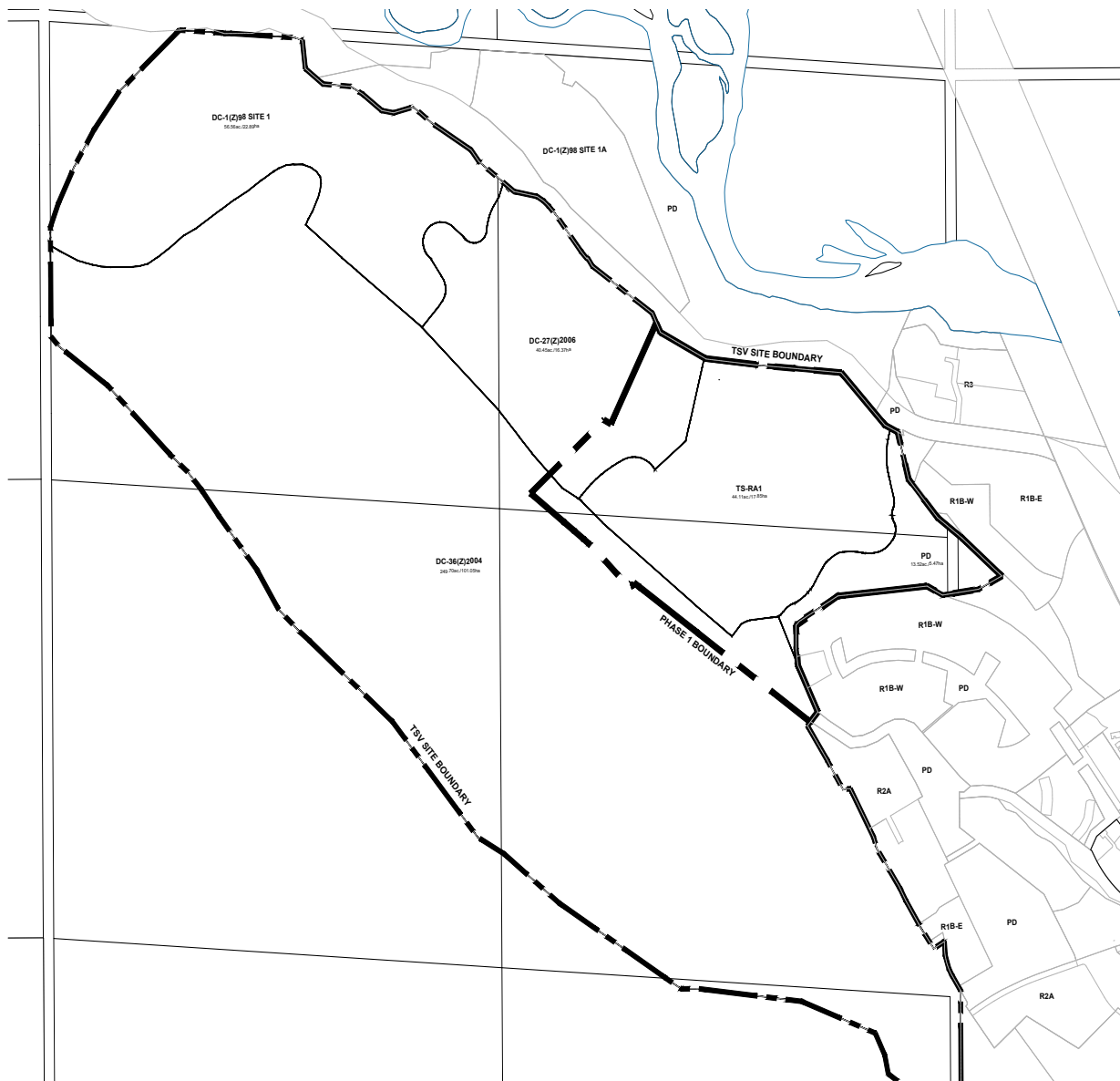
### Defining Characteristics

Phase 1 consists of approximately 29.05 ha of land owned by TSMVPL and is legally described as a portion of Plan 2010793, Block 1, Lot 1. The land is bisected by Three Sisters Creek.

The natural topography of the Phase 1 area slopes generally from southwest to northeast. The Phase 1 area is mostly vegetated with tree cover. Historically the area has seen significant industrial activities such as logging and coal mining. Certain areas of clearing exist that were introduced during the development of the abandoned golf course and installation of buried power lines. Three Sisters Creek runs through the Conceptual Scheme area along the eastern portion of the application area, flowing north toward the Bow River.

This is the first Conceptual Scheme proposed for the Three Sisters Village ASP area. Phase 1 is bounded by the Three Sisters Parkway to the north, Three Sisters Creek and Three Sisters Ridge neighbourhoods to the east and the AltaLink Right-Of-Way to the south as outlined in **FIGURE 2**. Existing approved land use within Phase 1 aligns with the 2004 Resort Centre ASP and includes the TS-RA1, DC 27Z2006, DC 1-98 (Site 1 and 2A) and DC 36Z2004 land use districts depicted in **DRAWING 1**. The 2020 Three Sisters Village ASP was adopted on October 24, 2023, Council will need to adopt the proposed land uses of this CS and land use application which includes new standard land use districts for Canmore's consideration.





**DWG1. EXISTING LAND USE PLAN**

### Provincial Wildlife Corridor

In 2020 the Province of Alberta approved the final portion of TSMV's wildlife corridor as documented in the Smith Creek wildlife corridor decision letter. The Province is responsible to maintain and manage wildlife corridors, including any necessary enforcement. TSMV is transferring ownership of designated wildlife corridor lands to the Province to better facilitate management and long-term oversight of the corridor. The Province is singularly responsible for the wildlife corridor and its management.

Submitted in support of the ASP, the Three Sisters Village Environmental Impact Statement (EIS) predicted that the Project would not contribute to any serious risks identified for wildlife or other valued environmental components (VEC). Key mitigation identified includes wildlife fencing, education, off-leash dog parks and other outdoor recreation opportunities. The EIS predicted that the application of mitigation will reduce environmental consequences



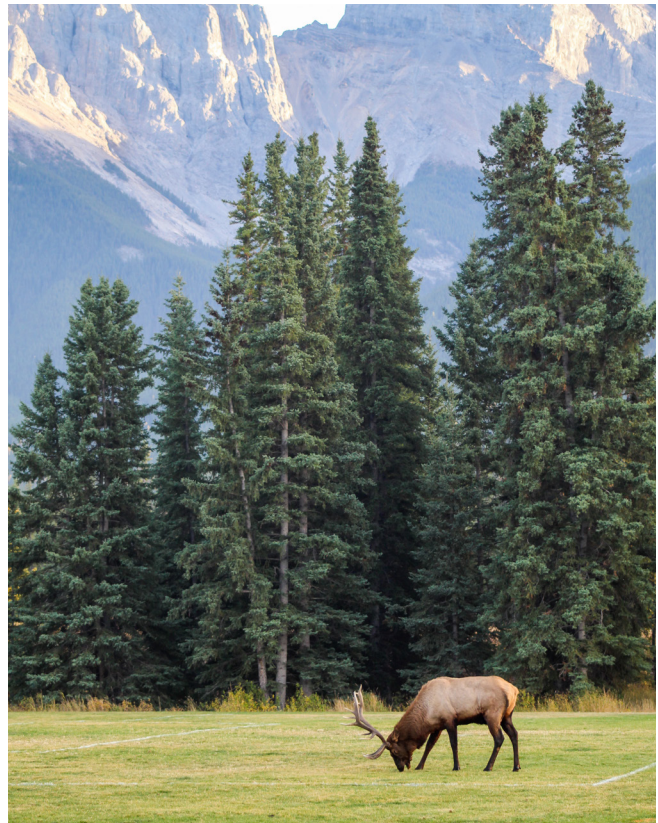
from the Project. In particular, the EIS predicts negligible to low residual effects on the environment from the Project. The only exception applies to grizzly bears, for which a moderate environmental consequence was identified due to habitat loss. The wildlife fence will limit access to the ASP footprint for bears. While this is a loss of habitat availability, the existing conditions where humans have unmitigated access to the wildlife corridor increasing the likelihood of human/bear conflict represents an ecological trap (i.e. the wildlife fence creates a positive outcome by reducing opportunity for human-bear conflict).

The following environmental benefits were also identified for the Project:

- By combining wildlife fencing with public awareness (e.g., signs) and alternative options for recreation (i.e., off-leash dog parks and designated trails) a substantial reduction in human use of undesignated trails in approved wildlife corridors is predicted, relative to existing conditions. Measures implemented by TSMVPL (wildlife fence, attractant management, creating spaces for people to recreate, and education initiatives) should improve public compliance with the Bow Valley Protected Areas Management Plan because many residents and visitors appear to be unaware of the restrictions that apply in designated corridors (e.g., seasonal trail closures and prohibitions on off-leash dog use).
- Changes in negative human-bear, human-cougar, human wolf, and human-elk interactions were predicted to be positive inside the ASP footprint but were considered neutral at the RSA scale owing to uncertainty about the level of negative interaction that could occur in adjacent wildlife corridors after development (i.e., outside the

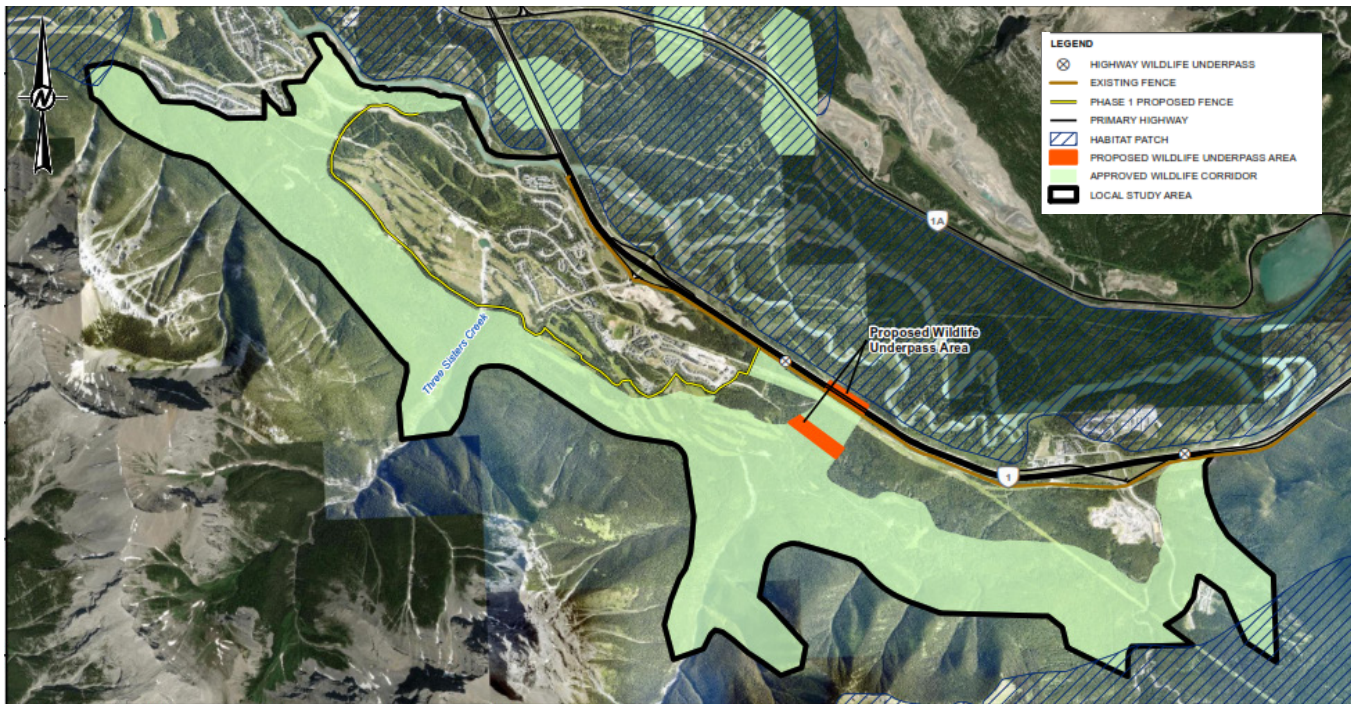
fence). For these conclusions to be maintained, mitigation and adaptive management strategies identified in the EIS, including monitoring, will be implemented as outlined in the EIS.

This Conceptual Scheme submission aligns with the recommendations found in Table 49 of the Three Sisters Village EIS. (**APPENDIX IV.**)



## Fence Design Report

In accordance with ASP policy 10.5.1, TSMVPL will be responsible to erect a wildlife fence around the ASP area in accordance with **DRAWING 2** and the Wildlife Exclusion Fence Design Plan. While the ASP stated the Town will be responsible for the fence, discussions with the Province have yielded a different outcome. The Province will be assuming the ongoing responsibility of operations and maintenance of the wildlife fence. In accordance with ASP Policy 11.1.1(b), a plan outlining the design requirements of the fence has been prepared and submitted as a part of this Conceptual Scheme and Land Use Amendment application. The extent of the proposed Phase 1 Wildlife Fence is found within **DRAWING 2**. Provincially approved, the Wildlife Exclusion Fence Design Plan (submitted under separate cover) provides general guidance for design elements of the fence, gates, wildlife escape mechanisms, and for areas that have special environmental considerations, such as water crossings. The approximate fence location is identified mostly on Crown lands and within the Stewart Creek Golf Course and Country Club. The finalized fence location will be determined at the time of detailed subdivision design. This report provides design elements that can be used by engineers and construction contractors who will complete the final fence design. The Plan is based on discussions with experts who have experience implementing wildlife fencing in the Bow Valley.



**DWG 2. PHASE 1 GENERAL ALIGNMENT OF THE WILDLIFE FENCE FOR PHASE 1**



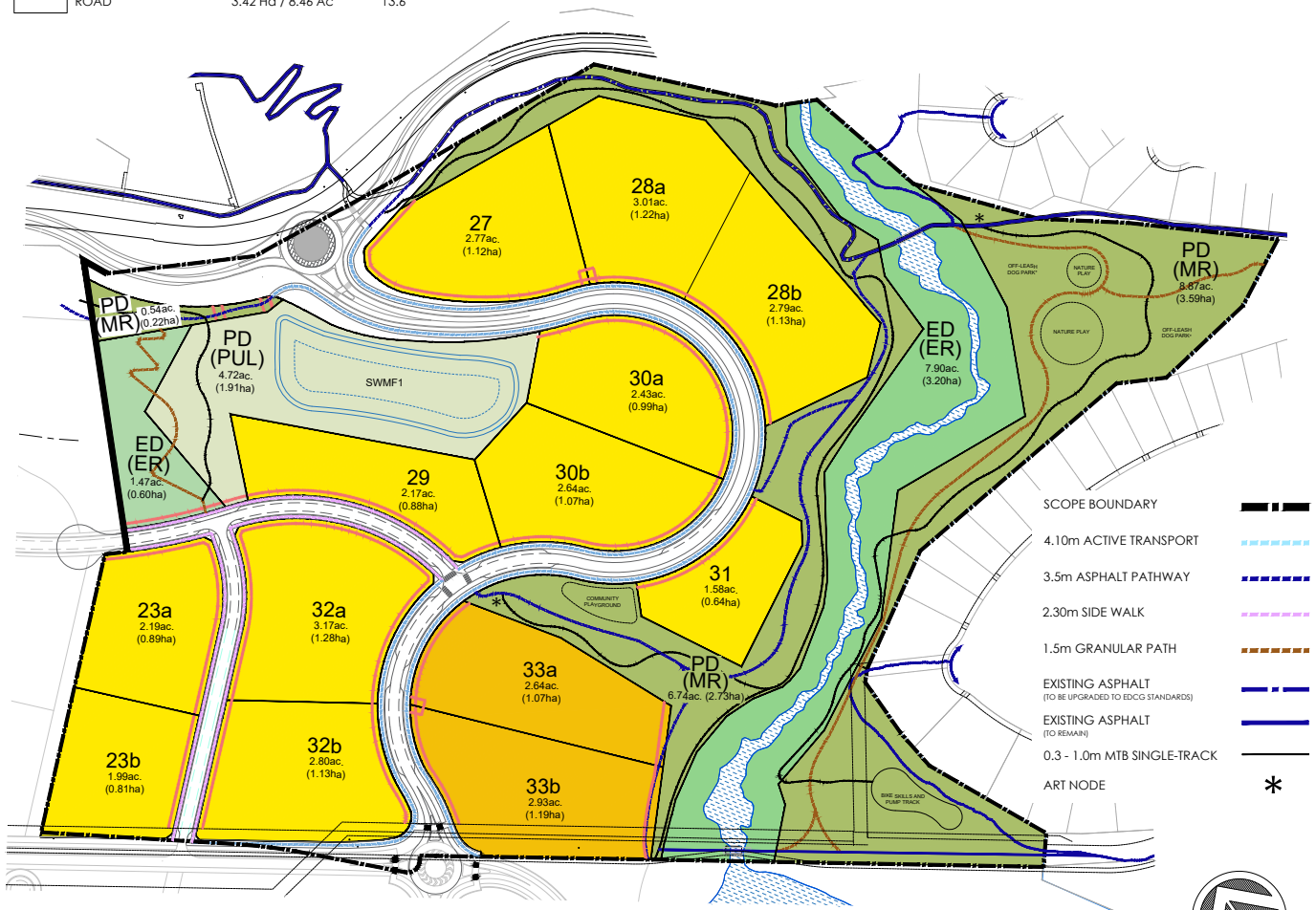


## Conceptual Scheme

The proposed Conceptual Scheme application is made up of a Conceptual Scheme Plan and a Land Use Plan along with supporting engineering technical details and reports. The Conceptual Scheme identifies proposed land use districts, parcel configuration, and a road network that are consistent with the ASP (**DRAWING 3**).



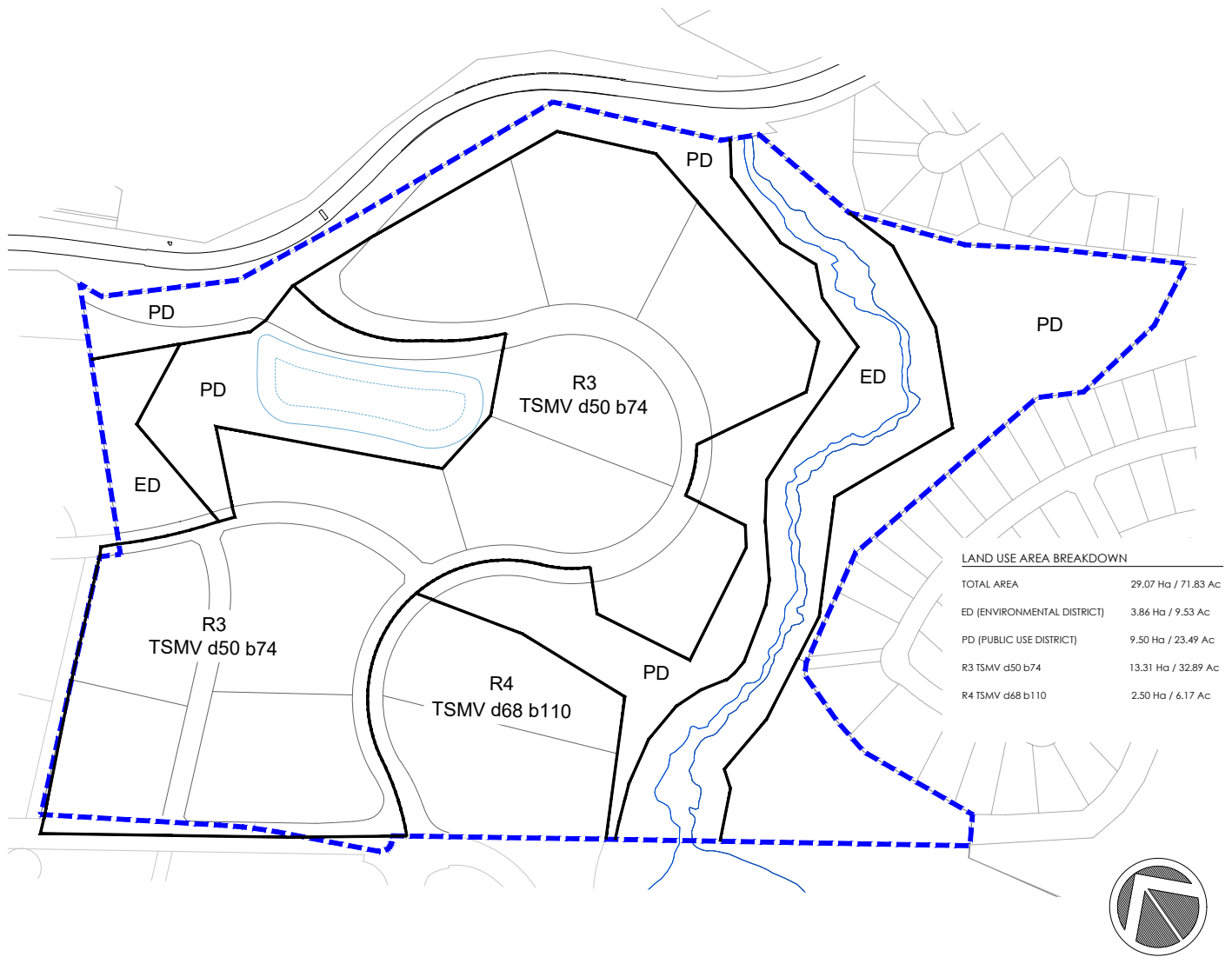
TOTAL AREA	29.07 Ha / 71.83 Ac	
ED (ER)	3.80 Ha / 9.38 Ac	
NET DEVELOPABLE	25.27 Ha / 62.45 Ac	100.0 (% NET DEVELOPABLE)
R3 TSMV d50 b74	11.15 Ha / 27.55 Ac	44.1
R4 TSMV d68 b110	2.26 Ha / 5.58 Ac	8.9
PD (MR)	6.53 Ha / 16.14 Ac	25.8
PD (PUL)	1.91 Ha / 4.72 Ac	7.6
ROAD	3.42 Ha / 8.46 Ac	13.6



DWG 3. PHASE 1 CONCEPTUAL SCHEME PLAN



## Land Use Plan



**DWG 4. PROPOSED LAND USE PLAN**

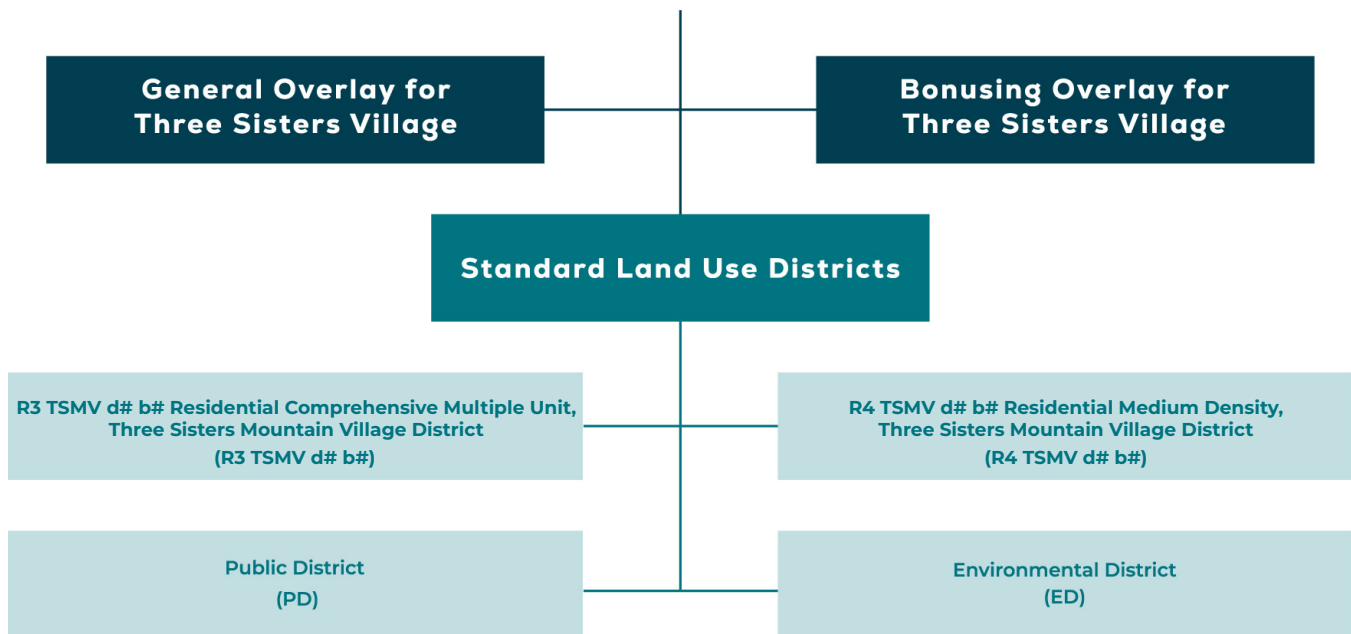
### Land Use Overview

In alignment with ASP policy 11.2.5, standard land use districts have been proposed within this application. In addition, a Three Sisters Village Overlay and an overlay for Density Bonusing are proposed to direct development and implement ASP policy (**FIGURE 4**).

The general overlay for the Phase 1 area is proposed to create rules that apply only to Three Sisters Village and to exempt or change rules within the current Land Use Bylaw that conflict with the direction or implementation of the ASP. The Bonusing Overlay for Three Sisters Village will work with the standard land use districts to ensure that developers applying for density bonusing have a consistent framework from which to work. The proposed land use districts, overlays and a Bonusing Calculator are submitted under separate cover in support of this application.



# Canmore Land Use Bylaw Amendments



**FIG 4. - STRUCTURE OF BYLAW IMPLEMENTATION FOR THREE SISTERS VILLAGE: PHASE 1**

## General Land Use Overlay

The Canmore Land Use Bylaw outlines the purpose of “Development Overlays” as a tool to facilitate the implementation of specific goals and objectives contained in adopted statutory plans. In this case, TSMV has proposed the use of Overlays for the purpose of implementing the Three Sisters Village ASP; the Three Sisters Village and Density Bonusing Overlays. As new phases come forward for municipal approvals, amendments may be needed to one or both overlays to ensure the ASP can be appropriately regulated. The Density Bonusing Overlay is required to be reviewed annually in accordance with ASP Policy 5.8.11 to ensure bonusable items remain relevant.

Per the Canmore Land Use Bylaw, an Overlay is used to provide measurable and implementable exceptions or changes to the rules of the Land Use Bylaw in alignment with ASP policy. Rules of the Land Use Bylaw that do not align with the implementation of the Three Sisters Village ASP within the current Canmore Land Use Bylaw include:

- Section 10 Green Building Regulations;
- Section 11 Community Architectural & Urban Design Standards; and
- Section 12 Density Bonus Regulations.

Further, Overlays are being used to:

- Add regulations that apply to the entire Three Sisters Village ASP area;
- Harmonize the measurement of height;
- Provide clarifications to parking for Tourist Homes and shared parking studies;
- Make adjustments to lighting zones to add clarity around open space lighting;
- Prohibit bed and breakfast developments;
- Provide clarity to Tourist Home regulations in alignment with ASP policy;
- Provide clarity to amenities for Multi-Unit Residential Development; and
- Insert new regulations to implement the wildlife fence.

Rationale for the specific amendments can be found within the proposed land use districts and overlays documents of the submission package and within the paragraphs below.



## Green Building Regulations

The Town of Canmore is not legally able to issue permits with requirements for buildings to exceed the current Alberta Building Code per s. 66(1) of the Safety Codes Act:

*“66(1) Except as provided in this section, a bylaw of a municipality that purports to regulate a matter that is regulated by this Act is inoperative.”*

To encourage aspects of sustainability (building construction, low impact development and sustainable energy generation) within the Three Sisters Village ASP area, policy has been developed to guide the use of density bonusing. It is through capping units and strongly encouraging developers to use bonusing that greenhouse gas (GHG) reduction will be achieved, alongside housing affordability, cultural and community incentives. Using the unit cap, the bonusing overlay allows for the opportunity for builders to choose building and energy sustainability and align to Council's climate change goals and action plans. As a result, climate change and sustainability are addressed through a specific Density Bonusing Overlay proposed for Three Sisters Village. Bonusing and the Bonusing Overlay is discussed in a later section. Bonusing is only earned if the Alberta Building Code is exceeded.

## Existing Community Architectural & Urban Design Standards

Section 11 of the Canmore Land Use Bylaw outlines the Community Architectural & Urban Design Standards. Section 11.1 speaks to public realm, site planning and design, and building design. TSMV is committed to providing a quality-built environment and public realm for residents and visitors to enjoy. Although the Three Sisters Village ASP and the Conceptual Scheme align with the goals of Section 11, the specificity in which architectural & urban design requirements have been written in the Bylaw is often in conflict with implementation of the ASP and the Density Bonusing Overlay. Landscaping, roof line requirements, and building form standards for commercial and mixed-use buildings are examples of where this difficulty has been encountered.

The proposed TSV General Overlay exempts the Conceptual Scheme from Section 11 of the Bylaw. By exempting the current Architectural & Urban Design Standards, TSMV and partnered builders can build sustainably while maintaining designs which acknowledge a mountain town character. The

current urban design guidelines are outdated and do not recognize advances in building design, new building code or how Section 11 standards will be implemented in light of bonusing. The combination of landscaping and bicycle rack calculations are not achievable at their current rate as they result in overages that can neither be serviced or utilized and cannot be squeezed onto a development parcel without coming into conflict with each other.

At the subdivision stage, TSMV will work to develop new Architectural Guidelines for Three Sisters Village that will maintain the mountain character of the town. A draft Urban Design Guideline for Phase 1 of Three Sisters Village has been submitted to the Town so they can see the direction of the vision of architecture and design for Three Sisters Village. This document will be amended over time. The guidelines are intended to be registered on title at the time of subdivision and may be updated from time to time by TSMV to incorporate learnings from the ongoing development within the ASP plan area or to recognize Alberta Building Code changes.

Commercial and mixed-use design standards within Section 11 have been based on outdated residential standards that do not account for current building code requirements, including materials and building façade articulation. To meet climate change reduction targets, green building must meet the latest in energy codes and standards – the current design guidelines are often in conflict with newer building codes. Achieving energy efficiency requires simplified roof lines, which reduces energy consumption. Current Canmore regulations regarding building massing and roof style prevent efficient building practices.

TSMVPL will submit architectural guidelines for Three Sisters Village for consideration at the time of subdivision application. These guidelines and standards will be developed by a qualified design professional. This will ensure a cohesive built form achieved through updated building and design standards and allow for more sustainable modern mountain architecture.

## Existing Bonus Density Overlay

Canmore has its own density bonusing overlay section within the Land Use Bylaw, however, those rules have not been updated for some time and do not consider the specific parameters for which density bonusing is outlined for Three Sisters Village. As such, the current Density Bonusing overlay should not apply to TSMV and a new one is proposed that



aligns to Three Sisters Village ASP policy.

### **Land Use Districts**

The majority of the Phase 1 area is proposed as R3 TSMV Residential Comprehensive Multiple Unit District (R3 TSMV), and based on the R3 District, which provides for comprehensively designed, medium density, multi-unit residential development and other compatible neighbourhood uses. Uses listed within the R3 TSMV District align with the ASP which identifies residential and medium density residential areas within Phase 1. Within the residential policy area, semi-detached dwellings (only when steep slopes require it per ASP policy) and townhomes must be accommodated. As a result, the apartment uses have been eliminated from this district to achieve alignment with ASP policy. Stacked townhomes are only permitted when it is necessary to achieve bonus densities. Reduced front yard setbacks have been proposed to align with ASP policy intended to create active, pedestrian friendly streetscapes.

The area of the ASP corresponding with medium density residential in Phase 1 is made up of two (2) parcels proposed as R4 TSMV Residential Medium Density District. This district is based on Canmore's R4 district with some modification to allow for uses in alignment with ASP policy. Tourist Homes may occur on these parcels within the ASP. For this reason, a permitted use has been added with rules to ensure compliance with ASP use placement. At the time of development permit, one of these uses (residential or tourist home) must be selected as a combination of both uses will not be permitted within a single building.

The maximum UPH in both proposed standard districts (TSMV R3 and TSMV R4) will be determined using a density modifier the number and will be expressed as a number on the land use bylaw maps i.e. TSMV R3 d50 b74. The number adjacent to the "d" represents the maximum units per hectare without bonusing and the number adjacent to the "b" represents the maximum number of units allowed with bonusing. Both "d" and "b" numbers correspond to the 3,000 base unit maximum and 5000 unit bonusing maximum prescribed in ASP Policy. This will allow the TSMV district to be applied in several TSMV ASP areas as a standard district.

The Public District (PD) and the Environmental District (ED) is proposed as appropriate for parcels intended for Municipal Reserve (MR), Public Utility Lots (PUL) or Environmental Reserve (ER).

### **Bonusing Overlay**

This application proposes a bonusing overlay to incentivize development, encouraging further contributions towards sustainability, social equity, public art, and cultural facilities. The Density Bonusing Overlay recognizes Canmore's priority for affordable housing and their desire to support inclusive communities, protect the environment and value art, culture and community connection. These goals are reflected in how the Density Bonusing Overlay is proposed with the first priority being the creation of additional affordable housing through density bonusing above the 10% required affordable housing. According to ASP policy 5.8.5, the Bonusing Overlay shall be implemented within Land Use Bylaw at Conceptual Scheme stage. The Bonusing Overlay proposes a bonusing framework that uses Units per Hectare (UPH) as opposed to the Floor Area Ratio (FAR) method contemplated in the ASP. The ASP recognizes that superior methods of implementation could be developed after ASP adoption and policy 5.8.6 allows for changes to bonusing to be adopted through the Land Use Bylaw without an amendment to the ASP.

FAR, as a measure for bonusing, would result in an unpredictable number of units. This is because FAR can be divided in multiple ways. For instance, a FAR of 1.0 on a 10,000 m<sup>2</sup> lot could produce 10 units that are 1000 m<sup>2</sup> each or 100 units of 100m<sup>2</sup> each. As FAR does not produce a predictable number of units it is incompatible with the level of certainty both TSMV and the Town needs to ensure that the development does not exceed the units allocated to the development. Further, due to the uncertainty on unit size, FAR does not easily predict and measure cost and commensurable benefit by earning additional units. By using UPH, we can draw from existing costs and known benefits to better predict and measure outcomes. Through ASP policy, development is capped at 3,000 residential units. Given, the known base density of 3,000 and the limited bonusing of up to 2,000 units UPH provides a more predictable and measurable way forward.

The Bonusing Overlay allows bonusing to occur up to the maximum of 5,000 residential units in alignment with ASP policy 5.2.2. Residential units for the purpose of the ASP refers to all types of housing units including market units, tourist homes, affordable housing, employee housing and common amenity housing. For the purpose of working within the maximum unit count, the proposed land use districts and Overlays counts all types of units, and caps development per ASP policy at 3,000 units and 5,000 units with bonusing.



The proposed Bonusing Overlay focuses on incentivizing three areas emphasized by the ASP and include:



**Affordable Housing**

Increasing provision of affordable housing (PAH / Vital Homes) within Canmore through incentivization



**Building Efficiency and Carbon Reduction**

Working toward Net Zero in advance of the Alberta Building Code requirements through incentivization and integration of sustainable development practices



**Socio-Cultural Amenities**

Providing opportunities for builders and developers to contribute to the arts and cultural fabric of the community

To align with Council’s strategic priorities, the Bonusing Overlay provides increased incentivization or prioritization of affordable housing beyond the required 10%. This is accomplished by increasing the amount of market units a developer can build in exchange for one additional affordable housing unit. Canmore’s current bonus incentives (Section 12.2.0.9 of the Canmore Land Use Bylaw) allow for three bonus units for the provision of each unit of affordable housing. There has been little uptake of this incentive; likely caused by there being insufficient benefit for developers to partake in the initiative.

An accompanying Bonusing Calculator provides a consistent tool for the use of the Town and builders to effectively evaluate how a developer submitting a development permit application can voluntarily participate in the Bonusing Overlay. Further explanation regarding each bonusing criteria and the maximum allowable incentive (bonus units) a builder can receive for each criterion is detailed below and within the Bonusing Overlay.

**How does the Bonus Overlay Function?**

The Three Sisters Village ASP sets a residential unit base of 3,000 units and distributes that density over the entire ASP area by parcel. Allowable units per parcel are calculated using UPH based on the land use district. Building above that density is not permitted unless the builder participates in the bonusing program as set out in the proposed Density Bonusing Overlay. The maximum bonusing potential is restricted to 5,000 units by the ASP and proposed Land Use Bylaw districts to comply with the NRCB decision. A developer must exceed the minimum standards set out in the proposed standard district of the Land Use Bylaw by providing the baseline of 10% of all units as affordable housing and meet the current standards of the building code

(Tier 1) before any bonus units can be earned.

To earn the maximum possible bonus, a developer must build more sustainably than Tier 1 of the Step Code, incorporate community, cultural and educational amenities, or increase the number of affordable units offered within the development. The developer may use a combination of the bonus incentives outlined within the Bonusing Overlay to achieve additional market units. A builder will not be able to exceed the maximum UPH in the Bonusing Overlay. Units Per Hectare (UPH) (rather than FAR) are proposed as a measure to ensure that the limits of the ASP are adhered to with regards to the base level of development and maximum with bonusing.

A builder, during their development permit submission would work with the Town to determine their bonusing application using the Bonusing Calculator provided for this purpose. If the number of units proposed is higher than what is allowed by the base land use district (R3 TSMV or R4 TSMV), then the builder will have to demonstrate what bonusing opportunities they will participate in.

The Bonusing Calculator assumes a cost for the provision of each incentive and uses a measure of approximately \$15,000 in increased construction costs to the project to earn one (1) bonus unit. Assumptions on costs used within the calculator are based upon a BILD Alberta Report and existing construction costs within Canmore today.

Three Sisters Village has a maximum potential of 2000 bonus units distributed across the five ASP phases. The amount of units per parcel is also limited by housing typology. Three Sisters Village can achieve bonus units through stacked townhome development and apartment units, however both typologies cannot exceed the maximum allowable storeys per area as defined in the ASP. In addition, apartment



buildings are restricted within the ASP area to R4 TSMV Residential Medium Density District parcels. These factors lead to a reduction in parcel capacity and efficiency which needs to be balanced against costs to encourage developers to utilize the bonusing program.

Each time a new Conceptual Scheme and land use amendment is proposed for each Phase of the Plan Area, the Bonusing Overlay will be reviewed per policy 5.8.11 of the ASP and amended as needed. If a builder can achieve a higher efficiency of construction at a cost less than what is assumed in the Bonusing Calculator, the benefit is that Canmore can achieve its overall strategic objective faster.

TSMVPL is aware that if the Bonusing Calculator is set too high, it will discourage participation in the bonus program and Council's strategic priorities may not be achieved. Set too low and it will be far too easy to earn bonus units. Any additional cost to build bonusing units would be absorbed by the market units causing further divergence in affordability.

The proposed rate achieves a balance between objectives of seeing Council priorities achieved and viability of the bonusing program.

As this is the first phase and we are testing how this Bonusing Overlay functions, it would be appropriate to review and evaluate at the time the next residential conceptual scheme and land use redesignation is proposed in Three Sisters Village. At that time, TSMV and the Town would work together to determine if further changes are necessary.

For affordable housing, given the Council strategic priority, the cost of earning this incentive is a better balance than current regulation and can help to prioritize increased affordability within the project. Socio-cultural incentives function off the same essential formula, an investment of \$15,000 of value earns the developer one (1) unit of bonusing. A change to C.C.H policy to recognize land cost in the cost repayment formula would equalize the relative cost/benefit of each of these bonus opportunities.



## Types of Incentives

### Affordable Housing Incentives

The ASP directs bonus incentives for provision of affordable housing units. According to the ASP, a minimum 10% of units within multi-residential developments shall be provided as affordable housing. This is enabled via the Three Sisters Land Use Overlay and aligns to policy 8.1.2 of the ASP. Developers will receive one (1) bonus market unit for the provision of four (4) required affordable units under the maximum density without bonusing.

To encourage developers to go beyond the 10% minimum requirement for affordable units, the Bonusing Overlay proposes a more significant incentive for the extra affordable units beyond the 10% requirement. This incentive is aligned with the purpose of Canmore's existing Density Bonus in the Land Use Bylaw (Section 12). Since the provision of extra affordable units is optional and voluntary, the Density Bonusing Overlay would apply, and developers would receive five (5) bonus market units for each affordable housing unit provided on or off-site in excess of the 10% requirement.

All residential unit types will be counted towards the overall ASP total. This will ensure development closely aligns with the unit cap policy of 3,000 and 5,000 unit maximum with bonusing in the ASP policy. It is likely that the maximum allowable units per phase will not be achieved and that there will be additional unit capacity left undeveloped in each phase. At each phase, the Land Use Bylaw and Bonusing Calculator may be amended to ensure that the 3,000 and 5,000 capacity accounts for built units and allows for the maximums to be maintained through each phase. This can be facilitated through amendment to the General Overlay and Density Bonusing Overlay by altering the UPH base and maximums at the time of each CS and land use redesignation application.

The bonus market units gained as the result of providing affordable units are not subject to the general rule of a minimum 10% requirement for affordable housing.

Developers can receive up to 100% of the bonus units through the affordable housing incentive criteria. It will encourage developers to provide more affordable housing options within Canmore. As affordable housing units are meant to be acquired by C.C.H or another non-profit agency, these units will fulfill all of Canmore's criteria with regards to

the C.C.H affordable program (section 8.1 of the ASP).

### Building Efficiency and Carbon Reduction

The Bonusing Overlay also encourages community resilience through the construction of sustainable buildings. Sustainable buildings are designed to have minimal environmental impact, which is intended to assist in mitigating the negative effects of climate change. Such buildings typically use renewable energy sources, efficient heating and cooling systems, and eco-friendly materials, all of which help to reduce carbon emissions and conserve natural resources.

To receive density bonusing for energy efficiency or renewable energy generation, developers must achieve Built Green Gold or Tier 2 of the Alberta Building Code. Tiered codes were introduced in 2020 and will be implemented in January 2024 by the province as a means to eventually achieve Net Zero Housing. Given the development timeline for TSV Phase 1 and the expectation that Tier 1 will be implemented shortly, the Bonusing Overlay stipulates that incentives will not be earned unless Tier 2 is reached. This will ensure that any development in Three Sisters Village will exceed the Building Code requirements as the Tiered Code proceeds. Developers can receive more bonus units by meeting higher tiers. These incentives are intended to encourage developers to build to Net Zero earlier than required by the current Alberta Building Code.

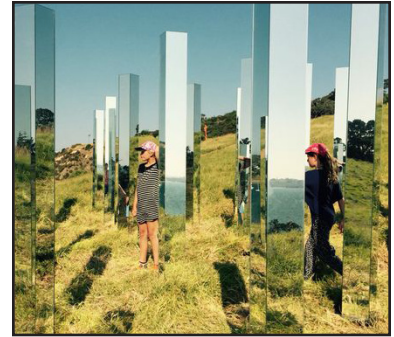
To receive density bonusing through energy generation, developers will receive one (1) bonus unit for every \$15,000 invested in energy generation. Incentives for the inclusion of Electric Vehicle (EV) capable infrastructure or EV chargers into development and Low Impact Development (absorption of rainwater) have also been included and are allocated to yield one (1) bonus unit for \$15,000 of estimated investment. See the proposed Bonusing Overlay for more details.





## Socio-Cultural Incentives

Socio-cultural incentives are included to encourage developers to create a sense of place and community through the inclusion of public art, community amenity and facilities. This could be through: the sponsorship of specific pavilions related to Indigenous culture and Canmore's history; participation through the provision of publicly accessible amenities; public art contribution on site; or by contributing to public art within the plan area to create an arts trail. Within the Three Sisters Village Phase 1 Open Space Plan (**DRAWING 7**), placeholders for public art have been integrated throughout the network of pathways. This will allow public art to be embedded within the Town's community fabric and extend throughout the Village, creating both a sense of place and community benefit. Contributions to the program can work with the Town and TSMV and local or national artists to ensure the art selected contributes meaningfully to the Three Sisters Village. Public art contributions can be in the form of cash contribution or a direct commissioning. Further program details will be solidified at subdivision when a bonusing program that is supported by Council is better understood.



EXAMPLES OF PUBLIC ART

## Bonusing Calculator

As part of the application submission, a Bonusing Calculator (similar to the Sustainability Screening Review tool) is provided. The Bonusing Calculator is envisioned as a convenient tool that will help applicants decide their contribution towards bonusing criteria and for the Town to consistently evaluate the bonusing amongst a variety of applications.

The Bonusing Calculator will outline and facilitate the following:

- The maximum number of units that can be built on the site without bonusing;
- The number of affordable housing units that must be developed on the subject parcel (a minimum of 10%);
- The maximum number of bonus units that can be earned, based on the cap of 5,000 units;

- Allow the builder to test various scenarios by having the applicable bonus calculated based on inputs;
- The maximum number of market units that can be built on site after bonusing inputs have been inputted; and
- The total number of units that can be built on the site (market and affordable) after bonusing has been inputted.

This tool will provide a convenient way in which the Town and applicants will be able to determine the outcomes of the bonusing program.



### **Proposed Lot Layout**

The proposed lot layout identifies thirteen (13) multi-residential parcels. The parcels identified are in alignment with the development areas and policy outlined within the ASP. A complete street network, also in alignment with ASP maps and policy, provides access and egress to parcels and is defined by a Collector Road, which will also serve as the east entrance road to the Phase 1 area. All other streets are identified as Local Roads.

Parcel sizes have been proposed based on the topography and designed to ensure a variety of builders will be attracted to build within the development. Open spaces in the form of public parks and environmental reserve provide connections around and through the Phase. Open space connections are part of a larger network of parks and open space that will be added to over time as the resort builds out. In each Phase, MR and ER dedications will be proposed and over time as phases develop the cumulative MR contributions will form a larger park within the eastern portion of the Plan Area. The area that is not designated a public park will be considered Resort Recreation Amenity Area per policy 6.4.1 in the ASP. This is discussed further in the Parkland and Recreational Facilities section of this document.

ASP policy, land use designation and phase design encourage comprehensively planned development which will blend with adjacent parcels and the open space network to create a compact, community focused design.

### **Street Names**

Street names will be proposed and formalized at the time of subdivision application. Several themes for naming of streets and parks and open spaces can be incorporated at future stages including the incorporation of indigenous heritage, Canmore heritage, and arts and cultural themes. Street names could combine all of the above or just one of the themes.



## Development Details

### Green Development

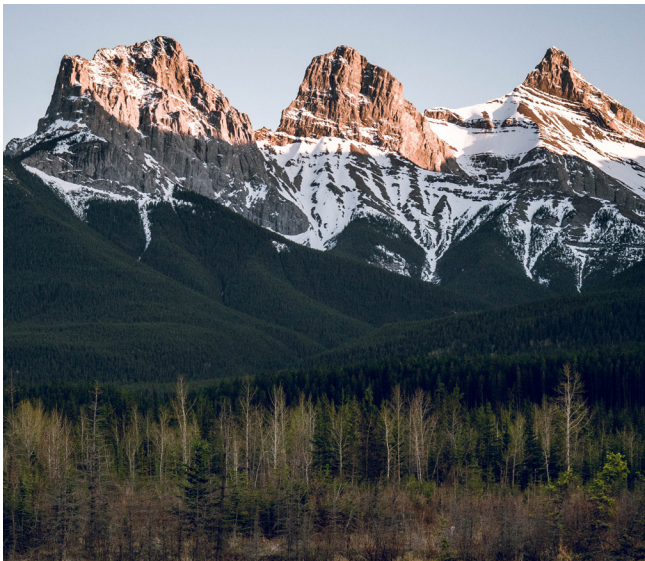
Three Sisters Village has been designed with preservation and sustainability in mind. By focusing on a compact built form with mixed uses, we are reducing the cost of servicing infrastructure and reliance on private vehicles. As discussed under the Bonusing Overlay section, TSV also incentivizes green development by allowing developers to access additional market units through contributions to energy efficiency, energy generation and sustainability.

### Energy Efficiency - National Building Code

Energy efficiency can be achieved through various construction innovations and building materials. Insulation, heat recovery and even building design contribute to a more energy efficient building. Energy efficiency beyond the Building Code has been facilitated through encouraging density bonusing.

### Energy Generation

Any additional energy generation which reduces a building's use of carbon-based energy sources is a step towards Net-Zero. As per ASP policy, all residential buildings within Phase 1 should be "solar ready". Energy generation has been facilitated through encouraging density bonusing.



### Electric Vehicles

Advances are being made swiftly in the area of electric vehicles. To encourage developers to incorporate charging units at the construction stage, additional bonus market units can be earned through the provision of EV Capable infrastructure, Level II and Fast EV chargers. There is a significant cost benefit to incorporating charging access at the construction and development stage rather than at a later stage.

### Modal Split

Three Sisters Village has been designed to exceed the 40% modal split targets of the Canmore Integrated Transportation Plan. Encouraging sustainable transportation modes such as transit, cycling and walking. Three Sisters Village exceeds this goal by setting their target for trips conducted via sustainable transportation means at 50%. An extensive path network has been designed throughout the ASP area to facilitate bicycle and pedestrian use. Collector Streets include transit stops, an active transportation zone with one (1) single lane bicycle path in each direction and a 1.8 m pedestrian sidewalk.

### Low Impact Development

Low Impact Development refers to construction techniques/ materials and landscaping which preserves or mimics natural drainage systems. These strategies can be applied on a singular lot/building basis and/or incorporated into comprehensive design. Low Impact Development would be addressed at the development permit stage and has been incentivized by the Bonusing Overlay.



## Urban Design

### Development Form

Three Sisters Village places a focus on human-scaled development inspiring a vibrant, innovative mountain community. Street oriented, compact design will allow residents to live, work and play within their community.

### Community Centres & Nodes

Phase 1 is designed as a residential area of the Three Sisters Village ASP and its form is centered around the open space and recreational amenities. As a primary entrance to the resort centre, steps have been taken to provide a welcoming atmosphere to greet both visitors and residents. A parametric welcoming feature has been conceptualized for the gateway road (Collector A), and community and regional park features have been placed at strategic locations throughout the site. For further detail on the Open Space Concept Plan please see the Parkland and Recreational Facilities section.

### Natural Landscape Retention

One of the highlights of the Three Sisters Village area is the natural splendor which permeates the site. Development and design considerations in the ASP encourage preserving the natural environment, as much as possible, and using design adaptation when required to sensitively incorporate buildings within the site. Phase 1 contains X ha of MR and ER, the majority of which will be maintained in its natural state. Grading at the initial construction stage will only take place where necessary to complete the transportation network and ensure slope stability and drainage. Both the regional and community park within the regional open space have been preliminarily designed to utilize the natural topography and vegetation.

### Seasonal Design Strategies

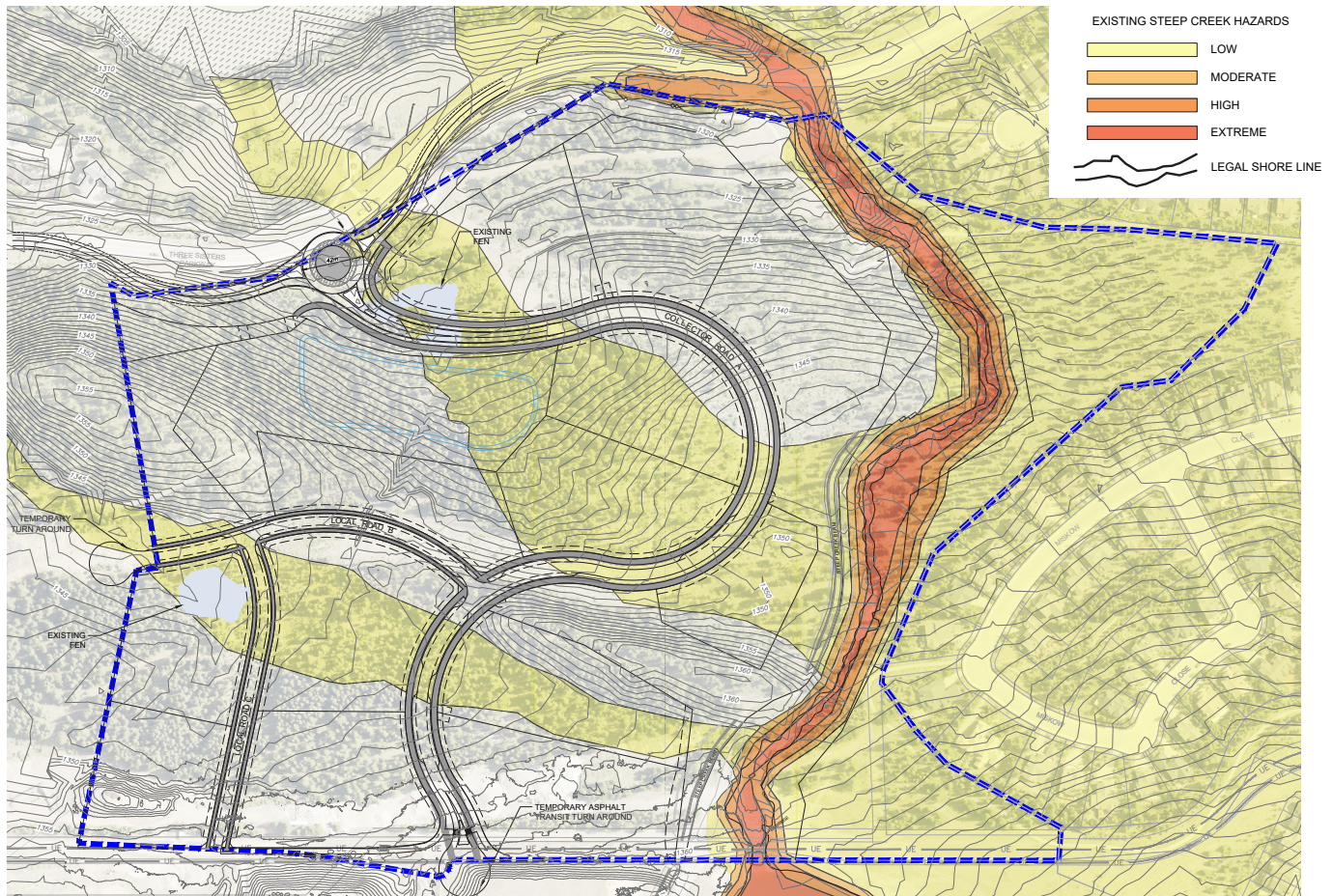
Phase 1's pathway network and stormwater infrastructure has been specifically designed to handle Canmore's seasons. Boulevards within the street network will accommodate snow storage in winter. Multi-use pathways have been designed according to Town standards to ensure they can be adequately maintained. Additionally, TSMVPL has partnered with Canmore & Area Mountain Biking Association (CAMBA) in maintaining the provided mountain bike trails and are collaborating on a fat bike strategy for winter cyclists.

### Transitional Areas & Connectivity

Phase 1 is one part of the Three Sisters Village ASP and has been designed to maximize connectivity through the site and to adjacent development and amenities. The transportation network has been designed to expand to subsequent Conceptual Scheme phases. The Phase 1 Conceptual Scheme fits into the overall ASP open space and trail network. The pathway network connections can be seen in **FIGURE 4** and **DRAWING 7** and further detail has been provided in the Parkland and Recreational Facilities section regarding the significant MR allocation which is the beginnings of a regional open space.



## Environment and Ecology



DWG 5. ENVIRONMENTAL FEATURES AND HAZARDS

### Biophysical Analysis

The Three Sisters Village EIS submitted within the ASP was a detailed assessment of the area and what is required to mitigate the project. In accordance with Table 49 of the Three Sisters Village EIS, further analysis was completed on the vegetation and wildlife within the Phase 1 area.

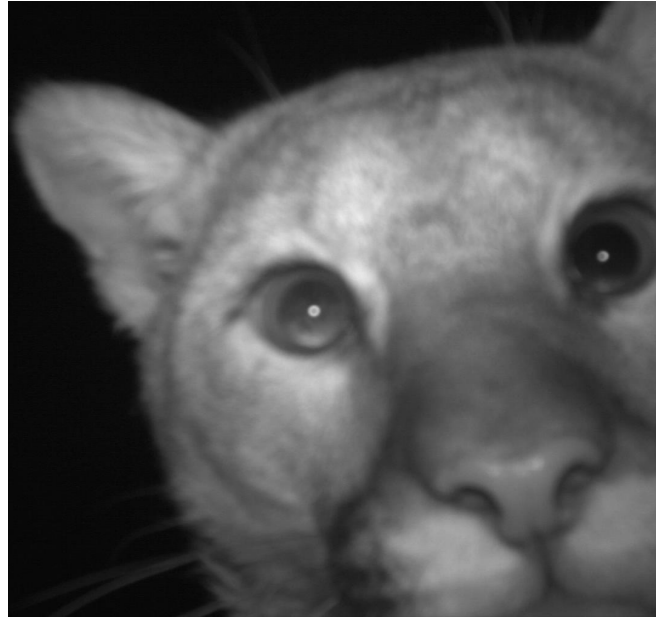
#### Vegetation

A desktop review and vegetation and wetland reconnaissance field surveys were completed to assess historical and existing native vegetation and wetlands within the Study Area. Results include four noxious weed species occurrences, one recorded rare plant, yellow sedge, and no historical rare plant. No occurrences of ecological communities of conservation concern or species of conservation concern were found. Wetlands in the Study Area comprise 0.4% (0.3 ha) and uplands, primarily pine dominated forest (d2), comprise 71.6% (51.7 ha). Mitigation measures during construction are recommended. Water Act applications for the removal of the temporary shrubby swamp and seasonal graminoid marsh in the Study Area are required. These will be completed at the subdivision application stage.





*Three Sisters Mountain Village Wildlife Camera  
- Grey Wolf, Winter*



*Three Sisters Mountain Village Wildlife Camera  
- Cougar*

### **Wildlife**

Wildlife habitat reconnaissance surveys were completed at 12 survey target locations within the Study Area with meandering transects gathering data on incidental species of management concern (SOMC) and wildlife features within the Study Area. Results indicate suitable habitat exists for SOMC in the study area, including a high potential for western toad and long-toed salamander breeding in one wetland. Amphibian visual encounter surveys were completed twice at five target locations. One species of amphibian was identified: long-toed salamander. Additional surveys are recommended prior to beginning construction, including a wildlife sweep. It is recommended that construction activities avoid restricted activity periods when possible and that appropriate mitigation be implemented when required.

These recommendations are in alignment with the EIS. The recommendations within these two additional reports, will be appropriately integrated into a future subdivision approval and considered as a part of the construction and design development process.



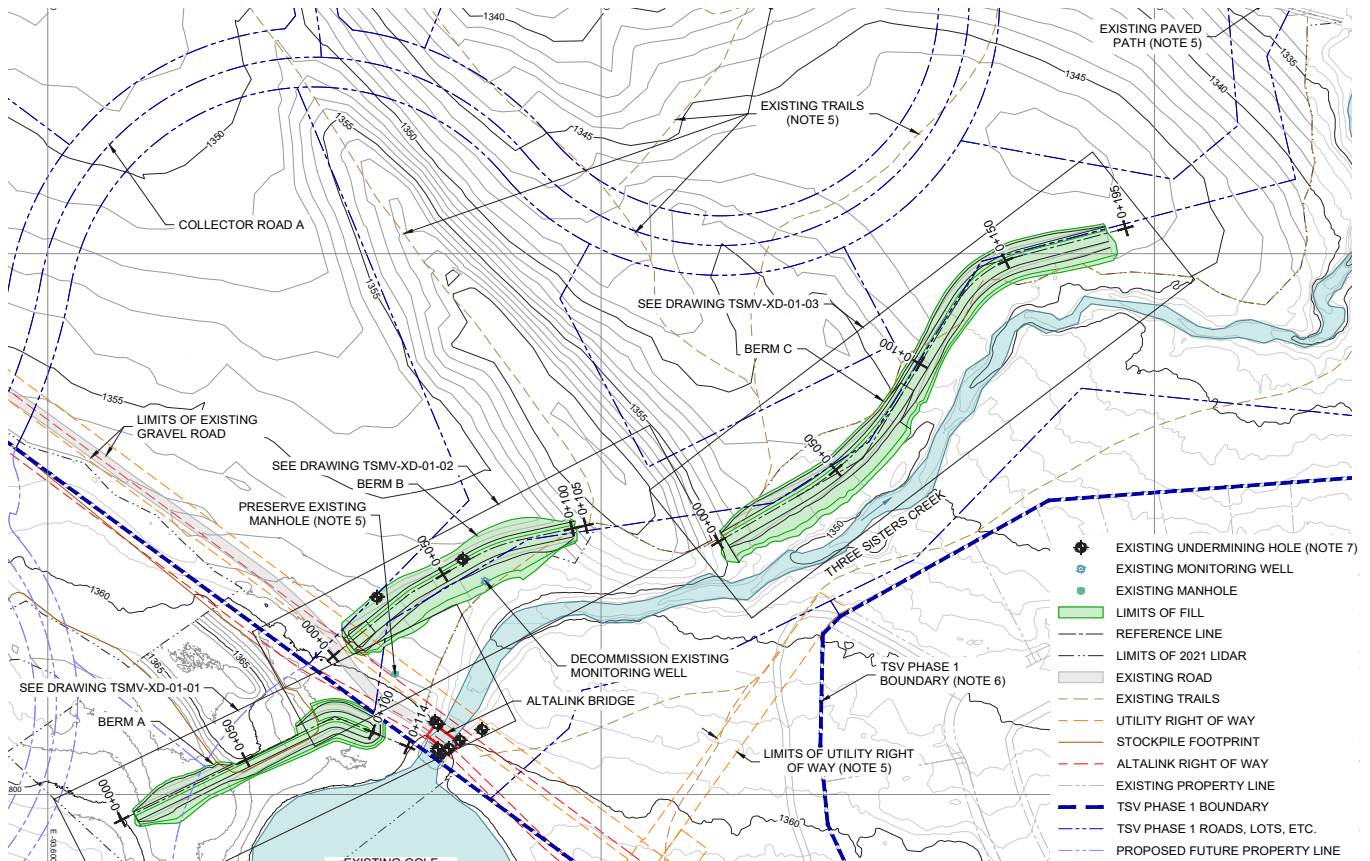
*Three Sisters Mountain Village Wildlife Camera - Black Bear and Cub*



## Steep Creeks

Three Sisters Village includes Three Sisters Creek, which is prone to steep creek hazards including clear water floods and debris flood. Flood risk is illustrated in **DRAWING 5**. The hazard is characterized in a report titled Three Sisters Creek Hazard Assessment Update. Mitigation to control the hazard within the proposed development is described in three reports including: the Three Sisters Creek Option Analysis, the Three Sisters Creek Preliminary Mitigation Design and

the Three Sisters Creek Phase 1 Detailed Mitigation Design reports. For Phase 1 of the ASP, the mitigation comprises a series of berms to be built along the west edge of Three Sisters Creek (DRAWING 6). For future phases of the ASP, berms are to be built along both sides of Three Sisters Creek to mitigate the hazard both for proposed and existing development under a cost sharing agreement between the Town of Canmore and TSMVPL.



**DWG 6. THREE SISTERS CREEK PHASE 1 DETAILED DEBRIS FLOOD MITIGATION DESIGN**

Per ASP policy, these berms form the designation of ER on the west side of the creek and generally, a 20 m buffer from Three Sisters Creek forms the eastern boundary due to this area being a designated area affected by floods. For steep creek considerations within the design and installation of the wildlife fence please refer to the wildlife exclusion fence design plan, the BGC wildlife fence commentary technical memorandum, and the provincial letter regarding Steep Creek fencing design, dated February 13, 2024, for the wildlife fence design over the creek.



## Undermining

To address the potential risks associated with development in proximity to the historic surface and underground coal mining in and around Canmore, the Province of Alberta had previously approved Alberta Regulation 114/97, the Canmore Undermining Review Regulation. As part of the review of the MGA undertaken by the Province of Alberta between 2012 and 2020, TSMVPL had requested that the 114/97 regulations and associated guidelines be updated to reflect over 20 years of experience accumulated within the regulated area, and to better align the regulations with the stages of planning approvals to provide improved clarity of process to TSMVPL and the public. QuantumPlace, the Town of Canmore and the Province of Alberta worked together to prepare updated guidelines and regulations, which led to the Province of Alberta publishing the updated Alberta Regulation 34/2020 dated 17 March 2020 (the Regulations) concerning development on undermined lands so that risks from surface and underground mines can properly be considered and mitigated as appropriate. In addition to the Regulations, an updated set of guidelines entitled 2020 Guidelines to Evaluate Proposed Development Over Designated Undermined Lands in the Town of Canmore, Alberta dated 01 April 2020 (the Guidelines) was prepared that reflected current practice and modern risk considerations; the Guidelines were also approved by the Province for use.

A Subdivision Mining Impact Report has been prepared and submitted as part of this Conceptual Scheme and land use application as required by Alberta Regulation 34/2020. The work included assessing the proposed Three Sisters Village Phase 1 for undermining considerations. An area hazard map was prepared for the entire Three Sisters Village (including Phase 1) and is presented in a report by WSP (previously known as Wood Environment & Infrastructure Solutions) titled Area Mining Impact Overview Report Resort Village Area Structure Plan\_Rev2, dated 16 November 2020.

The present work included reviewing the following: LiDAR, orthophotos, Canmore coal mine plans and boreholes in the Alberta Energy Regulator database, Canmore bedrock geology maps, proposed land uses and conceptual roads for Three Sisters Village, boreholes, downhole camera videos, multiple site walkthroughs, and existing undermining and

inspection reports for the area written by WSP and others spanning back for over 20 years. Using various sources of data, a three-dimensional model for the underground mine workings was created. This model allowed for specific sections to be cut through areas of interest. A total of two mines partially underly Three Sisters Village, Phase 1: Morris No. 1 mine and Stewart No. 2 mine.

Recommendations are provided to mitigate against undermining hazards in the following areas: the four portal locations, four gravel subcrop locations, areas where workings are shallower than 20 m deep, and the four depillared panels. These recommendations must be confirmed in subsequent and more detailed reports required at the subdivision and development permit stages. Subsequent investigation either prior to or as part of the mitigation process may provide opportunity to adjust the recommended mitigation areas. Areas not impacted by undermining include parcels 27, 28a, 28b, 30a, 30b and 31, as well as the stormwater facility and nature play park and tree fort park spaces.





## Wildfire Risk Assessment

A Wildfire Risk Assessment for TSMV was submitted with the application for the Three Sisters Village and Smith Creek ASPs. The Wildfire Risk Assessment was not a required report but was provided at the time of the ASP application given the level of public interest in wildfire risk. The Wildfire Risk Assessment was prepared for the proposed Three Sisters Village and Smith Creek Plan Areas. The report provides conceptual FireSmart hazard mitigation recommendations to reduce the threat of wildfire to the proposed developments with an emphasis on the vegetation management requirements within and adjacent to wildlife corridors.

ASP policy directs that mass stripping and grading programs should not occur within the ASP area and grading should be limited to only what is needed to install the required infrastructure to service the subdivision (MR, parks, roads and infrastructure like stormwater management facilities). Parcels will be left vegetated until a development permit is approved by the Town of Canmore and wildfire risk is addressed in accordance with the Wildfire Risk Assessment and Town of Canmore Land Use Bylaw. Open space will be mitigated, deadfall cleared and trees thinned, in accordance with a more detailed report submitted at the time of subdivision.



## Cultural & Heritage Resources

### Historical Resources Impact Assessment

A Historical Resources Act approval has been granted for the development area within Three Sisters Village (HRIA Report & Approval documentation). There are no further Historical Resources Act requirements for this site relative to the current project.

### Traditional Ecological Knowledge Assessment: Desktop Study

A TEK desktop study is submitted in support of this application. While a TEK is not a requirement of the Conceptual Scheme submission, the Corporate Directive does state that it could become a requirement. At the time of a pre-application meeting the Town of Canmore determined it was not a requirement. TSMVPL opted to submit a TEK: Desktop Study.

To date, SNN have not meaningfully participated in any engagement proposals made by TSMVPL to discuss the Project, outside of government required consultation needed for the land exchange between the Province of Alberta and TSMVPL. As such, this Assessment relies on research products produced by the SNN and Treaty 7 groups, along

with archival documents, peer reviewed articles, regulatory filings, and published books to meet the requirements of the Corporate Directive. This TEK Assessment involved a comprehensive research process designed to understand and document

Indigenous knowledge systems, practices, and protocols related to the natural environment. The Assessment also involved an exploration of the traditional knowledge held by SNN and Treaty 7 groups to better understand mitigation strategies that can be developed and incorporated into the Three Sisters Village and Smith Creek Phase projects.



Three Sisters Village and Smith Creek Phase 1 propose mitigation measures such as wildlife fencing and education to reduce the Project's impact. Additionally, Land transfers to the Province provide additional Crown lands for the SNN use. Areas of divergence include grizzly bear habitat loss and access to private lands. These divergences primarily arise during the development phase and are the result of a NRCB approved project.

Lastly, areas that could not be classified in this assessment are place names and cultural resource management due to the unavailability of specific inputs from SNN. The EIS documents submitted with the Three Sisters Village and Smith Creek Phase 1 ASPs outlines mitigations appropriate for these impacts and this Assessment recommends its full implementation as the Project proceeds.

Finally, it is recommended that TSMVPL remain diligent in their efforts to develop a relationship with the Stoney Nakoda Nations. Continued efforts should focus on working with the SNN to include place names into future developments, collaborate on cultural resource management strategies, and identify economic opportunities for community members and businesses.

The Town cannot delay decision making due to a requirement to engage with unwilling parties or delay regulatory decisions based on an application-imposed requirement to engage. The MGA sets out the requirements for engagement.

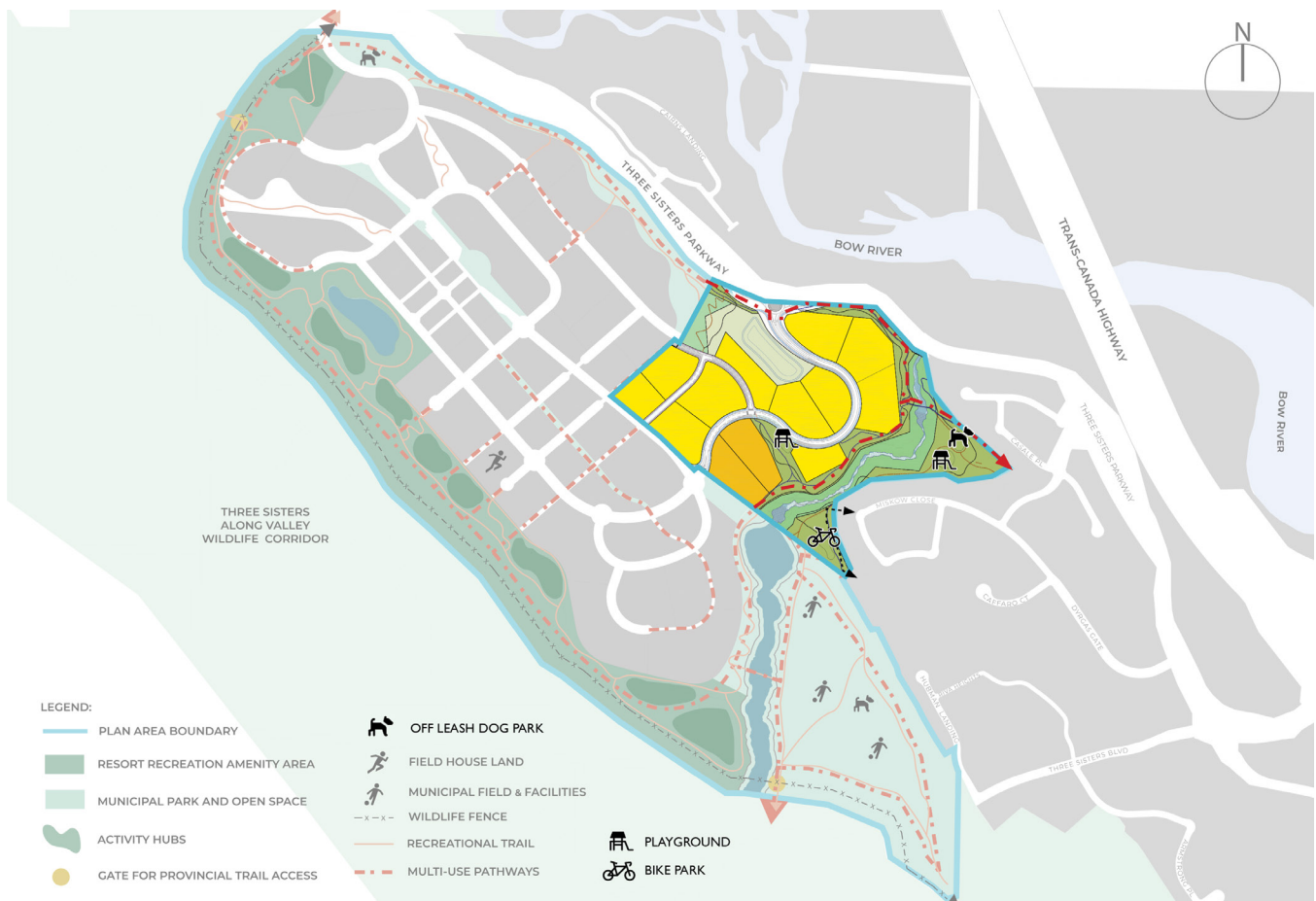
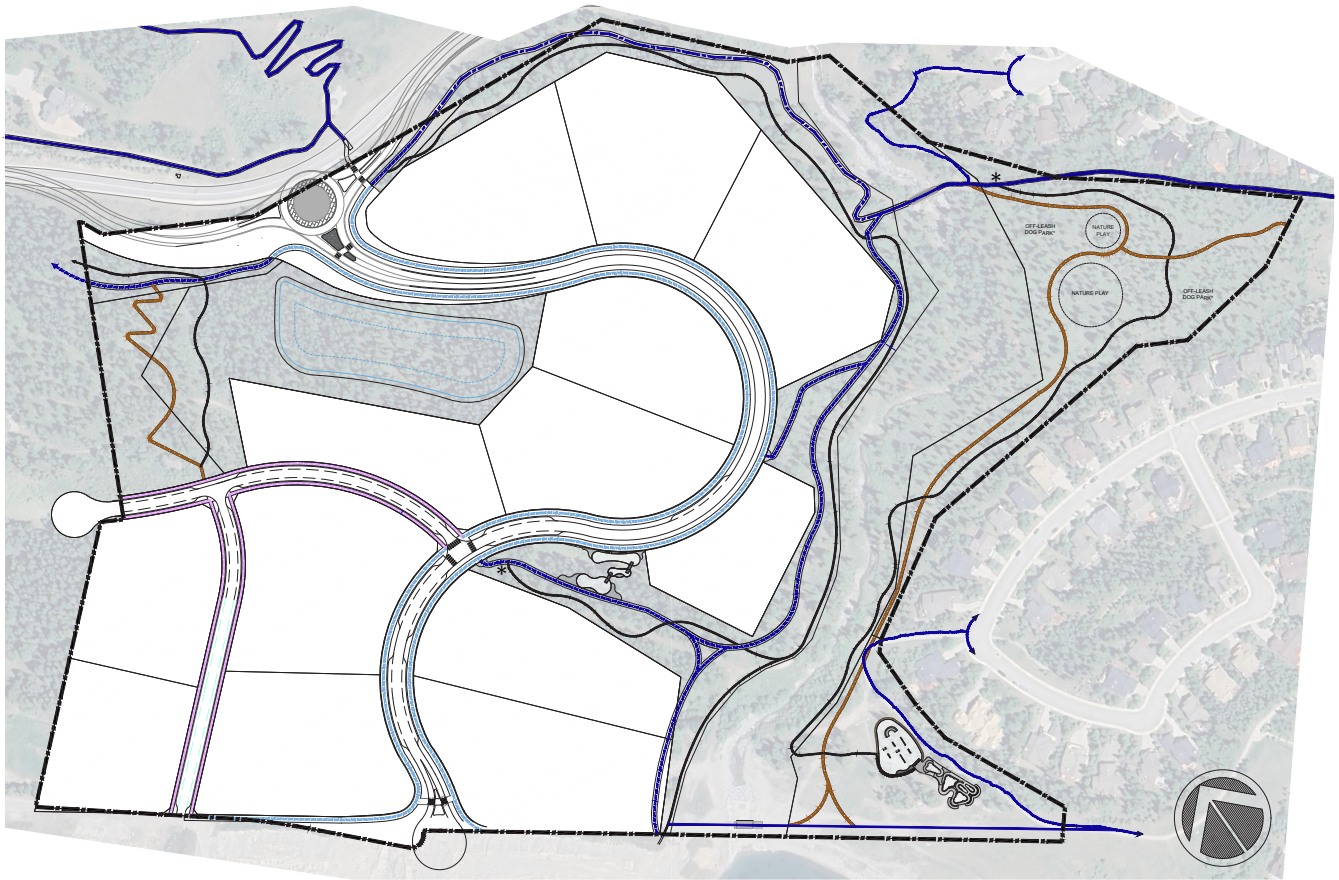


FIG 5. - OVERALL OPEN SPACE PLAN

### Parkland and Recreational Facilities

The Three Sisters Village ASP identifies naturalized open spaces and public recreation facilities that are accessible to residents and guests alike. Existing topography and natural habitat will be utilized in the design of parks and open spaces to minimize impacts of development, to the extent possible, and create spaces that fit into the well-established natural landscape. TSV Phase 1 open space plan expands on the vision of the ASP providing thoughtful, activated community open space which integrates with the surrounding area (FIGURE 5). Pathways and trails link the space to neighbouring amenities such as the Hubman off leash dog park. Playgrounds, public art, regional amenities, extensive trail networks and open spaces provide a wide variety of options for people to linger and enjoy the community (DRAWING 7).

The proposed public amenities will incorporate modern recreational elements while respecting the traditional architecture and natural beauty of the Bow Valley. Sustainable and eco-friendly practices are embraced by using materials and resources that pay homage to the community's identity while reducing impact on the environment. All amenities described within this section of the Conceptual Scheme are conceptual in nature and are intended to provide insight into possible uses for the recreational areas. These designs may be changed, revised or relocated at the discretion of TSMVPL. Detailed design will occur at the time of subdivision.



DWG 7. PHASE 1 OPEN SPACE CONCEPT PLAN

- SCOPE BOUNDARY
- 4.10m ACTIVE TRANSPORT
- 3.5m ASPHALT PATHWAY
- 2.30m SIDE WALK
- 1.5m GRANULAR PATH
- EXISTING ASPHALT (TO BE UPGRADED TO EDCC STANDARDS)
- EXISTING ASPHALT (TO REMAIN)
- 0.3 - 1.0m MTB SINGLE-TRACK
- ART NODE \*

\*NOTE: EXTENT AND SIZE OF THE DOG PARK WILL BE DETERMINED AT THE DETAILED DESIGN STAGE.



## Regional Park | Off Leash Dog Park Concept

As a part of TSMV's commitment to reducing human use within the wildlife corridor, an off-leash dog park is proposed. During engagement Canmorites expressed a desire to have a place where they can walk their dogs off-leash in a forest setting. Envisioned as a space for people to recreate and train their dogs and accommodate dog of various sizes and temperament.



## Regional Park | Nature Play and Tree Fort Concept

As users explore the pathways and trail systems in Three Sisters Village, they will come across a captivating tree fort park. Entering this site will encourage exploration and creative play, through implementing tree fort like play structures, boardwalks and net climbers. This is intended to maintain as much of the existing vegetation in this space to establish the forest setting while providing tree forts to explore. Trails will wind through the space allowing people to experience the elements from every angle, enticing their imagination to take over. With seating provided, families can come and enjoy the natural surroundings this space has to offer.

### LEGEND

- ① Raised tower structure
- ② Connecting platform
- ③ Slide or climbing structure
- ④ Rope structure
- ⑤ Nature play zone
- ⑥ Proposed seating
- ⑦ 1.5m gravel pathway
- ⑧ 3.5m gravel multi-use pathway

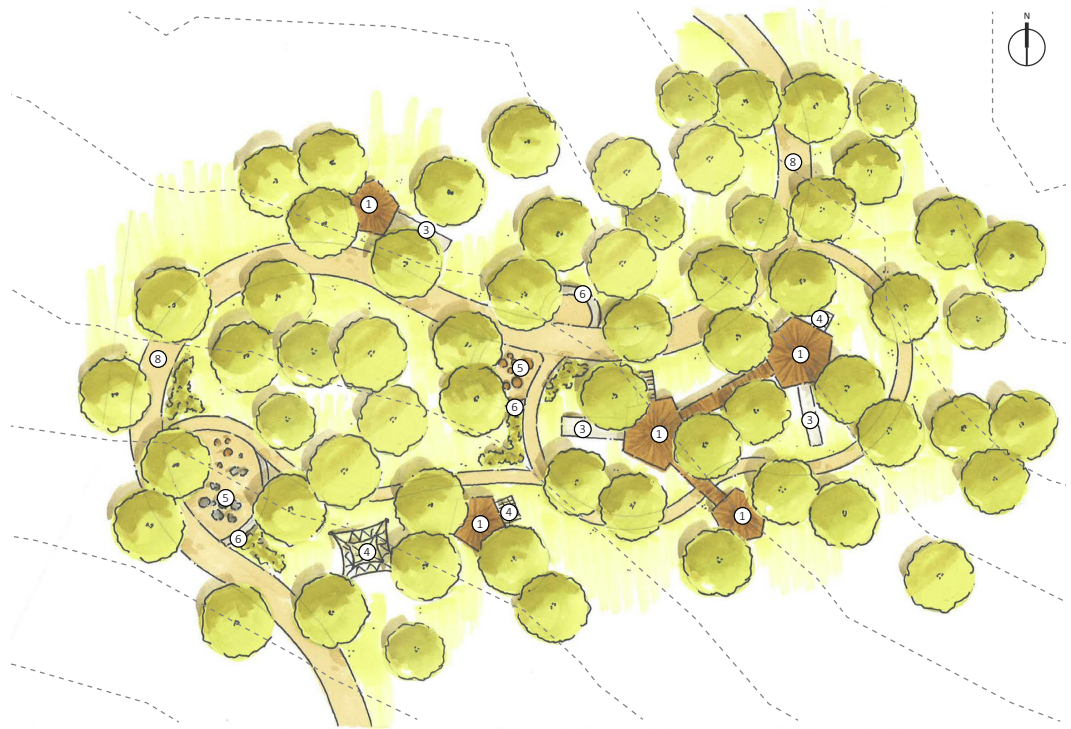
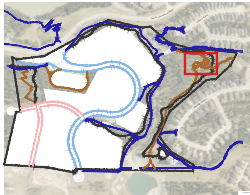


FIG 6. - REGIONAL PARK, NATURE PLAY AND TREE FORT CONCEPT



## Regional Park | Bike Skills and Pump Track Concept

This will be the first amenity of what will become a much larger regional park that will include sports and playing fields as new phases of development move forward. Canmore has a vast mountain biking community which is growing in numbers annually. While pathway and trail connections are provided throughout Three Sisters Village, a bike skills park and pump track have been incorporated to encourage further discovery of user's skills. The bike skills park and pump track will be developed to allow for user groups of varied skill levels to learn and hone their mountain biking abilities. The space will have direct connection to the variety of pathway designations making it a family friendly, accessible space. Although the existing landscape in this location does not have a lot of native forest, topography and landscaping will be re-introduced to create a welcoming space where you feel enveloped within the natural beauty of the area and provide safe sight lines and observation areas while learning bike skills.

### LEGEND

- ① 1.5m gravel bike park access track
- ② Asphalt access ramp
- ③ Proposed seating
- ④ Asphalt pump track
- ⑤ Berm addition
- ⑥ Nesting zone
- ⑦ Start/finish zone
- ⑧ Bike skills park
- ⑨ 3.5m gravel multi-use pathway

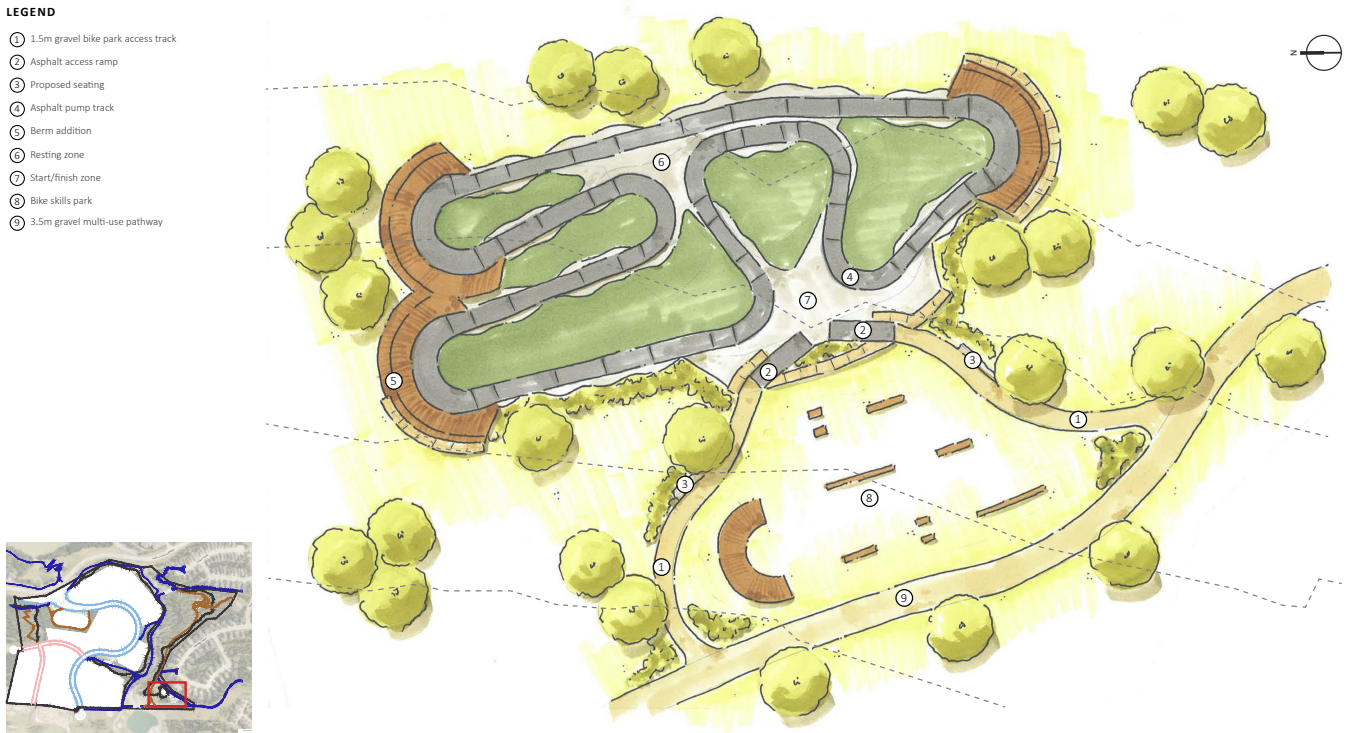
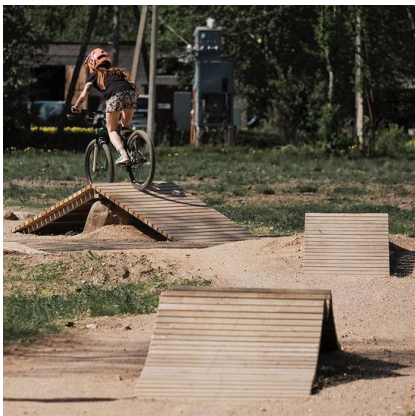


FIG 7. - REGIONAL PARK, BIKE SKILLS AND PUMP TRACK



## Community Park | Welcoming Feature Concept

On entering Three Sisters Village, a sense of identity will be created. TSMV will explore a welcoming feature like a parametric structure utilizing materials complementary to the surroundings. The Welcoming Feature area will provide a gathering space allowing for community events as well as beautiful photo opportunities to guests from afar. This design element is conceptual and if chosen to move forward a location will be selected at the time of subdivision. Pathway connectivity from the Welcoming Feature will encourage residents and guests to further discover Three Sisters Village and the amenities it has to offer.



### LEGEND

- ① Parametric Feature Structure  
(Material to be steel or similar)
- ② Gateway Plaza
- ③ Seating
- ④ Low Amenity Planting
- ⑤ Natural Surface Pathway (Gravel, etc)
- ⑥ Asphalt Multi-use Pathway



FIG 8. - COMMUNITY PARK, ENTRY FEATURE CONCEPT



## Community Park | Playground Concept

Pathway connections ensure that the community playground is easily accessible for residents and guests of Three Sisters Village. This space provides play elements for all ages and abilities while working with the topography. Utilizing the existing grades allows for the introduction of passive climbing as well as distinctive climbing walls which lead to a hill slide connecting the varied elevations of playgrounds. Through the introduction of seating areas, this space encourages families to come and spend quality time in the park allowing children to explore and play while parents take in the natural surroundings.



### LEGEND

- ① Entrance paving
- ② Proposed seating
- ③ Rock wall
- ④ Timber steps/ramp
- ⑤ Indicative play area (Suitable for all ages)
- ⑥ Indicative play area (Suitable for ages 6+ years)
- ⑦ Rock climbing wall
- ⑧ Proposed slide between platforms
- ⑨ Asphalt pathway (R.O.W)
- ⑩ 3.5m gravel multi-use pathway

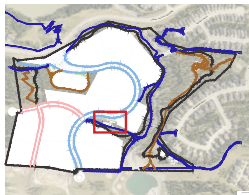
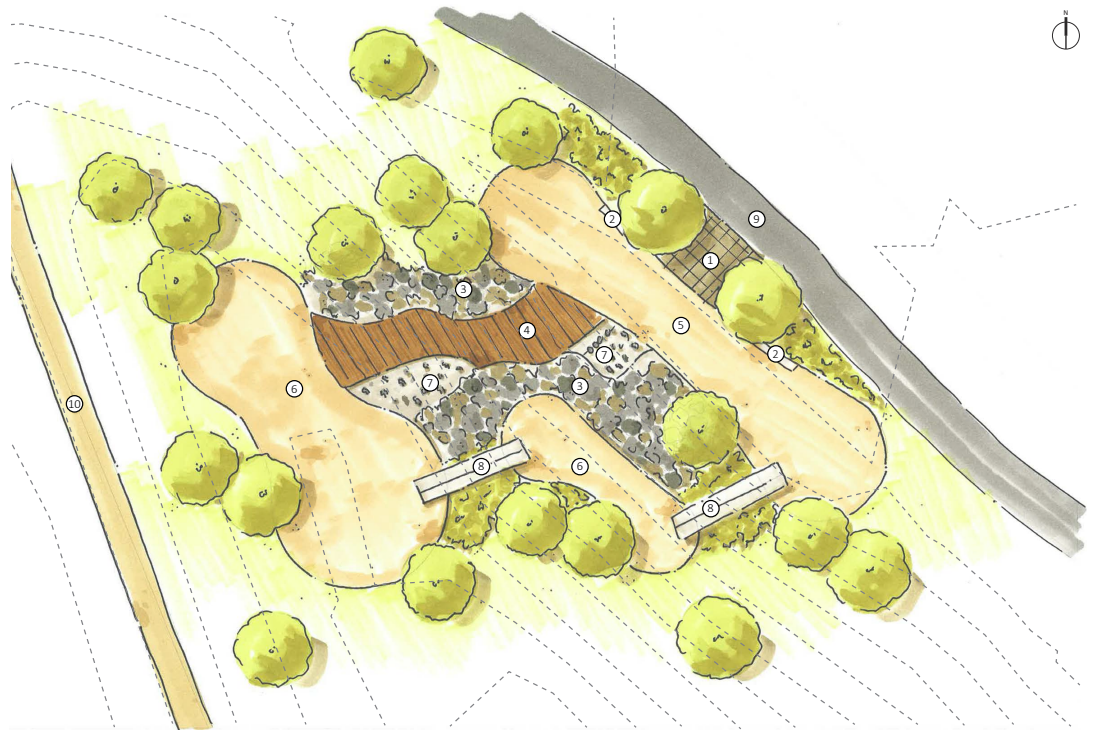


FIG 9. - COMMUNITY PARK, PLAYGROUND





### Pathways, Trails and Art Walk Concept

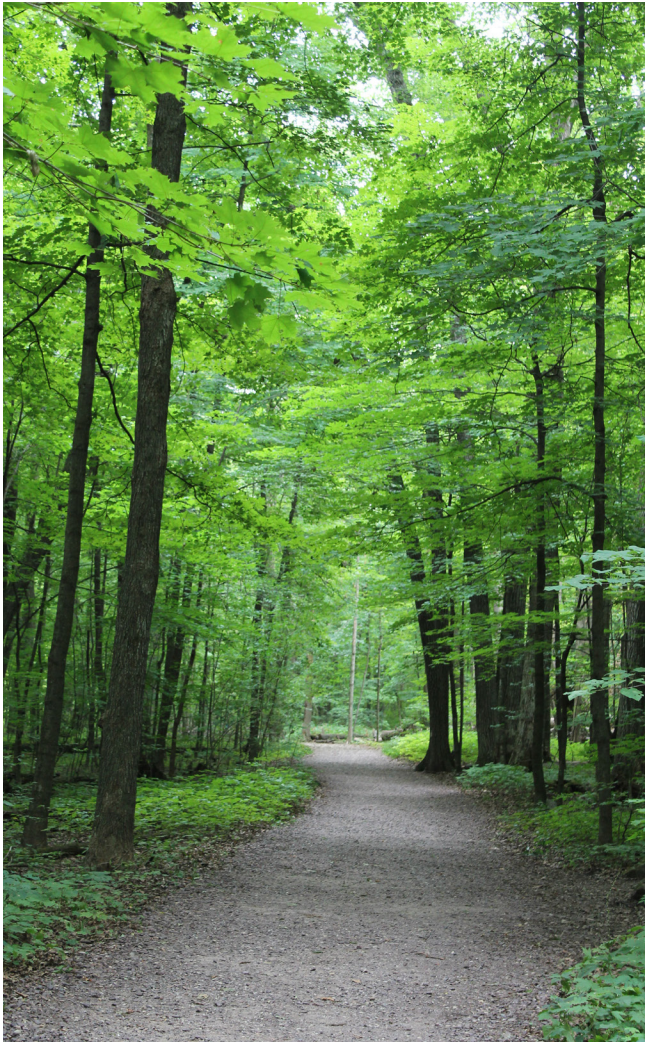
Pathways and trails are a highly desirable recreational feature for Canmore's residents and guests to enjoy. The pathway and trail network will continue to build and connect the Three Sisters Village community with other areas of Canmore and provide connections to provincial trail systems. Trail loops and stacked loops which incorporate the terrain and provide varied lengths and skill levels will allow for multiple user groups to benefit from the amenity.

Trail alignment and design facilitates a hierarchy of trails that accommodate a variety of diverse recreational needs. Trail designations identified in **DRAWING 3, 7, 8** and **11** include:

- 4.1 m multi-use path (asphalt)
- 3.5 m multi-use path (asphalt)
- 1.8 m multi-use path (sidewalk)
- 1.5 m pedestrian path (gravel)
- Bike track (0.3-1.0 m single track)

All trails are consistent with the classification of trails outlined in the Town of Canmore Open Spaces and Trails Plan.

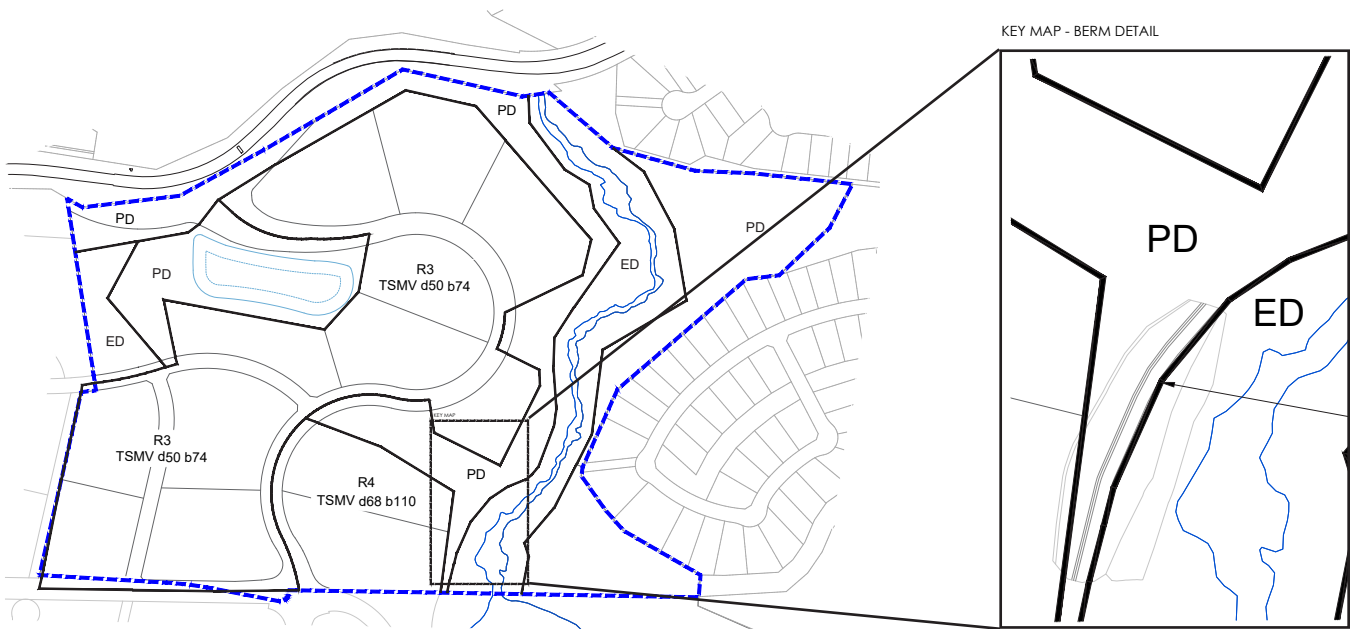
Along selected areas of the trail systems users will experience an art walk with designated installations and seating nodes to enjoy the views and surroundings. Incorporating the art into areas of selected trails encourages users to further explore the site and interact with the spaces.



## Environmental Reserve (ER)

The MGA identifies certain types of land that can be dedicated by a municipality as ER. This type of land is not suitable for development and contains features such as swamps, gullies, ravines, coulees, floodplains or land adjacent to a body of water. ERs are used to preserve natural features of land, prevent pollution, ensure public access and prevent the development of land that is subject to flooding or geotechnically unstable.

The majority of ER designation within the Phase 1 Conceptual Scheme area relates to the floodplain for Three Sisters Creek. Similar to what is depicted in **FIGURE 10** below, the area relating to floodplain is designated as ER within the Conceptual Scheme (**DRAWING 3**) and proposed to be designated as Environmental District (ED) within Canmore's Land Use Bylaw. The area of the floodplain also includes diversion berms to mitigate the hazards associated with steep creek workings. (**DRAWING 4**) Only low maintenance pathways (i.e., gravel surfaced pathways or mountain bike trails) are proposed within the area designated ER with more substantial pathways and recreation offerings occurring within the MR areas. One other area qualifies as ER within this phase due to a knoll landform which contributes to a gully north west of phase 1. All other lands and slopes are considered stable and do not qualify as ER.



DWG 4. PROPOSED LAND USE PLAN

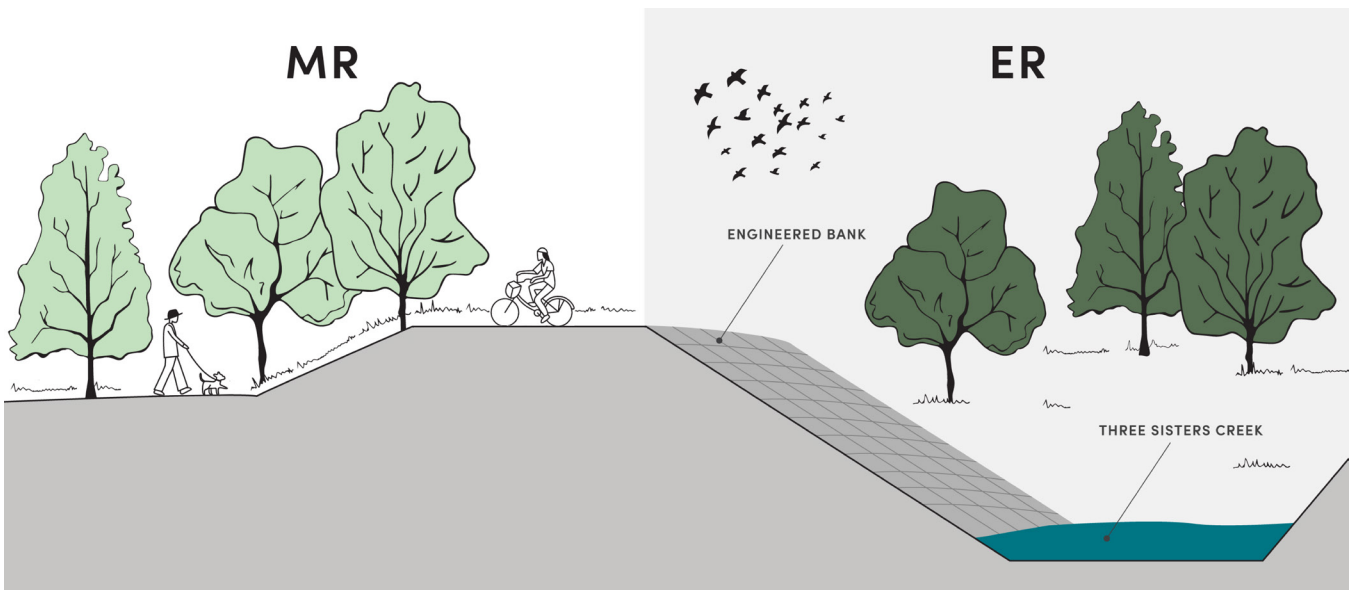


FIG 10. - STEEP CREEK MR ER DESIGNATION INFOGRAPHIC



### Use of Deferred Reserve Caveats

Policy within the Three Sisters Village ASP identifies that Deferred Reserve Caveat (DRC) should be transferred to the Three Sisters Village lands where it will be used to facilitate additional dedication of MR lands within the ASP area (See Policy 6.1.8). The last remaining DRC is registered within the Smith Creek ASP area (DRC 001185759) and has a value of 16.365 ac.

Once the Conceptual Scheme and Land Use Redesignation approval is provided, subdivision will be required to provide MR as per the MGA. This reserve area is indicated within the Conceptual Scheme drawings. The lands to be provided as MR will be aligned to Policy 6.2.5 and 6.2.6 the ASP. Any surplus MR dedication will reduce the amount of lands remaining within the DRC. In future phases when the DRC has been fully addressed (i.e., it no longer exists), surplus MR land dedication can be applied to future phases of development. In Phase 1, 6.53 ha or 16.14 ac are proposed as MR. Given that the required MR dedication for this phase is 2.527 ha (6.245 ac), the excess 9.895 ac will be used to reduce the 16.365 ac DRC 001185759 to 6.47 ac at the time of subdivision.

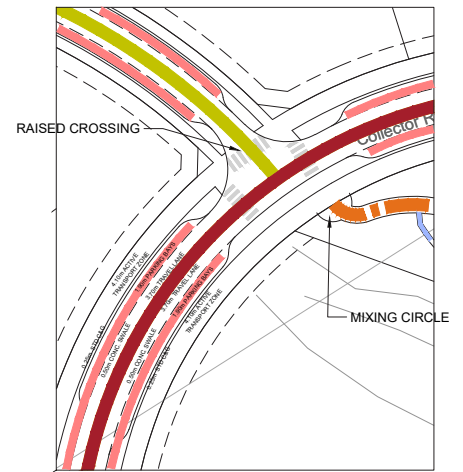


# Transportation & Servicing

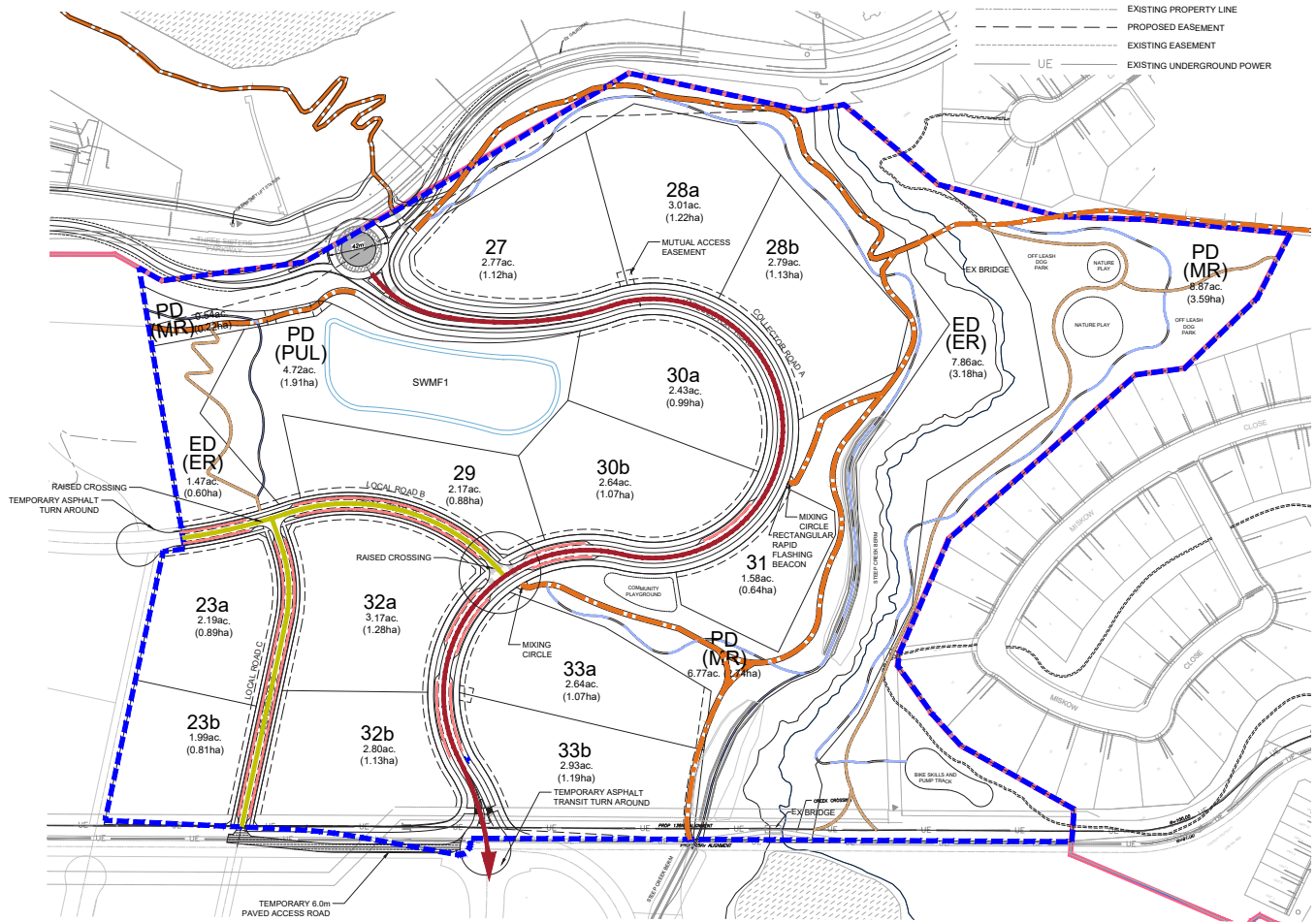
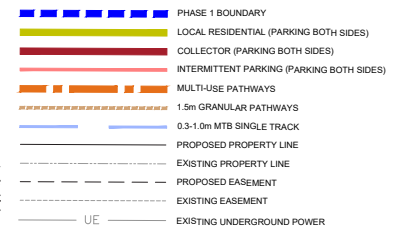
The Global TIA submitted in support of the ASP, provides the framework and direction for the Mobility Assessments that will be completed at each phase of development within Three Sisters Village.

The Mobility Assessment identifies the required roads, active transportation infrastructure, and transit service to achieve the long-term goals of the Three Sisters Mountain Village development. The Mobility Assessment includes the following tasks and outcomes:

- Confirm proposed multi-modal network infrastructure including road network cross sections within Three Sisters Village;
- Identify development and analysis horizons;
- Identify development and background traffic along Three Sisters Parkway within the development horizon;
- Confirm multi-modal trip generation, assignment, and distribution from Phase 1 Three Sisters Village
- Analyze capacity of proposed multi-modal infrastructure within Three Sisters Village at full build of Phase 1 and recommend any changes to proposed infrastructure.



INTERMITTENT PARKING BAYS  
SCALE - 1:500



DWG 8. PHASE 1 STREET NETWORK



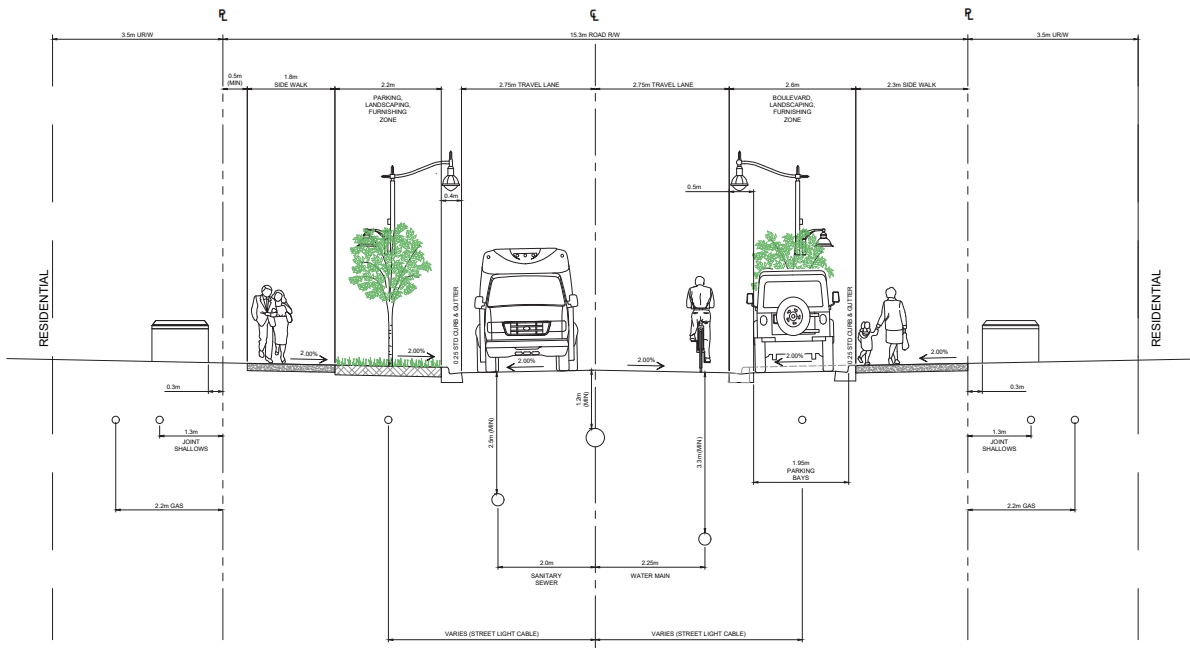
## Transportation

Phase 1 of the Three Sisters Village ASP includes development of a multi-modal transportation network that considers access for users of all ages, abilities and modes. Residents in the early build out of the development will have opportunities to walk, cycle and utilize public transit. This transportation network will connect them to open space, work, shopping and recreation. The street network identified in this Conceptual Scheme aligns with the analysis and recommendations identified through the 2020 Three Sisters Village and Smith Creek ASP processes and the associated 2020 Three Sisters Mountain Village Global Transportation Impact Assessment (2020 Global TIA). The proposed Phase 1 Complete Streets network is shown in **DRAWING 8**. The road network consists of a Collector Road (Collector Road A) which will provide a new access to Three Sisters Parkway. This access will be designed as a single lane roundabout, with pedestrian/cyclist crossings at two of the three legs. No other external network updates are anticipated to be triggered by development of Phase 1 of Three Sisters Village.

## Complete Streets Roads and Cross Sections

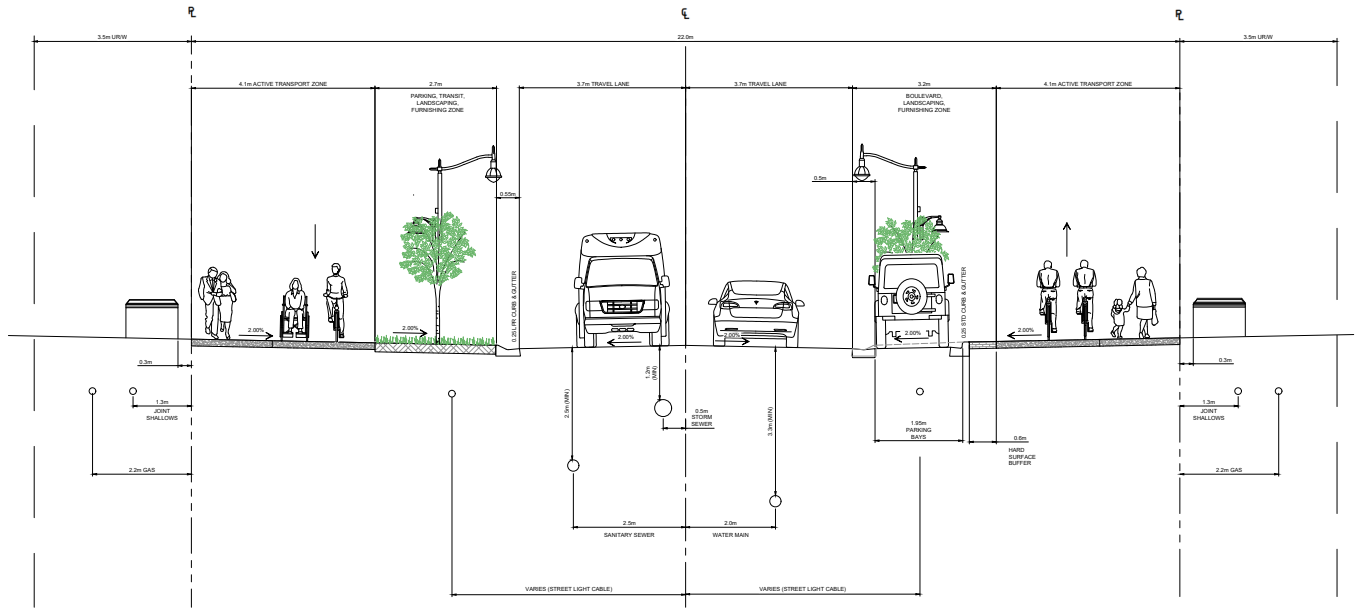
Collector Road A will be designed with a modified collector (UCU 50) Standard Parking Both Side shown in the Collector Cross-Section (**DRAWING 10**). Modifications were required to accommodate goods movements. The modified collector includes a 4.1 m active transportation zone on both sides of the street. This zone includes a single bike lane of 2.3 m, with a 1.8 m pedestrian zone. Bus stops in Phase 1 will be accommodated in the traffic lane. A total of two permanent stops have been identified with an additional temporary stop.

There are two local roads in Phase 1, Local Road B and Local Road C. Each of these roads will be designed with the Local Residential (ULU 50) Parking Both Sides Cross Section (**DRAWING 9**). On-street parking will be included throughout the site and a 2.3 m mono-walk will be located on each side of the streets when next to parking bays. At intersections and across accesses, the cross section will include a boulevard (or crossing) and separated walk. Cyclists can share the right-of-way with the street in these locations or choose an alternative trail route.

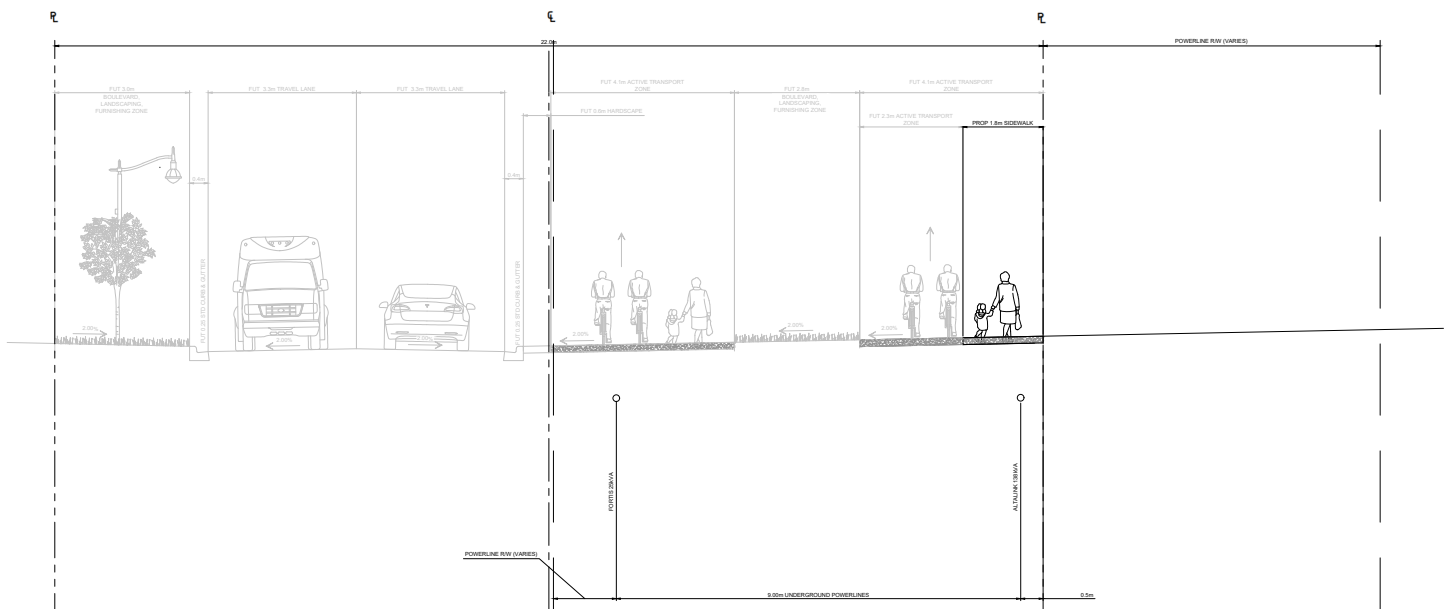


**DWG 9. LOCAL RESIDENTIAL CROSS SECTION**



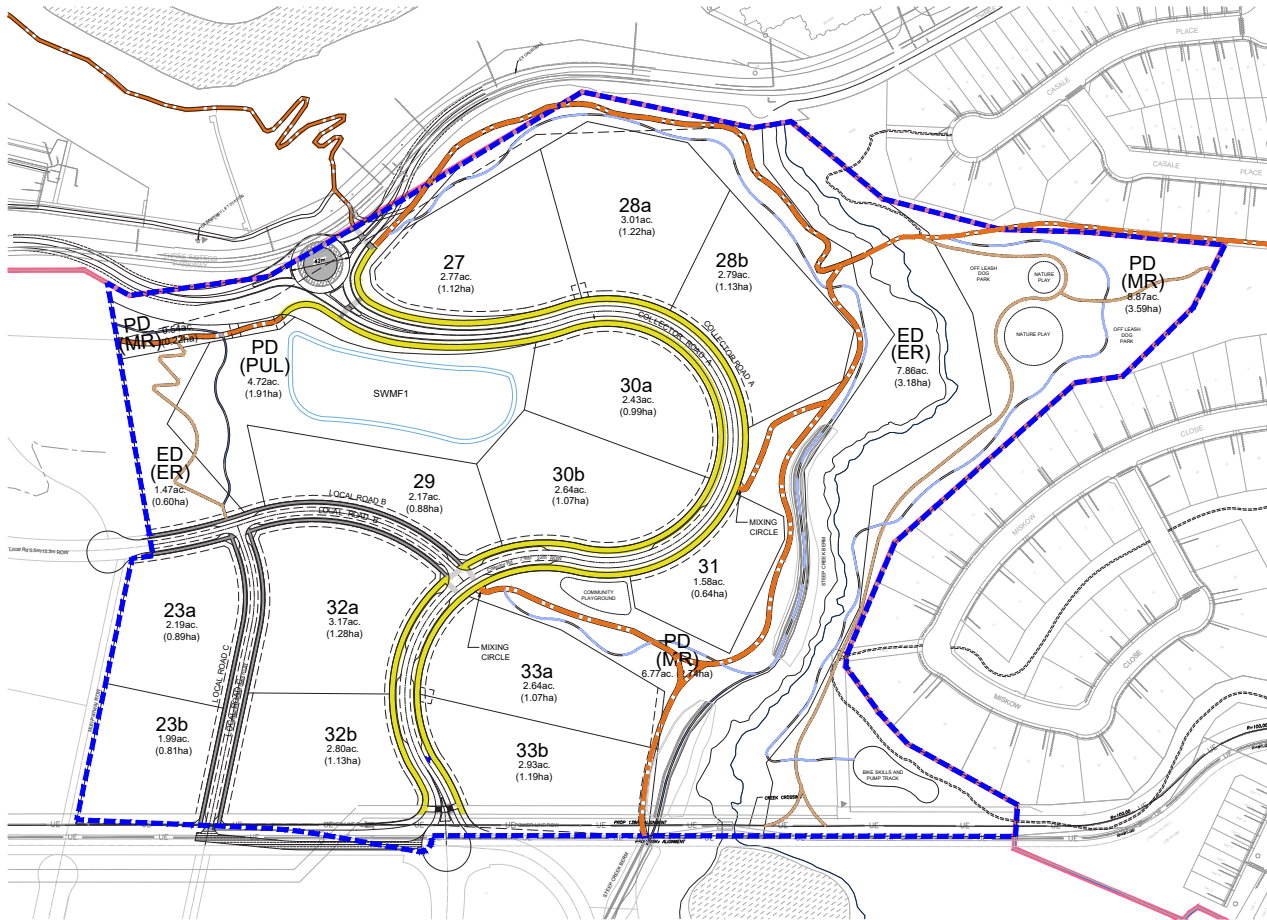


DWG 10. COLLECTOR ROAD A CROSS SECTION



DWG 11. FUTURE COLLECTOR ROAD B CROSS SECTION





- PHASE 1 BOUNDARY
- ACTIVE TRANSPORT ZONE
- SIDEWALKS
- MULTI-USE PATHWAYS
- 1.5m GRANULAR PATHWAYS
- 0.3-1.0m MTB SINGLE TRACK
- PROPOSED PROPERTY LINE
- EXISTING PROPERTY LINE
- PROPOSED EASEMENT
- EXISTING EASEMENT
- UE
- EXISTING UNDERGROUND POWER

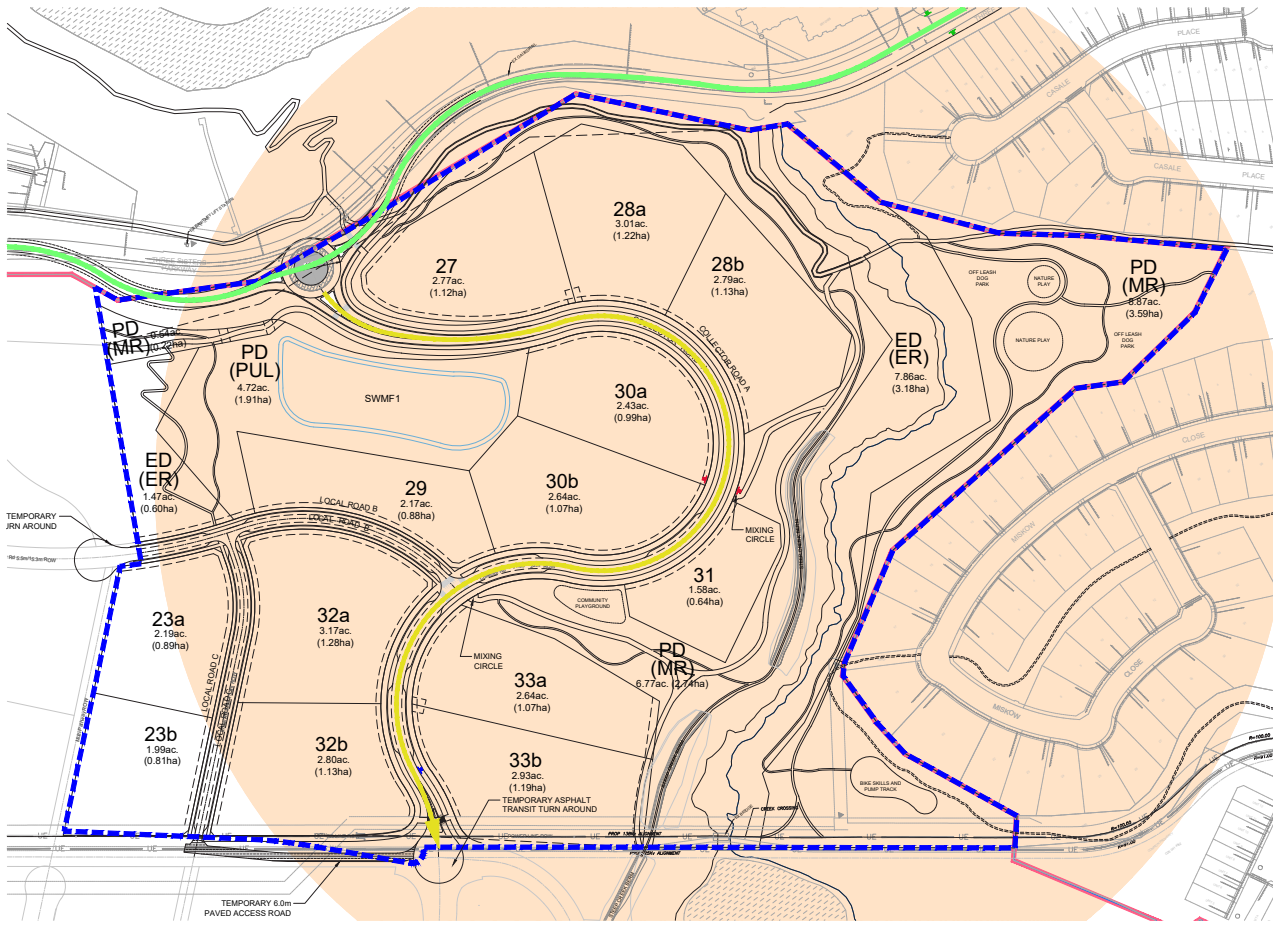
**DWG 12. PEDESTRIAN AND CYCLIST NETWORK**

### Active Modes Network

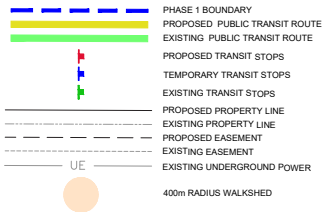
The pedestrian & cycling network is illustrated in **DRAWING 12**.

The accommodation of people walking and cycling is of high importance to the long-term success of the overall development of the TSMV community. Options for people who are living and visiting the area to access services and employment without a private vehicle has been considered at every stage of development. The core network in Phase 1 of the build out is within the road network right of way, with 4.1 m active transportation zones identified along Collector A and on-street shared with vehicles for Local Road B and Local Road C. Trails and pathways also provide options for cyclists and pedestrians to connect into the greater network.





DWG 13. TRANSIT NETWORK



**Transit**

Collector Road A is designed to a standard that can accommodate transit. Transit stops have been located midway up Collector A with a temporary stop to be included near the temporary turn around. The potential transit network is shown in **DRAWING 13**.

**Access and Egress to Lots**

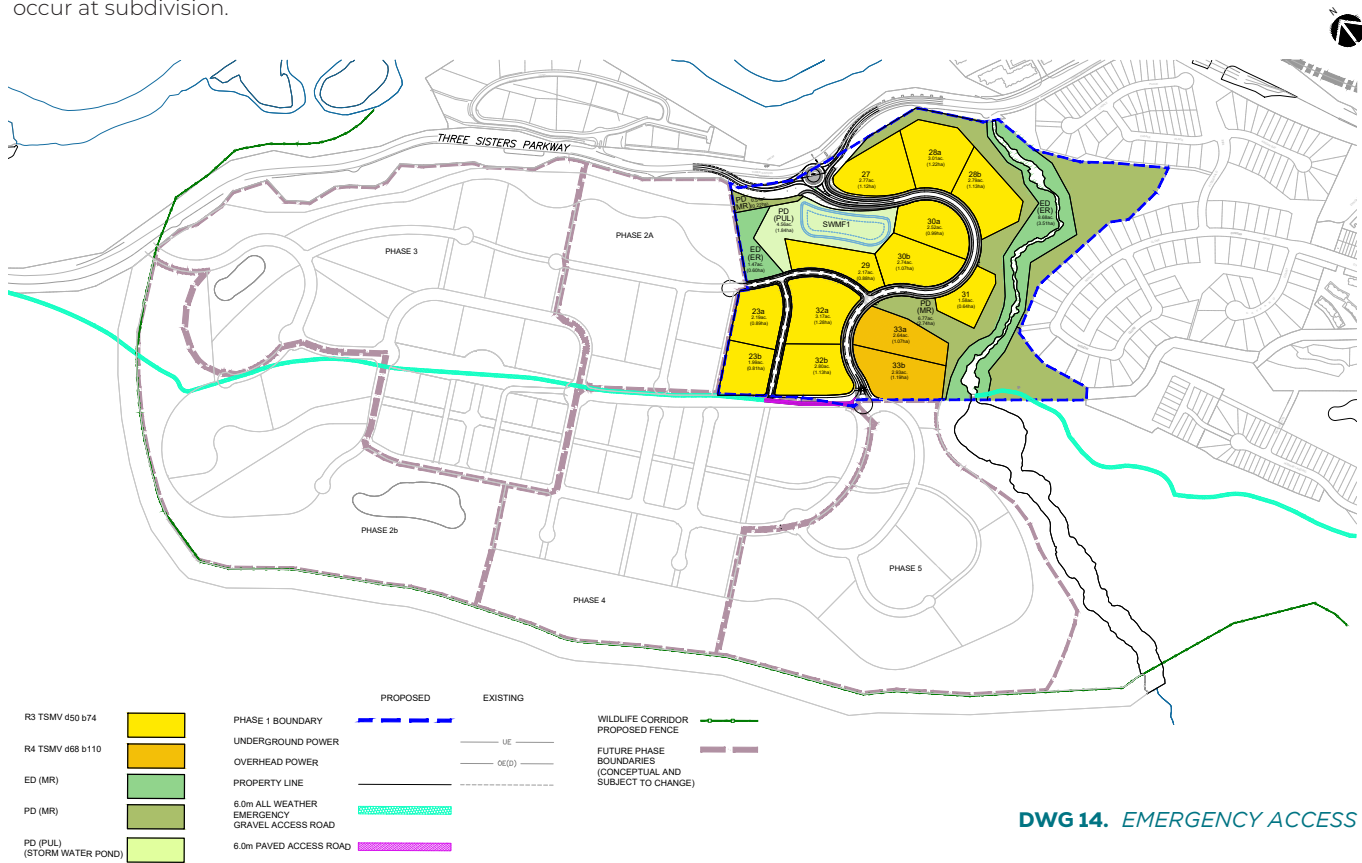
As identified in the Three Sisters Village ASP, access to lots will be limited where reasonable. Lot access will be identified at subdivision stage; assumptions about how trips will access the road network are included in the Mobility Assessment. Emergency access is identified on **DRAWING 14**. This temporary emergency access would be in place until ASP Phase 2a or 2b is proposed.





## Emergency Access

Emergency Access for Phase 1 will include Collector Road A as a permanent public access, an emergency access gravel road that will extend over the Altalink Bridge toward Three Sisters Boulevard, and an emergency access road (partially paved and the remaining gravel) from Collector Road A to the existing construction access road (See X drawing). Confirmation of the bridge loads will be required at subdivision. Discussion regarding how emergency access will be resolved as phases progress will also occur at subdivision.



**DWG 14. EMERGENCY ACCESS**

## Mobility Assessment

A Mobility Assessment has been completed to support the submission of this Conceptual Scheme. The Mobility Assessment evaluates the required transportation infrastructure needed to support development of Phase 1 in Three Sisters Village, in relation to the anticipated other new development intended in the area over the planning horizon for the Conceptual Scheme. This includes Phase 1 development of Smith Creek and completion of The Gateway at Three Sisters. Upgrades and new infrastructure needed within the transportation network are described above and illustrated throughout the Transportation Plan.

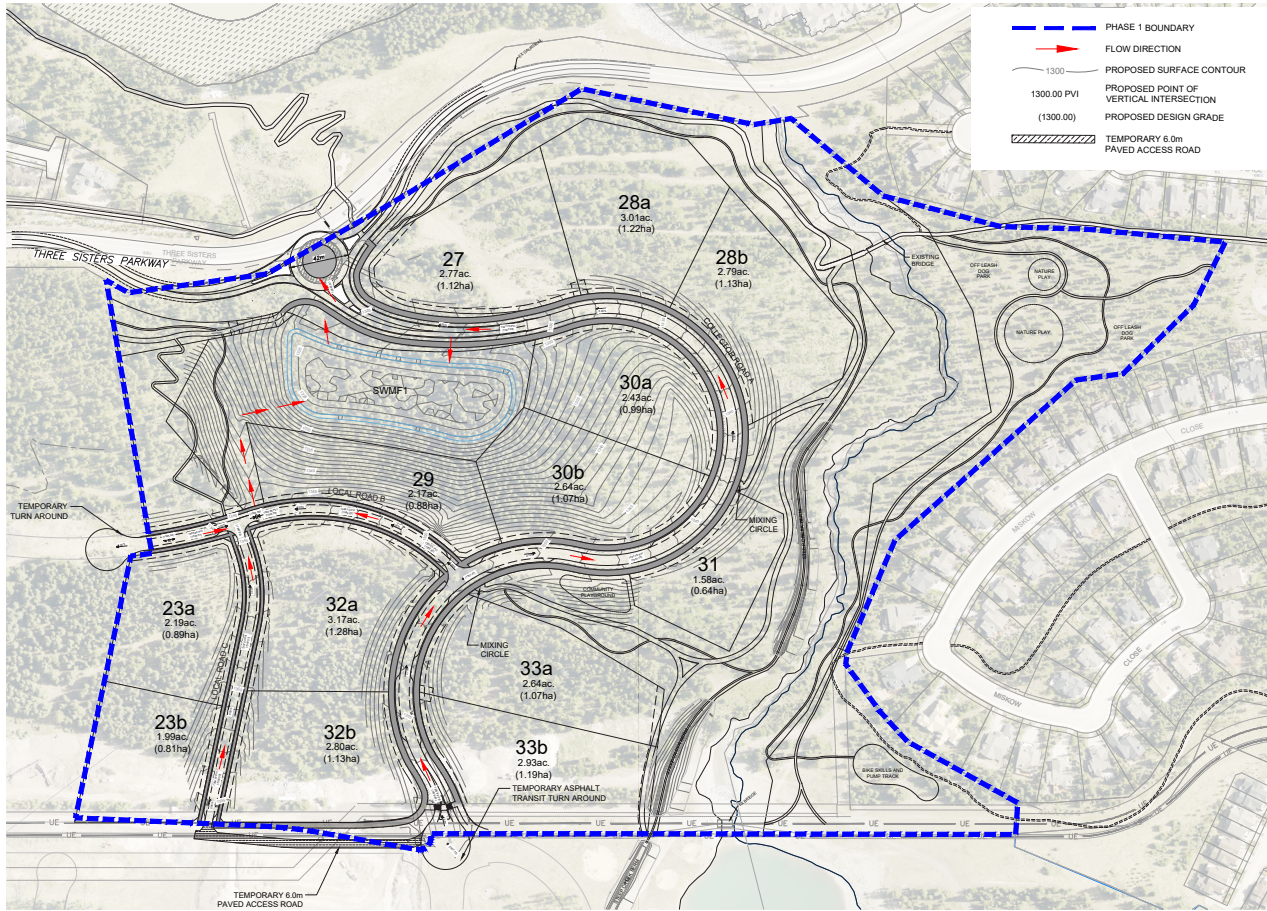
## Mobility Monitoring

As part of the ongoing evaluation of implementation of the overall TSMV, TSMVPL will partner with the Town of Canmore to co-fund and complete ongoing transportation data collection to: inform future mobility assessments for subsequent phases of Three Sisters Village and Smith Creek, improve understanding of travel behaviours and seasonal impacts on mode splits and behaviours, and assess the ongoing mode shift around the community with a

view to achieve targets set out in the Canmore Integrated Transportation Plan.

The monitoring program includes consideration of permanent count stations as well as seasonal turning movement counts completed annually. The Town of Canmore would be sharing their own traffic count data as a part of this program. Details of the monitoring program are outlined in the Mobility Assessment.



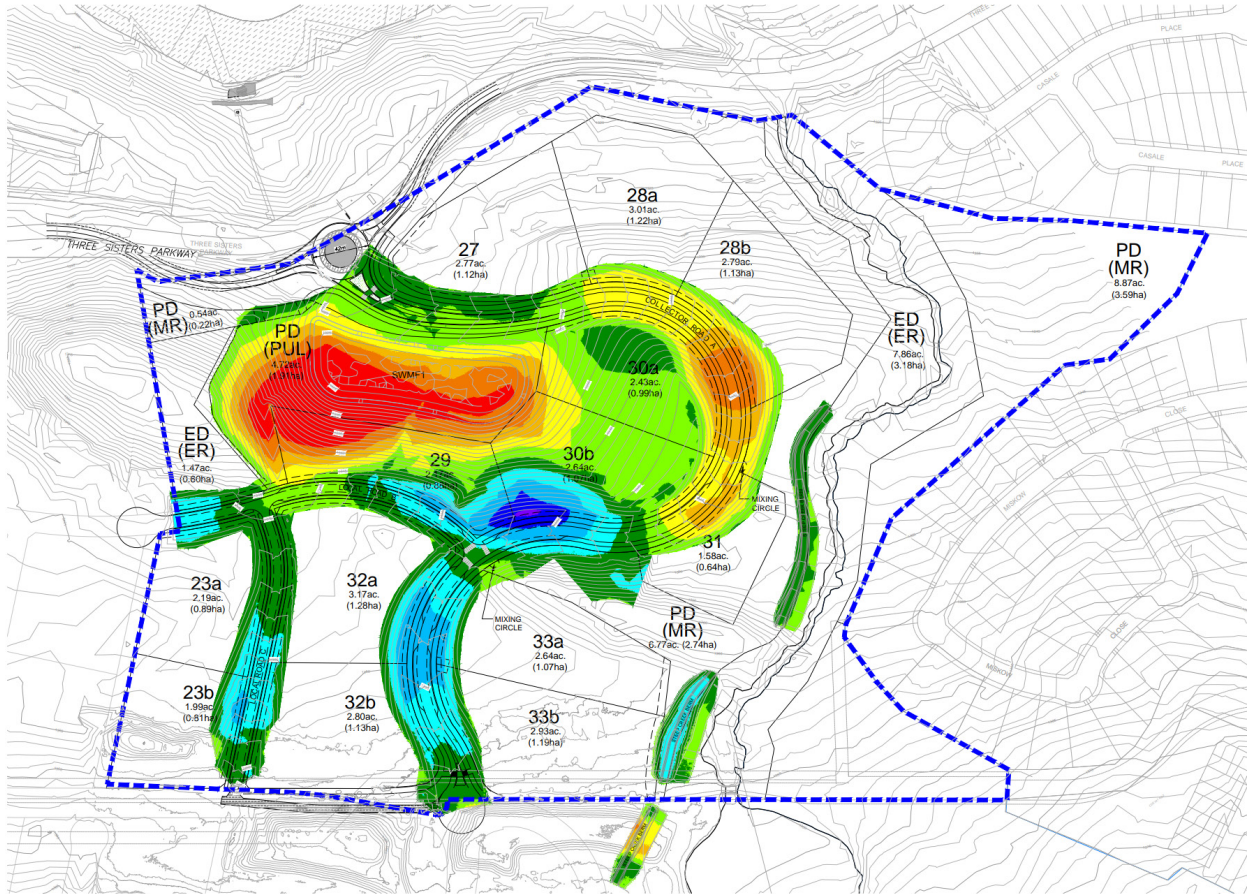


**DWG 15. GRADING PLAN (ORTHOGRAPHIC PHOTO)**

### Lot Grading Plan

The proposed site grading of the Conceptual Scheme area is shown in **DRAWING 15**. Site grading focuses predominantly on grading of the road rights-of-way and grading of the proposed stormwater dry pond in the PD parcel which aligns with the policies of the ASP. Individual residential parcels will remain under natural vegetation cover until their development unless grading is required for any utility or road right-of-ways. Additionally, a series of earth berms for the purpose of steep creek mitigation will be constructed along the west edge of Three Sisters Creek to protect the Phase 1 area from flooding. A map demonstrating the locations of cut and fill are shown on **DRAWING 16**. A geotechnical report is submitted in support of the Conceptual Scheme. There are no geotechnically unstable lands identified within the Phase 1 area and a factor of safety (FOS) equal or greater than 1.5 is achieved. The grading plan is in alignment with the ASP commitment to reduce impact on the environment and Policy 4.5.6 that seeks to limit impacts on the visual landscape reducing tree clearing to required roads and infrastructure in balance with achieving grading efficiencies. In alignment with this policy, grading of lots will be proposed at DP and further geotechnical assessment will need to be examined at that time. A post grading geotechnical assessment has been conducted for the lots that are being graded and a FOS of 1.5 is maintained for developable parcels. There is a steep grade change between the western most ER parcel and the PD surrounding the dry stormwater pond, this occurs in order to preserve the knoll feature.



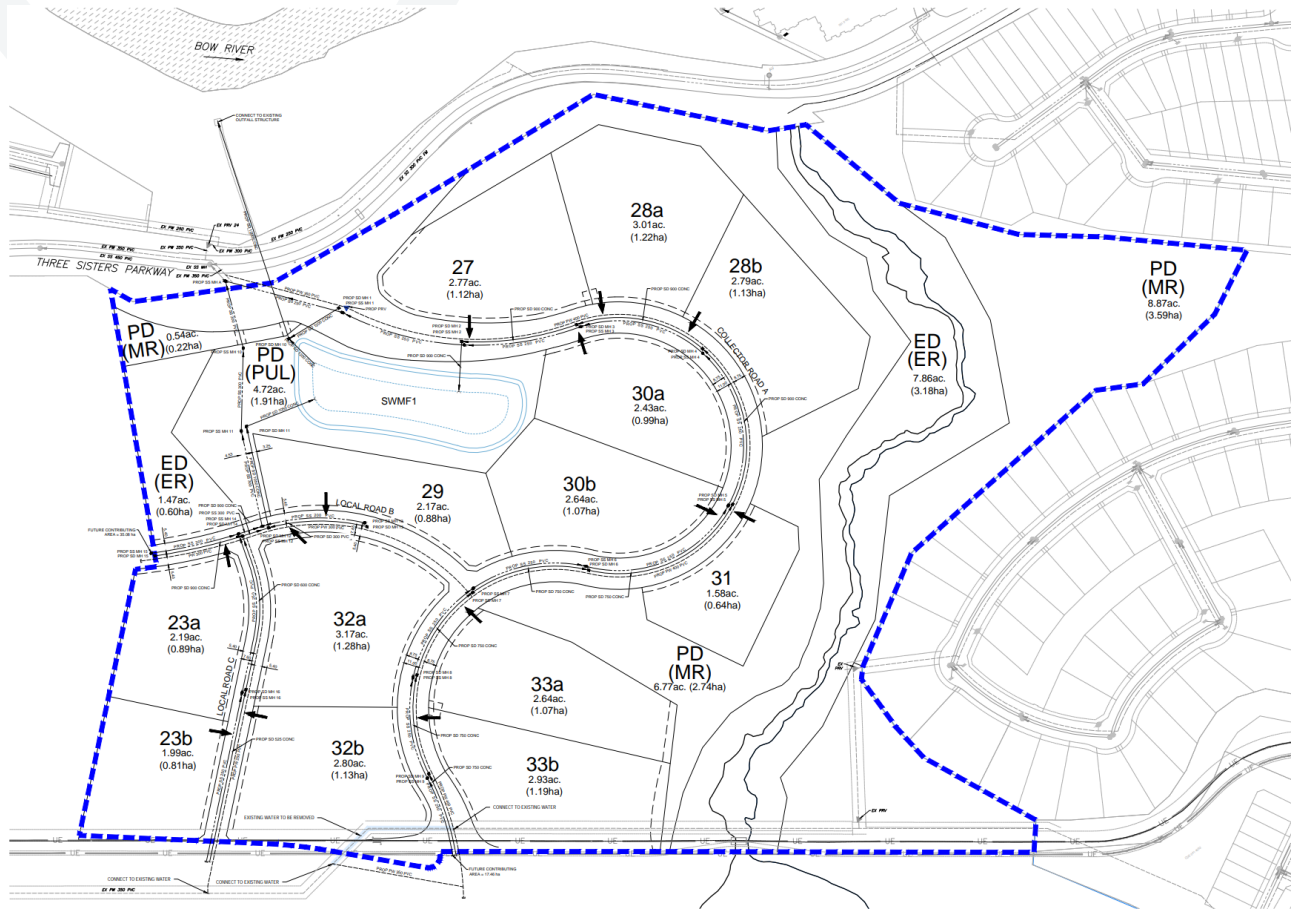


Elevations Table				
Number	Minimum Elevation	Maximum Elevation	Area	Color
1	-14.86	-8.00	7776.45	Red
2	-8.00	-6.00	6935.62	Orange
3	-6.00	-4.00	7718.66	Yellow
4	-4.00	-2.00	13590.53	Light Green
5	-2.00	0.00	23405.88	Green
6	0.00	2.00	26189.77	Dark Green
7	2.00	4.00	12016.24	Cyan
8	4.00	6.00	4994.84	Blue
9	6.00	8.00	958.48	Dark Blue
10	8.00	8.36	63.64	Purple

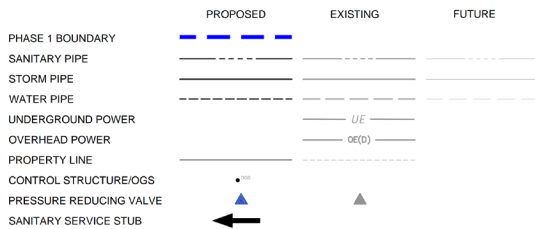
CUT & FILL VOLUMES  
 CUT = 225177.34m<sup>3</sup>  
 FILL = 87888.08m<sup>3</sup>  
 NET VOLUME = 137289.26m<sup>3</sup> (CUT)

DWG 16. CUT & FILL PLAN





DWG 16. PHASE 1 SERVICING PLAN



## Servicing Water

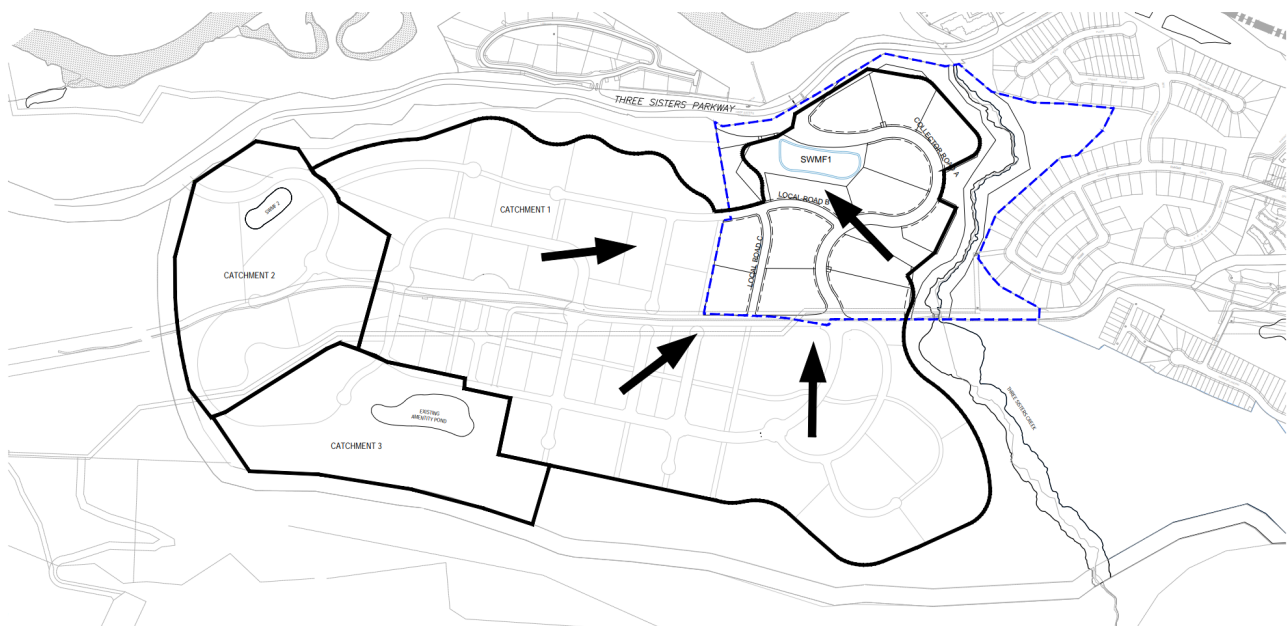
For municipal water servicing, an existing 350mm PVC water main runs along the north and south sides of the development lands and continues to service the Three Sisters Creek and Ridge developments, and the Stewart Creek ASP area to the east. The proposed water network (**DRAWING 16**) will tie in with the existing water network at two locations, the first is located on the Three Sisters Parkway and the second connection to the existing water main that runs from the Grassi Reservoir to the east.

The proposed water system will tie into the existing pipe loop that is connected to the Grassi Reservoir and that runs along the Three Sisters Parkway. The proposed water distribution system consists of 250 mm, 300mm and 400mm diameter PVC pipe. The existing 350mm water main that runs south of Phase 1 will be relocated in a new right-of-way and the proposed water system will connect into it at various points. The proposed water system servicing is in alignment with the Three Sisters Village Servicing Report submitted in support of the ASP. Furthermore, water demand from the Conceptual Scheme area is well within the 5 year horizon growth projections as noted in the Town of Canmore 2022 Utility Master Plan CAP 720. C04-00496

## Wastewater

For municipal sanitary sewer, a 450mm and 525mm PVC sanitary sewer trunk main flows from east to west under the Three Sisters Parkway and ultimately connects into Lift Station 8. The development will connect to this sanitary trunk through the existing servicing stub at the entrance to the Phase 1 Conceptual Scheme area (see **DRAWING 16**). The current Utility Master Plan (UMP) for the Town of Canmore recommends eventual upgrades to Lift Station 8 at 15 year horizon, but those will depend on the build out of Three Sisters Village, Stewart Creek, and Smith Creek ASP areas. No wastewater projects were noted in the UMP that are required to support growth in the first 5 year horizon and no upgrades are required to service Phase 1 Conceptual Scheme lands. Lift Station 8 and the sanitary trunks along Three Sisters Parkway have more than sufficient capacity to support the development lands. Lift Station 8 is also subject to a capacity reservation agreement between the Town of Canmore and TSMVPL due to servicing Dead Man's Flats.

Sanitary sewer servicing in the Phase 1 Conceptual Scheme area is in accordance with the previously submitted and approved Three Sisters Village Servicing Report that was submitted in support of the ASP and with the Town of Canmore's Engineering Design and Construction Guidelines (EDCG). The report does not recommend any additional upgrades than those already identified in the UMP and states that the flows from the Conceptual Scheme area will be determined at the subdivision stage of Phase 1.



**DWG 18. STORM CATCHMENT AREA**

## Stormwater Management

The site generally slopes from southwest to northeast with slopes ranging from 2% to 15%. The topography is typical of mountain slope regions with two drainages (Three Sisters Creek and Creek X which affect a small portion of the ASP Plan area) flowing adjacent to the ASP area. Within the ASP area, three manmade ponds exist, including one deep pool within the limits of the Three Sisters Creek. Ultimately, the drainage courses and all runoff from the site discharge into the Bow River.

The Master Drainage Plan was prepared and submitted in support of the ASP. The following paragraphs illustrate the stormwater management approach laid out in the ASP area and which the Phase 1 area aligns to (**DRAWING 18**). It should be noted that stormwater management facility (SWMF) 1 in the Phase 1 area is designed to service a larger catchment area than Phase 1 alone, and that temporary measures will be designed at the subdivision stage to intercept and channel the overland flows from future development areas.



The Intensity-Duration-Frequency (IDF) data from Environment Canada was utilized to determine the applicable rainfall amount for the site. City of Calgary design events have been applied as prescribed in the Canmore EDCG. The design events selected were the 5, 25, and 100 return year events over a 24-hour duration with a Chicago distribution. The 24-hour events simulate long duration events, highlighting issues associated with volume concerns, and release rates, while the Chicago distribution of the event assists in assessing peak flow conditions.

The Conceptual Scheme area will be serviced by a SWMF system consisting of stormwater gravity mains located underneath the public roadways, and a dry storm pond facility designed to treat and retain the stormwater. A SWMF is proposed to manage the runoff to an appropriate release rate, and to provide water quality treatment. This SWMF will be a dry pond, as it will provide better performance in terms of storage volume and water quality versus a wet pond, due to limited space and surface area. The proposed SWMF 1 will include a control structure that will outlet to the downstream stormwater system. An existing stormwater outfall located on the southern bank of the Bow River was constructed in anticipation of the development of this area and is a combined structure that also contains an irrigation intake. SWMF 1 is proposed to outlet to this existing structure. The control structure in SWMF 1 will be designed to limit discharge from the critical design storm event to the pre-development rates of 30 L/s/ha.

The proposed stormwater system will be designed following the major and minor methodology. The minor storm system will be designed to convey the 1 in 5-year storm runoff. The streets and gutters will be used to convey flows exceeding the 1 in 5-year storm (i.e. major) to the proposed SWMF. The minor system will be designed in detail at a later phase, but the methodology to be applied is the unit area release

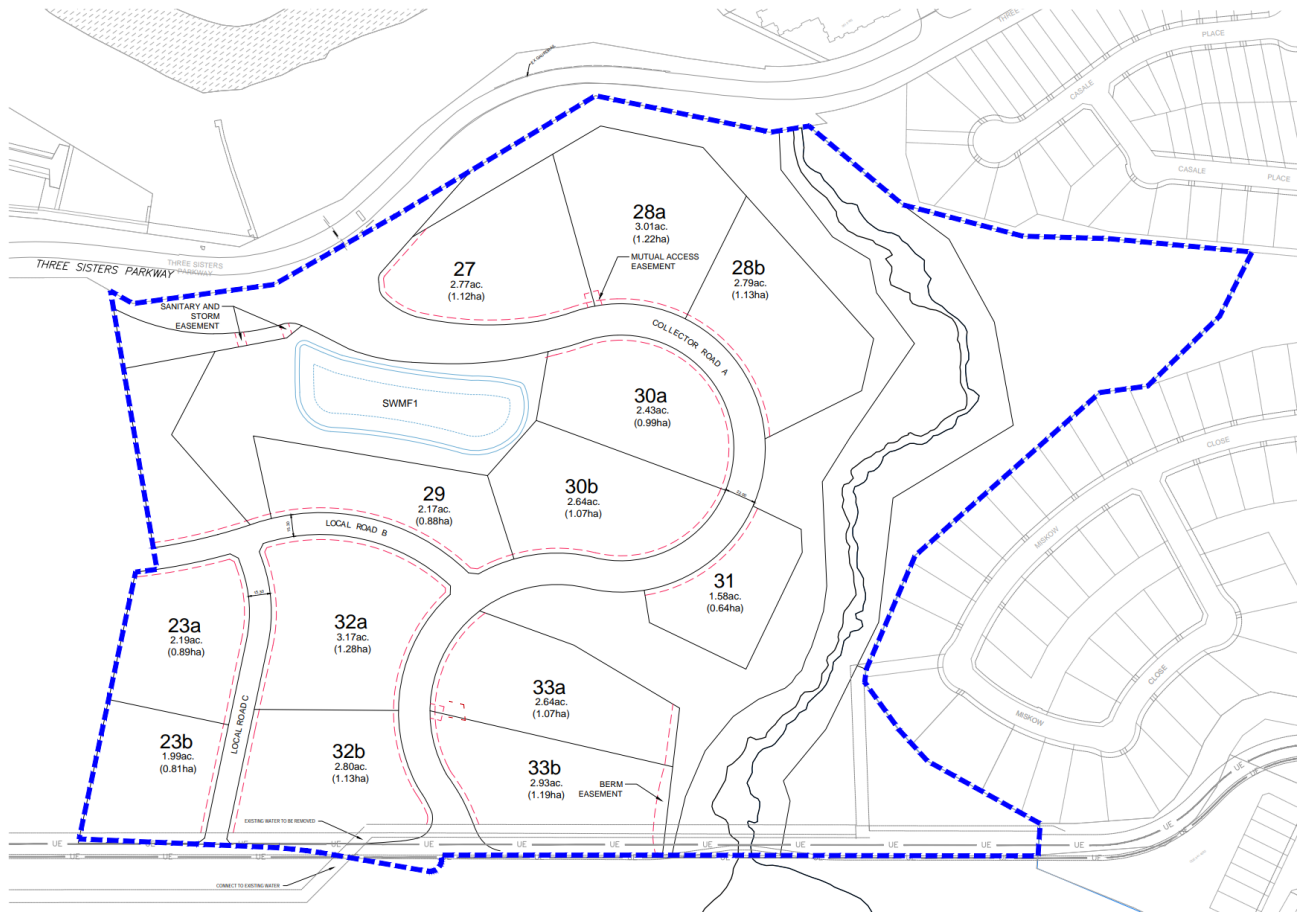
rate approach, applying a rate of 50 L/s/ha, which will encourage the use of low impact development and other stormwater features on an individual lot development level.

The stormwater management system shall recognize the subalpine forest environment of the ASP plan area and will account for freeze-thaw cycles and demonstrate the site can be appropriately serviced in accordance with ASP policy 9.4.8. Low impact development is encouraged at the individual lot level through bonusing. Stormwater management system has taken this into account and has demonstrated through the Staged Master Drainage Report that development can be properly serviced.

The bonusing approach to the development further encourages the use of low impact development and other stormwater features throughout the development and Phase 1 Conceptual Scheme. The roads throughout the development will be built conforming to the EDCG. The SWMF will be designed to detain excess run-off for the most critical design storm event and will discharge at the pre-development release rate.

The SWMF will discharge to the Bow River via an existing outfall structure just north of the Phase 1 area, that was installed in 2005 when the golf course was constructed. A 2100 mm pipe will be able to accommodate the required flows from SWMF 1 with a gradient of only 0.2%. Given the topography of the site, storm pipe grades in excess of what is required will be easily achievable. A site investigation and review of as-built drawings for the outfall and upstream pipe will be completed during future design phases.





	PROPOSED	EXISTING
PHASE 1 BOUNDARY		
PROPERTY LINE		
EASEMENTS		
UNDERGROUND POWER		

**DWG 19. RIGHT-OF-WAYS & EASEMENTS**

### Shallow Utilities

Connections to the shallow utility infrastructure for Phase 1 are available along Three Sisters Parkway and along the development lands in the case of Fortis's buried electrical distribution facility. At subdivision stage, further coordination with ATCO, Fortis, Telus and Rogers will be required to confirm exact servicing routing and potential upgrade requirements. Electrical, communications and natural gas would be generally allocated within a common trench, which has been the preferred approach between the Town, landowner and utility companies. Right-of-ways and easements are illustrated in **DRAWING 19**.



# Implementation

## Development Phasing

The Conceptual Scheme represents one phase of development and is in alignment with the ASP Phasing Map 22 as seen in **FIGURE 2** of this document.

## Monitoring and Adaptive Management Plan (MAMP)

The Province is responsible to maintain and manage wildlife corridors, including any necessary enforcement and is assuming the responsibility for the operations and maintenance of the wildlife fence. The Province has the sole authority over wildlife and wildlife corridors and enforcement within them. A MAMP has been reviewed and accepted by the Province. The MAMP was identified as a requirement to be prepared and submitted at the time of the first Conceptual Scheme (Policy 11.1.2) within either the Three Sisters Village or Smith Creek ASPs. After the ASPs' approval by the LPRT, the Province determined that they were the appropriate regulator of the MAMP due to its contents and their authority over wildlife corridor and Crown lands. The MAMP applies to Three Sisters Mountain Village mitigations identified within the EIS and is meant to address uncertainty in the predictions within the EIS.



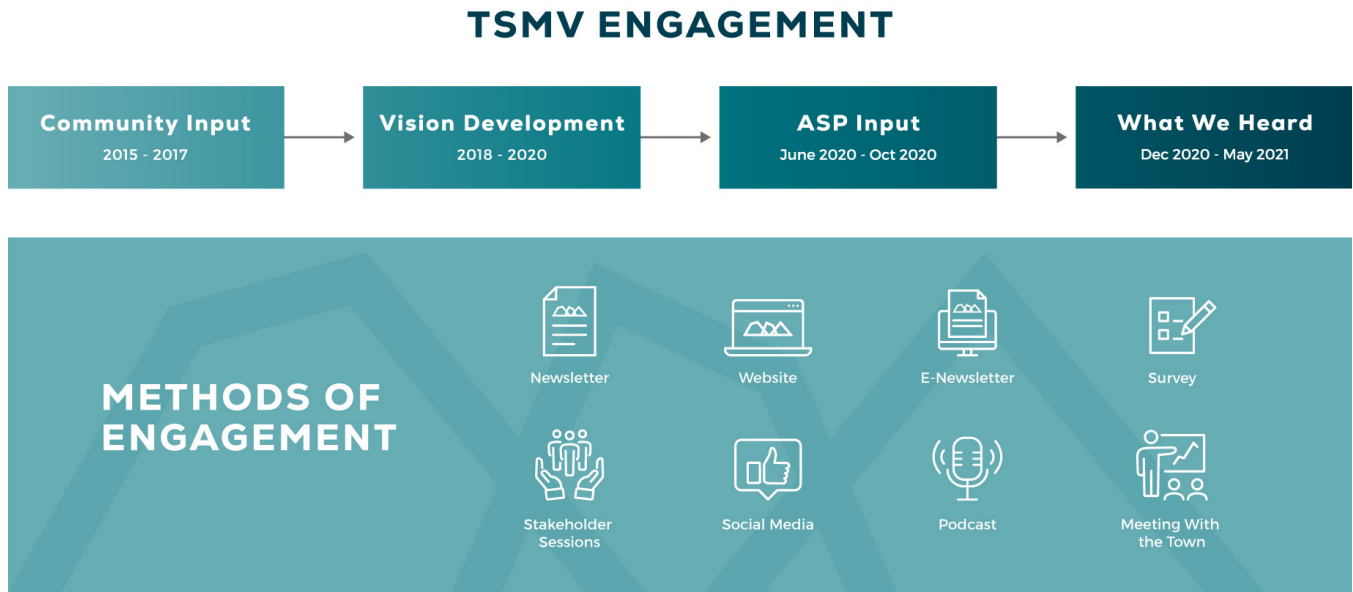
*Three Sisters Mountain Village Wildlife Camera  
- Grey Wolf*





# Engagement

The ASPs for Three Sisters Village and Smith Creek were informed by four stages of communication and engagement:



**FIG 10 - ENGAGEMENT PROCESS INFOGRAPHIC**

- **Phase 1:** Between 2015 and 2017 to inform Smith Creek ASP and included engagement that led to TSMV’s Resort Centre ASP amendment (now called Three Sisters Village);
- **Phase 2:** Between September 2018 and June 2020 to better understand communication gaps and articulate the vision for development prior to completing Draft ASPs;
- **Phase 3:** Input on Draft ASPs between July 2020 and October 2020 to identify the common themes and key directions in the ASP submission; and
- **Phase 4:** Communication following engagement on Draft ASPs with the intent to reinforce how input informed the plan and marketing efforts to leverage project support.

Throughout these stages of engagement, the TSMV team has held space for the tension that exists around common themes in the community, such as:

- Members of the community want a corridor that mitigates impact on wildlife, yet there are community members who want to recreate in the corridor;
- Residents want more housing to improve factors that impact affordability, yet there are residents who do not want property values impacted through the addition of housing;
- Engagement showed there is support for more sustainable transportation alternatives and a stronger connection between Three Sisters development and downtown, yet many do not want to see an increase in congestion or population growth; and
- Many who provided input wanted access to more convenient amenities (recreational or commercial), however, there are members of the community who do not want any more development.



The What We Heard Report summarizing the ASP engagement was completed in November 2020. This document identifies six (6) key themes developed through the engagement process. They are undermining, sustainability, transportation and mobility, affordable housing/staff housing/seniors housing, wildlife corridor, tax implications, and unit counts/density.

The TSV Phase 1 Conceptual Scheme marries the policy outlined in the ASP with land use and subdivision implementation in the creation of a Conceptual Scheme plan which honors the engagement themes.

**Undermining:** An undermining overview report was completed by experienced professional engineers at the ASP stage. An undermining assessment is submitted in support of this application in alignment with the Undermining Review Regulation and Guidelines. Further assessment may be completed at the subdivision stage in accordance with the finding of this report.

**Sustainability, Transportation & Mobility:** The Conceptual Scheme further outlines the sustainable transportation network considered within the ASP. Existing and proposed multi-modal trails within the Phase 1 area will service the community and provide connectivity to other areas of Canmore. Bonusing and carbon reduction is encouraged through the Density Bonusing Overlay.

**Affordable Housing/Staff Housing/Seniors Housing:** In addition to the implementation of inclusionary housing, a Bonusing Overlay is proposed to allow for affordable housing contributions beyond the 10% minimum requirement.

**Wildlife / Human-Use Mitigations:** A wildlife fence will be erected at the time of construction of Phase 1. Phase 1 recreational offerings encourage usage of the open spaces rather than the corridor. Phase 1 will be constructed to limit

wildlife attractants, provide alternatives to recreating in the corridor and provide off-leash spaces for those with dogs.

**Tax Implications:** In 2022, Council addressed the discrepancy of commercial taxation between Tourist Homes and other commercial businesses within Canmore by equalizing the rate of taxation applied to both. This would only increase the revenue contribution considered within the Municipal Fiscal Impact Assessment (MFIA) submitted in support of the Three Sisters Village ASP. The MFIA demonstrated that the tax revenue more than covers maintenance and replacement expenses to infrastructure, providing a net benefit to the Town of Canmore.

**Unit Counts/Density:** The units proposed within this Conceptual Scheme are in alignment with the ASP and the bonusing strategy proposed to support Council's strategic priorities. Units are capped at 3,000 and any units above that amount must be earned through criteria outlined in the Bonusing Overlay. Bonusing is based on the themes of carbon reduction, affordability and socio-cultural incentives.

The multimodal transportation network and opportunity for higher density provided by the land use plan contributes to increased sustainability. Density bonuses create incentive for builders/developers to contribute to community amenity and green building strategies.

Public engagement was conducted for both the Smith Creek and Three Sisters Village ASPs. Feedback from this engagement is applicable to the Conceptual Scheme. The LPRT hearings for the Smith Creek and Three Sisters Village ASPs began on February 22, 2022 with the LPRT issuing their decision on May 16, 2022. The LPRT concluded that both the Three Sisters Village and Smith Creek ASPs are consistent with the NRCB Approval and ordered the Town to adopt them.

## Conceptual Scheme Engagement

After submission of the Conceptual Scheme and land use redesignation application, TSMV will host public engagement sessions to provide the public an overview of the application, its consistency with the ASP, and detail the proposed land use districts and bonusing program that is intended to regulate development. Feedback received from the two events will be summarized in a What We Heard Report and submitted to the Town of Canmore after the open houses are held. Registration for both open houses will be available through the [tsmv.ca](https://www.tsmv.ca) website and social media. Advertisements for the open houses will be placed on social media and within the Rocky Mountain Outlook. Invitations to participate will also be sent directly to the Stoney Nakoda Nations consultation office and TSMVPL's email list.



## Parameters of Conceptual Scheme and Land Use Engagement

In accordance with S. 619 of the MGA, the Town of Canmore has 90 days to adopt the proposed amendments to the Land Use Bylaw. Given the Conceptual Scheme and land use application are consistent with the Three Sisters Village ASP which has been deemed consistent with the NRCB decision by the LPRT, there is no requirement for a public hearing.

As such, any public engagement will focus on educating the public on the Conceptual Scheme and land use application submission, anticipated timelines for the application, construction phasing, responding to frequently asked questions with regards to the project TSMVPL has updated their website and will examine public appetite for information sessions on the application in the New Year.

## First Nations Engagement

Building trust takes time and TSMVPL is working on several initiatives to build trust with our Indigenous neighbours including adopting an Indigenous Relations Policy, undergoing Indigenous Cultural Awareness training with the Stoney Nakoda Nations for our senior level employees, and ongoing communication with the Stoney Nakoda Nations.

TSMV's Indigenous Relations policy focuses our work with Indigenous communities and individuals into four key areas; building relationships, economic opportunities, community involvement and employment and training.

Since 2019, TSMVPL has attempted to meet with the Stoney Nakoda Nations (SNN) to begin the process of building a relationship. It is important to TSMVPL that this relationship is a direct connection between TSMV and SNN representatives. Through direct connection and communication, trust can grow and build.

Although significant effort has been made, it has not yielded enough time for in-depth conversation in advance of this application submission. A summary of our attempts to engage with the Nations is provided as part of the Traditional Ecological Knowledge Assessment – Table 2. While we acknowledge that engagement with our SNN neighbours is important, TSMVPL must also be able to submit timely municipal applications to enable development to move forward in alignment with the NRCB decision.





## APPENDICES



  
THREE SISTERS  
mountain  
village

# Appendix I

## Phase 1 Conceptual Scheme Statistics

<i>Three Sisters Village Phase 1 Land Use</i>	<i>Maximum Units (without bonusing)</i>	<i>Maximum Units (with bonusing)</i>	<i>Area (Ha/Ac)</i>		<i>% Net Developable</i>
Gross Developable			29.07	71.83	
Environmental District (ER)			3.80	9.38	
Net Developable			25.27	62.45	100%
R3 TSMV	558	826	11.15	27.55	44.1%
R4 TSMV	158	249	2.26	5.58	8.9%
Public District (MR)			6.53	16.14	25.8%
Public District (PUL)			1.91	4.72	7.6%
Roads/Lanes			3.42	8.46	13.6%

## Phase 1 Conceptual Scheme Anticipated Population

<i>Land Use</i>	<i>Maximum Units</i>	<i>Persons per Dwelling</i>	<i>Estimated Population</i>	<i>Units per Net Hectare</i>	<i>Units per Net Acre</i>
R3 TSMV	558	2.1	1172	50	20
	826		1735	74	30
R4 TSMV	158	1.6	253	68	28
	249		398	110	45



## Appendix II

### Submission Package List

1. Cover Letter
2. Land Use Amendment and Conceptual Scheme Application Form
  - a. Fee Calculation
  - b. Fee Confirmation
  - c. Land Titles and Registered Instruments
  - d. SSR Matrix Submittal Form
  - e. SSR Matrix
3. Pre-Application Comments: TSMV Response Document
4. Application Comments: TSMV Response Document
5. Conceptual Scheme Planning Rationale & Support Report
6. Technical Drawing Package
7. Land Use Overlays and Districts
  - a. Track Changes Land Use Overlays and Districts
8. Bonus Calculator
  - a. R3 and R4- Bonus Test Fit Graphic
9. Provincially Approved Historical Resources Act Application
10. Subdivision Mining Impact Assessment Report
11. Pre-Development Slope Stability Assessment
12. Three Sisters Village Phase 1 Geotechnical Investigation
  - a. Phase 1 Geotechnical Report Addendum
13. Post-Development Slope Stability Assessment
  - a. Track Changes Post-Development Slope Stability Assessment
14. Three Sisters Village Phase 1 Conceptual Scheme Mobility Assessment 2024
  - a. Track Changes 2024 Mobility Assessment
  - b. Turning Movement Figures
15. Wildfire Risk Assessment;
16. Traditional Ecological Knowledge (TEK) Assessment: Desktop Study
17. Three Sisters Creek Phase 1 Detailed Mitigation Design
  - a. Track Changes 2024 Phase 1 Detailed Mitigation Design
18. 2024 Wildlife Exclusion Fence Design Plan (Accepted by the Province)
  - a. Track Changes Wildlife Exclusion Fence Design Plan
19. BGC Wildlife Fence Commentary Technical Memorandum
20. Provincial Letter Re: Steep Creek Fencing Design



21. 2020 Environmental Impact Statement (EIS): Three Sisters Village Area Structure Plan
22. 2024 Servicing Report;
  - a. Track Changes Servicing Report
23. 2020 Master Drainage Plan;
24. 2024 Staged Master Drainage Plan;
  - a. Track Changes Staged Master Drainage Plan
25. 2024 Monitoring and Adaptive Management Plan (Accepted by the Province);
  - a. Track Changes MAMP
26. Provincial Letter re MAMP and WEFDP
27. Vegetation and Wetlands Technical Data Report; and
28. Wildlife and Biodiversity Technical Data Report.
29. TSMV DRAFT Design Guidelines



## Appendix III

### Three Sisters Village Phase 1 Conceptual Scheme and Land use Resignation ASP Policy Compliance Table

<i>Item</i>	<i>Goals, Objectives &amp; Policies</i>	<i>Alignment</i>	<i>How?</i>
3.2.1.	Inclusive  Incorporate variety of building forms and housing spectrum that accommodates diverse household compositions and income levels;  Incorporate a diverse range of services and enriching amenities for residents and visitors of all ages and backgrounds within the overall development;	yes  yes	TSV is proposing two (2) new Land Use Districts accommodating multi-unit residential dwellings at medium densities and has developed a Bonusing Overlay to encourage the provision of affordable housing options.  TSV Phase 1 has a comprehensive open space plan which integrates with the surrounding area and compliments the overall ASP.
	Accommodate all ages and abilities through street design, housing variety and the types of amenities offered	yes	TSV Phase 1 addresses inclusivity through multi-modal transportation and all ages recreational amenities. The community is designed to be human-scale, with a mixture of uses to facilitate the provision walkable services.
3.2.2	Resiliency  Consider how community design contributes to safety by creating lively and active places that respect the environment and establish systems that are adaptable in response to the impacts of climate change;	yes	TSMV/PL has collaborated with the Province of Alberta to dedicate ±166 hectares of land to wildlife corridor providing habitat and preserving connectivity in the Bow Valley region. A wildlife fence will be built to deter human/animal conflict and intensive planning has resulted in an open space system which encourages residents and guests to utilize community amenities. Density Bonusing Overlay addresses CHG reduction above and beyond the Alberta Building Code.
	Implement the recommendations from the EIS;	yes	See <b>APPENDIX IV</b> EIS compliance table.
	Continue to implement FireSmart approaches;	yes	FireSmart approaches will be utilized in Phase 1. The Wildfire Risk Assessment completed in June of 2020 for TSV has been included in the Conceptual Scheme Submission.
	Mitigating Steep Creek Hazards to protect proposed and existing developments from existing hazards and the impact of climate change;	yes	Land use for TSV Phase 1 acknowledges required set backs of Steep Creek through ED/ER designation. Mitigation has been provided in the form of a berm located on the west side of the creek. See also Steep Creek reports.
	Remediate previous mining activities	yes	A subdivision mining impact report has been prepared which identifies undermining hazards within Phase 1. Recommendations have been made regarding mitigation of identified areas which will be confirmed through detailed reporting at the development permit stage.
	Encourage low impact design and natural planting that contributes to climate change resilience through sequestering carbon and mitigating heat island effects.	yes	TSV Phase 1 intends to minimize impacts to the landscape by only grading those areas where infrastructure is needed to service the Phase. The open space plan proposes amenities with minimal disturbance of natural areas by designing amenities to incorporate natural features and vegetation. Low Impact Development is being encouraged through the Bonusing Overlay.





<b>Item</b>	<b>Goals, Objectives &amp; Policies</b>	<b>Alignment</b>	<b>How?</b>
3.2.3	Sense of Place  Multi-use pathways, trails and a variety of other year-round outdoor activities. Activities may include hiking, rock climbing, cycling, snowshoeing, skating, cross-country skiing and other outdoor recreational pursuits;  Unique retail and restaurant experiences facing along fine grained, human-scaled, pedestrianized streets and plazas;  Public art, education and wayfinding signs throughout the development that will guide people's interaction with the surrounding community and environment;  Inspired spaces for social connectivity, events, cultural facilities and festivals;	yes  n/a  yes  yes	TSV has a comprehensive open space plan which incorporates diverse programming for all seasons.  TSV Phase 1 is a residential phase of the TSV ASP.  The Phase 1 open space plan includes public art nodes and will incorporate education and wayfinding to help residents and guests better understand the community and it's surroundings at future stages.  TSV has a comprehensive open space plan which incorporates diverse programming and integrates with surrounding communities through it's multi-modal path system. Amenities include community and regional level parks which will serve residents and draw guests from outside the community.
	Flexible and innovative spaces for Canmore's business community to work in;	yes	TSV Phase 1 is planned as a residential phase of the TSV ASP however, accommodations have been made in the residential land use districts for Home Occupations - Class 1 and 2, Care Facilities, Day Cares, and Cultural Establishments.
	Scenic views and vistas of the surrounding mountains and Bow Valley.	yes	TSV Phase 1 maximizes views of the mountains and Bow Valley.
3.2.4	Design  Facilitate a multi-modal transportation network that will connect to other areas of Canmore, emphasizing sustainable modes of transportation, including walking, biking and public transit;  Create a fine-grained and compact neighbourhood that provides for the needs of the community;  Create interesting and inspired spaces for people to enjoy, interact with and linger in;	yes  yes  yes	Phase 1 integrates with the surrounding path system and is designed at a human scale to encourage residents to reduce car usage.  Phase 1 is designed as a compact walkable community.  Phase 1 contains a comprehensive open space plan with diverse amenities designed to attract and hold the attention of residents and guests alike.
	Encourage resiliency in both community and building design	yes	The Conceptual Scheme complies with the design guidelines of the ASP. Architectural guidelines have been submitted for consideration and will be registered at subdivision stage to further encourage resiliency.
	Minimize impacts on the natural environment by utilizing slope adaptive development design principles; and	yes	Planned amenities have been incorporated into the topography of the site. Community design will follow EDCC. Architectural guidelines will be submitted for consideration at subdivision stage to further encourage resiliency.
	Create spaces that respond to the surroundings and focus on naturalized planting to blend with and enhance the beautiful location the land is situated within	yes	TSV intends to preserve as much existing tree cover as possible by developing only those areas where infrastructure needed to service the Phase are required. The open space plan prioritizes minimal disturbance of natural areas by designing amenities to incorporate natural features and vegetation. Landscaping plans will be submitted at development permit stage.
3.2.5	Economy		



<b>Item</b>	<b>Goals, Objectives &amp; Policies</b>	<b>Alignment</b>	<b>How?</b>
	Create a pedestrianized mixed-use core with a variety of services to address residents and visitors needs;	n/a	Phase 1 must be completed to provide access and infrastructure to the proposed mixed-use core. The Phase is designed as a residential area and will be serviced by further phases.
	Address Cammore's need for long term fiscal sustainability by increasing the commercial assessment base and generating more year-round activity	n/a	Phase 1 is planned as a residential area.
	Create an Innovation District that will provide flexible spaces for residents to explore creative new ways to generate community wealth;	n/a	The innovation district is not located in this Phase of development.
	Provide a recreation, health and wellness hub where the tourist economy can thrive while creating opportunities for economic diversification and business expansion within the Innovation District.	n/a	The recreation, health and wellness hub is not located in Phase one, however regional parks have been provided that will compliment the hubs mission.
<b>4</b>	<b>Development Consideration</b>		
<b>4.1</b>	<b>Managing Development in Undermined Areas</b>		
4.1.1	Proposed subdivision or development on undermined areas shall be undertaken in accordance with the Cammore Undermining Review Regulation, Alberta Regulation 34/2020 of the Municipal Government Act (MGA), as amended from time to time.	yes	A subdivision mining impact report has been prepared which identifies undermining hazards within Phase 1. Recommendations have been made regarding mitigation of identified areas and is in accordance with the Cammore Undermining Review Regulation and Alberta Regulation 34/2020 of the MGA.
4.1.2	Proposed subdivision or development on or in close proximity to undermined areas shall be subject to evaluation, mitigation (if required), review and reporting as per Guidelines approved by Ministerial Order under Section 7 of Alberta Regulation 34/2020, as amended from time to time.	yes	A subdivision mining impact report has been prepared which identifies undermining hazards within Phase 1. Recommendations have been made regarding mitigation of identified areas which will be confirmed through detailed reporting at the development permit stage.
<b>4.2</b>	<b>Managing Development on Sloped Lands</b>		
4.2.1	Development shall be informed by the Guidelines for Subdivision and Development in Mountainous Terrain. Where a conflict between that policy and this ASP exists, the policies within this ASP shall prevail.	yes	Phase 1 complies with the Guidelines for Subdivision and Development in Mountainous Terrain and the TSV ASP.
4.2.2	Placement of buildings and structures should be avoided on slopes of 35% (19 degrees) or greater.	yes	The parcels within the Conceptual Scheme slopes that range from 2% to 15%.
4.2.3	For development on slopes exceeding 15% (8 degrees), a geotechnical report demonstrating slope stability will be required at the Subdivision stage to determine that the proposed development can be safely constructed.	yes	Portions of Phase 1 have slopes exceeding 15%. A slope stability assessment and post-grading geotechnical assessment have been completed and further geotechnical reports will be required at Development Permit.
4.2.4	Buildings should be sited to identify and integrate unique or major natural landforms.	yes	Building envelopes will be identified at the Development Permit stage and integrate unique or major natural landforms
4.2.5	The layout of buildings on a parcel should aim to protect natural features and be responsive to existing topography.	yes	Building envelope will be established at Development Permit. At Development Permit policy encourages development to respond to existing topography and protect natural features.



<b>Item</b>	<b>Goals, Objectives &amp; Policies</b>	<b>Alignment</b>	<b>How?</b>
4.2.6	Where there is a conflict between the creation of an active street front and the retention of significant natural site features, a rationale will be required for consideration by the Town to support the site planning approach at Development Permit submission.	yes	The Conceptual Scheme emphasizes preservation of natural features on development parcels.
4.2.7	Indirect bicycle and pedestrian connections should be considered when direct all ages and all ability connections cannot be provided within sloped portions of the Plan Area.	yes	The pathways and trails described in the Conceptual Scheme are intended to incorporate the terrain, while providing varied lengths and skill levels. These recreational paths are to occur in the MR area. The CS specifies consistency with the Town of Canmore Open Spaces and Trails Plan, which specify that MR area (which the pathways are situated on) will not utilize steeply sloped lands.
4.2.8	Parcels within the Plan Area should be comprehensively designed to efficiently site appropriate building forms and plan for access on sloped sites. To allow for comprehensive site planning of multi-residential developments, the creation of lots smaller than 0.4 ha through subdivision in advance of Development Permit should be avoided.	yes	The CS specifies that access to lots will be limited where reasonable. Lot access will be determined at the subdivision stage. Mutual access agreement locations have been proposed.
4.2.9	Building envelopes should be sited to run parallel to the natural contours of the site to minimize the impact of site development on the natural environment.	yes	Slope-adaptive building can be utilized on sloped sites and examined at Development Permit.
4.2.10	Development on sloped parcels should contribute to the mountain town character of Canmore and consider opportunities to provide scenic views from the site	yes	Architectural guidelines will be registered as an instrument at the time of Subdivision addressing mountain character and scenic views. This should be considered at Development Permit.
4.2.11	Rationale from builders citing efficient construction methods and/or ongoing maintenance standards should be considered at Development Permit when one of more of these policies cannot be met.	yes	To be evaluated at Development Permit when necessary.
4.3	<b>Managing Steep Creek Hazards</b>		
4.3.1	Steep creek mitigation shall be designed and constructed to meet the risk thresholds identified within the Town of Canmore's Municipal Development Plan (MDP) and Land Use Bylaw.	yes	The berm design included in the conceptual scheme meets the risk thresholds identified in the MDP and LUB construction compliance would be considered at subdivision stage
4.3.2	Minimum setback requirements for steep creek hazards are defined in the mitigation reports. At the time of Conceptual Scheme, further analysis will be required to determine setbacks in alignment with other regulations and the proposed land use and built forms	yes	The Conceptual Scheme submission identifies that mitigation to control the hazard within the proposed development is described in three reports including: the Three Sisters Creek Option Analysis, the Three Sisters Creek Preliminary Mitigation Design and the Three Sisters Creek Phase 1 Detailed Mitigation Design reports.
4.3.3	Implementation of steep creek mitigation(s) shall be initiated in alignment with policy in Section 11.5.	yes	Mitigation to control the steep creek hazard in Phase 1 comprises a series of berms to be built along the west edge of Three Sisters Creek. See report. Per 11.5.2 Hazard zones of the policy will be updated in accordance with Town policies at the time of construction.



<i>Item</i>	<i>Goals, Objectives &amp; Policies</i>	<i>Alignment</i>	<i>How?</i>
4.4	Managing Wildfire Risk		
4.4.1	Subdivision and development proposals considered to be located within or adjacent to a high wildfire hazard area should align with Firesmart: Protecting Your Community from Wildfire design principles.	yes	The Conceptual Scheme is in alignment with the submitted Wildfire Risk Assessment which provides conceptual Firesmart hazard mitigation recommendations to reduce the threat of wildfire to the proposed developments with an emphasis on the vegetation management requirements within and adjacent to wildlife corridors.
4.4.2	Firesmart planning within wildfire corridors adjacent to the Plan Area shall be coordinated with the Province and may also be combined with wildlife habitat improvement strategies.	yes	The Wildfire Risk Assessment provides Firesmart hazard mitigation recommendations to reduce the threat of wildfire on the proposed development as well as adjacent wildlife corridors.
4.4.3	Conceptual Schemes deemed to be located within or immediately adjacent to a high or moderate wildfire hazard will be required to undertake a Wildfire Risk Assessment.	yes	A Wildfire Risk Assessment for TSMV was submitted with the application for the Three Sisters and Smith Creek ASPs. Though not required at the ASP stage, it was intended to respond to the amendments to the wildlife corridor and the conceptual schemes for the ASPs.
4.4.4	Wildfire mitigation strategies (i.e. Firesmarting), such as vegetation management, will be implemented in alignment with the Wildfire Risk Assessment at Subdivision stage.	yes	To reduce the threat from wildfire, the Development Authority may require the recommendations from the Wildfire Risk Assessment be included as a condition of Subdivision.
4.4.5	Buildings must be constructed using fire-rated and fire-resistant exterior building materials for roofing, siding and deck/balcony materials	yes	Roofing materials with a minimum Class B Underwriters Laboratory Canada Fire rating are specified as acceptable. Commercial-ready residential units shall be constructed to a 1-hour fire rating.
4.5	Visual Landscape Design and Assessment		
4.5.1	Parcels that are visible from the Trans-Canada Highway adjacent to the Plan Area will be identified at Conceptual Scheme.	n/a	Phase 1 has limited visibility from highway due to distance and natural features. Parcels which are visible have been identified in <b>FIGURE 1</b> .
4.5.2	Parcels that are intended to create a visual identity for the resort from the Highway will be identified at Conceptual Scheme.	yes	Phase 1 has limited visibility from highway due to distance and natural features. Parcels which are visible have been identified in <b>FIGURE 1</b> .
4.5.3	Visual Assessments may be required during the Conceptual Scheme phase.	yes	Visual assessments are not required unless buildings are proposed with a height greater than 5 stories.
4.5.4	At Development Permit, Visual Assessments will be required for development that propose development of 5 storeys or above. Where development is not visible due to previous development approvals or tree cover, a Visual Assessment should not be required.	yes	A visual assessment may be completed for development proposed at 5 storeys or above at the Development Permit stage, unless said development is not visible due to previously approved development or tree cover. Parcels which are visible have been identified in <b>FIGURE 1</b> .
4.5.5.a	Visual Assessments shall identify: How the architecture of the building complements or relates to the backdrop of the mountains landscape from a maximum of two high visibility points along the portion of the Trans-Canada Highway closest to the Plan Area;	yes	The visual assessment will occur at the Development Permit stage if required. Parcels which are visible have been identified in <b>FIGURE 1</b> .
4.5.5.b	Visual Assessments shall identify: Propose the use of landscaping, building siting and materials that are proposed to complement or standout from the surrounding landscape	yes	The visual assessment will occur at the Development Permit stage if required. Parcels which are visible have been identified in <b>FIGURE 1</b> .



<b>Item</b>	<b>Goals, Objectives &amp; Policies</b>	<b>Alignment</b>	<b>How?</b>
4.5.6	To allow for a change to the landscape to occur gradually within the Plan Area, vegetation should only be cleared within a development phase and will occur in accordance with an approved grading plan	yes	A grading plan has been completed and site clearing will only occur as needed to allow construction of utility/infrastructure. See <b>DRAWING 15</b> .
4.5.6.a	The size of development phases will be determined to maximize effectiveness for grading balances, infrastructure efficiencies and reduced maintenance costs for the Town and future residents and businesses. It is recognized that vegetation removal should be a staged process with portions of individual sites potentially left for future DP applications.	yes	The Conceptual Scheme aligns with Phase 1 within the Growth Phasing (Map 22). Grading and infrastructure efficiencies have been proposed in recognition of the requirements for steep creeks mitigation and MR/ER designations required for this phase.
4.5.6.b	Building materials should take inspiration from the colours of the surrounding natural landscape.	yes	Architectural Guidelines will be submitted at Subdivision stage with building form and materials detailed at the Development Permit stage.
<b>5</b>	<b>Neighbourhood Framework</b>		
<b>5.2</b>	<b>Land Use Concept</b>		
5.2.0.1	The Land Use Concept (Map 9) shall be considered by the Approving Authority as conceptual when reviewing future planning applications within Three Sisters Village. Boundaries of the land use concept and open space designations shown on the map are meant to be illustrative only and may vary to accommodate planning, design or engineering considerations at more detailed stages within the planning process.	yes	The Conceptual Scheme and Land Use plans are in alignment with the land use concept (Map 9).
5.2.0.2	The minimum number of residential units permitted within the Plan Area is 3,000. An additional 2,000 units are permitted to a maximum of 5,000 units, when bonusing is applied. Residential units for the purpose of this policy includes tourist homes.	yes	The minimum residential units for Phase 1 is 724 and Maximum when bonusing is applied is 1093. UPH has been used to give the town stronger control over density and bonusing units. The Phase minimum and maximum are in alignment with ASP policy.
5.2.0.3	Building heights within the Plan Area should generally be in accordance with Map 10.	yes	Building heights proposed within land use districts are in accordance with Map 10 of the ASP.
5.2.0.4	Tourist homes and visitor accommodation will be located as outlined in Map 9.	yes	Provisions have been made within the proposed Land Use District R4 TSMV Residential Medium District to allow Tourist homes as a permitted use. Further regulation is provided within the Three Sisters Village Land Use Overlay on development requirements and operation. Placement of the R4 TSMV/land use is in alignment with Map 9.
5.2.0.5	The highest densities and building heights shall be located within the Village Centre and generally transition as development moves away from the Village Centre.	yes	Phase 1 occurs outside of the Village Centre and is primarily a residential area with two parcels designated R4 TSMV allowing for medium density residential.
5.2.0.6	The mobility network should be designed to focus on the pedestrian, bicycle and transit experience and be accessible to people of all ages and abilities.	yes	A mobility assessment has been completed for Phase 1 with the complete street network designed to accommodate pedestrian and bicycle traffic accessible to all ages. Transit stops have been located to provide service throughout the phase within a 400m (or 5 min) walkshed.
5.2.0.7	Buildings should be human-scaled and oriented appropriately to frame streets and multi-use pathways.	yes	Architectural guidelines will be submitted at the subdivision stage. Urban design will comply with the ASP and Town of Canmore Community Architectural & Urban Design Standards and be addressed at the Development Permit stage



<i>Item</i>	<i>Goals, Objectives &amp; Policies</i>	<i>Alignment</i>	<i>How?</i>
5.2.0.8	Residential buildings should utilize glazing and primary at-grade entrances to activate facades fronting onto streets.	yes	Architectural guidelines will be submitted at the subdivision stage. Urban design will comply with the ASP and Town of Canmore Community Architectural & Urban Design Standards and be addressed at the Development Permit stage
5.2.0.9	Street furniture should be provided at appropriate locations throughout the Plan Area, particularly at transit stops, and provide options to linger in commercial areas.	yes	Architectural guidelines will be submitted at the subdivision stage. Urban design will comply with the ASP and Town of Canmore Community Architectural & Urban Design Standards and be addressed at the Development Permit stage
5.2.0.10	Street lighting should be pedestrian-scaled and light streets and multi-use-pathways.	yes	Architectural guidelines will be submitted at the subdivision stage. Urban design will comply with the ASP and Town of Canmore Community Architectural & Urban Design Standards and be addressed at the Development Permit stage. Lighting will comply with the Three Sisters Village Land Use Overlay.
5.2.0.11	Intercept parking areas for vehicles should be provided to encourage people coming to Three Sisters Village to park their vehicle and switch to an active transportation mode.	n/a	Intercept parking areas have been identified at the ASP level within the Village Centre and Innovation District. There is no intercept parking located in Phase 1 and the completed mobility assessment does not identify it as a requirement.
5.2.0.12	Convenient and secure bicycle parking will be provided in both private areas and the public realm throughout the Plan Area.	yes	Architectural guidelines will be submitted at the subdivision stage. Urban design will comply with the ASP and Town of Canmore Community Architectural & Urban Design Standards and be addressed at the Development Permit stage
5.2.0.13	Intercept parking lots should be provided in key locations within the Plan Area	n/a	Intercept parking areas have been identified at the ASP level within the Village Centre and Innovation District. There is no intercept parking located in Phase 1 and the completed mobility assessment does not identify it as a requirement.
5.2.0.14	It is important that intercept parking lots are visible from the street and with clear wayfinding, while screening the appearance of large parking areas via landscaping	n/a	There are no intercept parking lots located within Phase 1.
5.2.0.15	Onsite parking should be located at the rear of buildings via a lane or internal roadway or underground.	yes	Onsite parking will be located at the rear of buildings and access identified at the Subdivision stage.
5.2.0.16	On-street parking for short-stay visitors, residents and visitors will be provided on streets outside of the Pedestrianized Priority streets within the Village Centre.	n/a	On-street parking will be provided in Phase 1 on both Collector and Local Roads.
5.2.0.17	Building design and construction materials shall be consistent with a mountain design aesthetic through design or materiality.	yes	Architectural Guidelines will be submitted at Subdivision stage with building form and materials detailed at the Development Permit stage.
5.2.1	<b>Streetscapes and Building Frontage</b>		
5.2.1.1	All street-facing frontages shall: a. Be designed to appear human-scaled; b. Emphasize front entrances on the street with verandas, porches and other landscape features; c. Reinforce pedestrian-oriented activity to facilitate views, public space needs and prioritize safe pedestrian movement; and d. Consider pedestrian experience in winter with design elements such as lighting, arctic entries, barrier-free entryways and canopies.	yes	Architectural Guidelines will be submitted at Subdivision stage with building form and materials detailed at the Development Permit stage. Urban design will comply with the ASP and Town of Canmore Community Architectural & Urban Design Standards.



<b>Item</b>	<b>Goals, Objectives &amp; Policies</b>	<b>Alignment</b>	<b>How?</b>
5.2.1.2	Commercial frontages should be oriented towards the street and front setbacks shall consider buildings to be located on or within very close proximity to the front property line.	yes	Architectural Guidelines will be submitted at Subdivision stage. Urban design will comply with the ASP and Town of Canmore Community Architectural & Urban Design Standards.
5.2.1.3	Commercial frontages should turn the corner and address both streets where feasible.	yes	Architectural Guidelines will be submitted at Subdivision stage. Urban design will comply with the ASP and Town of Canmore Community Architectural & Urban Design Standards.
5.2.1.4	Residential frontages should be located within proximity to the sidewalk.	yes	Front yard setbacks have been reduced in the proposed R3 TSMV and R4 TSMV land use districts to 3.0m.
5.2.1.5	Service access for garbage, recycling and deliveries shall be accommodated where appropriate.	yes	Service access for garbage, recycling and deliveries will be detailed at the Development Permit stage.
5.2.1.6	Residential buildings on corner parcels should provide a transition between each facade, through strategies such as: a. Continuing architectural features, materials and glazing around the corner; b. Continuing public or amenity space around the corner; and c. Locating the primary entrance at building corners, or by providing entrances on both street-facing façades.	yes	Architectural Guidelines will be submitted at Subdivision stage with building form and materials detailed at the Development Permit stage. Urban design will comply with the ASP and Town of Canmore Community Architectural & Urban Design Standards.
<b>5.2.2 Commercial Primary Streets</b>			
5.2.2.1	Commercial and office frontages located on the ground floor facing a street shall accommodate: a. Entrances and windows to enhance permeability and transparency; and b. Barrier-free entrances accessed by hard surfaced walkways.	N/A	Commercial (retail, office and restaurants) is not proposed within Phase 1 and as such these policies do not apply.
5.2.2.2	All commercial frontages located on the ground floor facing a street should provide amenities including, but not limited to: a. Patios to sit and dine outside where appropriate; b. Urban trees; and c. Interactive elements, such as public art displays or public seating.	N/A	Commercial (retail, office and restaurants) is not proposed within Phase 1 and as such these policies do not apply.
5.2.2.3	Weather protection should be provided on Primary Commercial street frontages in the Village Centre, with such protection permitted to project beyond private property to provide public realm protection.	N/A	Commercial (retail, office and restaurants) is not proposed within Phase 1 and as such these policies do not apply.
5.2.2.4	All buildings should relate directly to the streets and public pathways on which they front that create identity and a sense of address.	N/A	Commercial (retail, office and restaurants) is not proposed within Phase 1 and as such these policies do not apply.
<b>5.3 Residential and Medium Density Residential Districts</b>			
5.3.1	Townhomes and semi-detached dwellings must be accommodated within comprehensive development sites in the Residential area in accordance with Map 9	yes	Townhomes and semi-detached dwellings are listed uses in R3 TSMV Residential Comprehensive Multiple Unit District which is the primary land use in Phase 1.
5.3.2	Apartments and stacked townhomes must be accommodated within the Medium Density Residential area in accordance with Map 9.	yes	Apartments and stacked townhomes are permitted uses in R4 TSMV Residential Medium Density District. There are two lots zoned R4 TSMV within Phase 1. Stacked Townhomes are also a permitted use when section 7.10 applies within R3 TSMV.



<i>Item</i>	<i>Goals, Objectives &amp; Policies</i>	<i>Alignment</i>	<i>How?</i>
5.3.3	Stacked townhomes, townhomes and semi-detached dwellings with driveways accessing a street should be avoided or consolidated to reduce pedestrian and bicycle conflict with vehicles.	yes	Lot access will be addressed at subdivision stage and consolidated to reduce pedestrian and bicycle conflict. A joint use easement has been incorporated at Land Use for lots 27 and 28a, and lots 33a and 33b, to facilitate this policy.
5.3.4	Parcels adjacent to the Village Centre District may have tourist home uses if they are built to front and access a collector street. In all other cases, parcels should be developed entirely as residential or a tourist home use.	n/a	Phase 1 is not adjacent to the Village Centre District.
5.3.5	The provision of Accessory Dwelling Units will be encouraged within semi-detached and townhome building forms.	yes	Accessory Dwelling Units are a permitted use in R3 TSMV and R4 TSMV.
<b>5.4</b>	<b>Village Centre</b>		
5.4.1	The Village Centre shall be accommodated in accordance with Map 9.	n/a	The Village Centre is not within the Phase 1 conceptual scheme area.
5.4.2	The Village Centre District shall accommodate a mix of retail, restaurant, entertainment, commercial services, office, residential and tourist home uses.	n/a	The Village Centre is not within the Phase 1 conceptual scheme area.
5.4.3	Office, residential, tourist home or other complementary uses shall be located on the second storey or higher within the Village Centre on Commercial Primary Streets (Map 11). a. Sales Centres and Real Estate Offices shall be considered retail uses within this Plan Area.	n/a	The Village Centre is not within the Phase 1 conceptual scheme area.
5.4.4	Commercial space will accommodate small to medium format uses at-grade. a. Small format retail should have a maximum shopfront width of 9 metres; b. Medium format retail should have a maximum shopfront width of 15 metres; c. Convenience grocery and grocery stores are exempt from the above shop width maximums; and d. Additional guidelines for formatting of commercial spaces in the Village Centre are outlined in Appendix A.2.	n/a	The Village Centre is not within the Phase 1 conceptual scheme area.
5.4.5	A limited number of medium format commercial retail stores will be allowed to anchor this zone and to support the small format commercial uses in on Commercial Primary streets.	n/a	The Village Centre is not within the Phase 1 conceptual scheme area.
5.4.6	A minimum of one Village Centre hotel parcel will be located within the northern part of the Village Centre District. This parcel will: a. Incorporate building heights up to 4 to 6 storeys; b. Incorporate high-quality and distinctive architecture to facilitate a sense of place and sense of arrival into the area; c. Ensure a high-quality, publicly accessible pedestrian connection is maintained through the site via a mews or similar features that is integrated into the site and/or building design; and d. Integrate strong pedestrian connections to and from the Village Centre and transit stops	n/a	The Village Centre is not within the Phase 1 conceptual scheme area.





<i>Item</i>	<i>Goals, Objectives &amp; Policies</i>	<i>Alignment</i>	<i>How?</i>
5.4.7	Tourist home and residential uses must be contained in separate buildings within this district.	n/a	The Village Centre is not within the Phase 1 conceptual scheme area.
5.5	The Innovation District		
5.5.1	The Innovation District shall be accommodated in accordance with Map 9.	n/a	The Innovation District is not within the Phase 1 conceptual scheme area.
5.5.2	The Innovation District will accommodate a range of uses including microbreweries/distilleries, light manufacturing, businesses that benefit from tourism, live/work units, artist lofts/studios, flexible co-working spaces for start-ups and remote workers, as well as other uses required to support or complement one another within the district.	n/a	The Innovation District is not within the Phase 1 conceptual scheme area.
5.5.3	Retail and food/beverage uses should be considered as ancillary to the primary function of creative manufacturing, offices, art spaces and music venues.	n/a	The Innovation District is not within the Phase 1 conceptual scheme area.
5.5.4	Buildings must be pedestrian-scaled and oriented, but floor and building heights and building form should be flexible to create interesting warehouse and market-style architecture that is distinctive to this district.	n/a	The Innovation District is not within the Phase 1 conceptual scheme area.
5.5.5	Retail and food/beverage uses not associated with primary functions like creative manufacturing, art space, music venues, etc. should be limited in the Innovation District until the majority of the commercial development in the Village Centre is constructed and occupied. At this time retail and food/beverage uses may be permitted in this district.	n/a	The Innovation District is not within the Phase 1 conceptual scheme area.
5.5.6	Patios associated with breweries, distilleries and food purveyors will be encouraged to face streets or multi-use pathways where feasible.	n/a	The Innovation District is not within the Phase 1 conceptual scheme area.
5.6	Hotel and Spa District		
5.6.1	The Hotel and Spa District shall be accommodated in accordance with Map 9.	n/a	The Hotel and Spa District is not within the Phase 1 Conceptual Scheme Area.
5.6.2	The heights in the Hotel and Spa District shall be accommodated in accordance with Map 10.	n/a	The Hotel and Spa District is not within the Phase 1 Conceptual Scheme Area.
5.6.3	This area will accommodate a variety of boutique hotels and lodges in a variety of forms as well as associated complementary uses like meeting rooms, spas and other health, wellness and recreational services.	n/a	The Hotel and Spa District is not within the Phase 1 Conceptual Scheme Area.
5.6.4	Buildings within this district will contribute to a high-quality public realm and will provide for and clearly delineate uses and activities for visitors and paid visitors.	n/a	The Hotel and Spa District is not within the Phase 1 Conceptual Scheme Area.



<i>Item</i>	<i>Goals, Objectives &amp; Policies</i>	<i>Alignment</i>	<i>How?</i>
5.6.5	Buildings should be integrated into the landscape utilizing policies outlined in Section 4.2 where feasible, and will consider access requirements, servicing requirements, undermining constraints and geotechnical aspects.	n/a	The Hotel and Spa District is not within the Phase 1 Conceptual Scheme Area.
5.6.6	Developments will provide strong pedestrian and cycling connections to the Village Centre, Innovation District and to other destinations within the Plan Area and beyond.	n/a	The Hotel and Spa District is not within the Phase 1 Conceptual Scheme Area.
<b>5.7</b>	<b>Indoor Recreation</b>		
5.7.1	The Indoor Recreation Area shall be accommodated in accordance with Map 9.	n/a	The Indoor Recreation Area is not within the Phase 1 Conceptual Scheme Area.
5.7.2	The heights in the Indoor Recreation Area shall be generally accommodated in accordance with Map 10.	n/a	The Indoor Recreation Area is not within the Phase 1 Conceptual Scheme Area.
5.7.3	The primary uses in the Indoor Recreation District will be indoor recreation facilities, movie theatres, youth camps, fitness facilities, arts and cultural uses, and may include other indoor recreation, entertainment and meeting and event spaces. Commercial uses such as equipment sales/rentals or maintenance services may also be accommodated.	n/a	The Indoor Recreation Area is not within the Phase 1 Conceptual Scheme Area.
5.7.4	The purpose of the Municipal Reserve land dedication within the Indoor Recreation District shall be to accommodate a recreation centre or field house	n/a	The Indoor Recreation Area is not within the Phase 1 Conceptual Scheme Area.
5.7.5	A public-private partnership may be pursued to build an indoor recreation facility and may incorporate such town needs as community meeting spaces, a multi-use field house, a library, a performing arts space, an indoor aquatic centre or other uses as identified in the Town of Canmore Recreation Master Plan or the Arts & Cultural Master Plan.	n/a	The Indoor Recreation Area is not within the Phase 1 Conceptual Scheme Area.
5.7.6	The Indoor Recreation area will be easily accessible by walking, biking and transit. Reduced vehicle parking requirements should be considered for the Indoor Recreation area.	n/a	The Indoor Recreation Area is not within the Phase 1 Conceptual Scheme Area.
5.7.7	Pedestrian linkages between buildings and adjacent open space should be prioritized when designing key facilities within this district. The organization of site elements should minimize conflicts between sustainable transportation modes and vehicles.	n/a	The Indoor Recreation Area is not within the Phase 1 Conceptual Scheme Area.
5.7.8	Pick-up/drop-off locations and parking areas should be designed to minimize their impact on the adjacent public realm.	n/a	The Indoor Recreation Area is not within the Phase 1 Conceptual Scheme Area.



<b>Item</b>	<b>Goals, Objectives &amp; Policies</b>	<b>Alignment</b>	<b>How?</b>
5.8	Bonusing Toolkit		
5.8.1	The FAR outlined in Table 2 shall apply to all parcels within the Plan Area as outlined in Map 9.	yes	As per policy 5.8.5 bonusing incentive will be implemented within the Land Use Bylaw. It was determined during preparation for the Conceptual Scheme that use of UPH to measure area density provides the Town with more control over implementation than that of FAR. Heights have been proposed in alignment with the ASP. UPH has been utilized in both the Bonusing and General Overlay and as a control for unit count in the R3 TSMV and R4 TSMV Land Use districts.
5.8.2	The maximum FAR shall only be achieved by including one or more bonusing items listed in Table 3 for Town consideration at Development Permit.	yes	Maximum units can be achieved through bonusing incentives assessed and approved by the Town at the development permit stage. A bonusing calculator has been provided for consistent assessment of the Density Bonusing Overlay.
5.8.3	To be eligible for the FAR bonus, the applicant shall demonstrate meeting requirements outlined in Table 3 to the satisfaction of the Town of Canmore.	yes	The applicant will demonstrate at the time of development permit that they have met the requirements of the Density Bonusing Overlay in order to be eligible for the unit bonus.
5.8.4	To participate in the art bonus incentive, Developers are encouraged to engage an experienced art consultant to coordinate their art process. Such consultants are encouraged to be responsible for the preparation of art plans and the coordination of artist selection and artwork fabrication, installation and documentation	yes	Developers eligibility for the art bonus incentive will be determined at the Development Permit Stage.
5.8.5	The Bonusing Incentive shall be implemented within Land Use Bylaw at Conceptual Scheme.	yes	A Density Bonusing Overlay has been developed for inclusion in the Land Use Bylaw that reflects and expands on the direction provided in Table 2 and 3 of the ASP. UPH has been utilized to determine minimum and maximum density.
5.8.6	Table 2 and Table 3 may be superseded through the adoption of land use bylaw and shall not require an amendment to this ASP.	yes	The land use bylaw will be amended at the conceptual scheme and land use stage to include the proposed Density Bonusing Overlay which utilizes UPH as opposed to FAR.
5.8.7	Relaxations to setbacks and heights at Development Permit stage should be considered to allow for the bonus program to work effectively.	yes	Existing land use districts have been adapted to facilitate development within TSMV included setbacks and building height. If relaxations are required they will be requested at Development Permit stage.
5.8.8	The base FAR for each district within the Land Use Concept shall be 50% of the maximum FAR.	yes	The 3000 base unit count and the 5000 maximum was chosen as a more effective way to control for unit counts than FAR.
5.8.9	The maximum Gross Floor Area (GFA) with bonusing in each district should be: a. 415,000 m2 in the Residential area; b. 160,000 m2 in the Medium Density Residential area; c. 100,000 m2 in the Village Centre; d. 28,000 m2 in the Village Centre Hotel area; e. 29,000 m2 in the Innovation District; f. 25,000 m2 in the Indoor Recreation Area; and g. 27,000 m2 in the Spa and Hotel District.	yes	The bonusing calculator outlines that we will not exceed the 5000 units. The bonus calculator has been altered to utilize UPH (as opposed to FAR) as it is a more effective way to control unit counts



<i>Item</i>	<i>Goals, Objectives &amp; Policies</i>	<i>Alignment</i>	<i>How?</i>
5.8.10	The FAR Bonusing table (Table 2) shall be confirmed at Conceptual Scheme to ensure that the FARs are aligned with parcel size given that Environmental Reserve allocation is unknown.	yes	Phase 1 utilizes UPH as opposed to FAR to calculate bonusing density as this gives the Town better control over proposed unit counts and incentives.
5.8.11	An Administrative and Developer review of the Bonusing Incentive should occur annually to ensure bonusable items remain relevant.	yes	A review of the bonusing incentive will be completed annually by administration and developers. Once established, it might be more prudent to review at each time a new Conceptual Scheme is proposed.
6	Municipal Parks & Open Space		
6.1	Municipal Parks & Open Space General Policy		
6.1.1	The dedication of Municipal Reserve will primarily be focused on creating a large municipal park located within the eastern portion of the Plan Area as indicated on Map 12.	yes	MR within Phase 1 represents the first portion of the planned municipal park and open space within the ASP.
6.1.2	Dedication of Municipal Reserve will occur in phases and will be provided to the Town at Subdivision	yes	The Phase 1 Conceptual Scheme has identified MR associated with the ASP Concept Plan. These lands will be provided to the Town at the time of subdivision.
6.1.3	All development within the Plan Area will consider and comply with mitigation strategies outlined in Table 50 of the EIS and any adaptive management implemented as a result of monitoring.	yes	This Conceptual Scheme submissions aligns with the recommendations found in Table 50 of the Three Sisters Village EIS. See <b>APPENDIX IV</b> .
6.1.4	Undermining considerations for open spaces, Municipal Reserve area of private recreation areas shall be undertaken for the purpose or public safety of outdoor recreation use.	yes	The Phase 1 Conceptual Scheme aligns to the Undermining Review/regulation and Guidelines. Please also refer to the Undermining Report submitted under separate cover.
6.1.5	A centralized multi-functional plaza shall be incorporated into the design of the Village Centre as part of the Municipal Reserve contribution to allow for seasonal amenities, public art, passive and active spaces, cultural spaces and event programming to promote year-round vibrancy.	N/A	The Village Centre is not within the Phase 1 Conceptual Scheme area.
6.1.6	It is encouraged that open spaces be designed to accommodate community gathering.	yes	The Entry Feature area describes in the Conceptual Scheme provides a gathering area allowing for community gathering. Phase 1's recreational offerings encourage locals and guests to gather and recreate in open spaces as an alternative to the wildlife corridor. The park and open spaces encourage connectivity and gathering. The tree fort park and the bike pump track all provide different way in which locals and guests to connect within Phase 1.
6.1.7	Public open spaces should be designed to provide a variety of opportunities such that people of all ages and abilities have an opportunity to participate in physical activity.	yes	Phase 1 of Three Sisters Village includes a multi-modal transportation network that considers access for users of all ages, abilities and modes. Residents will have opportunities to walk, cycle and access a transit network. Collector Road A includes an all access sustainable modes connection through Phase 1. Other pathways also provide sustainable transportation connections and allow for a variety of recreational opportunities to occur within the Phase 1 area.



<b>Item</b>	<b>Goals, Objectives &amp; Policies</b>	<b>Alignment</b>	<b>How?</b>
6.1.8	Deferred Reserve Caveats registered on lands within Three Sisters Mountain Village should be used within the Plan Area.	yes	Deferred Reserve Caveat (DRC) will be used within Phase 1 to facilitate additional dedication of Municipal Reserve lands. The last remaining DRC (after the MR Reconciliation Agreement is signed between TSMVPL and the Town) is registered within the Smith Creek ASP area (DRC 001185759) and has a value of 16.365 acres. The ASP acknowledges that there will be an excess of Municipal Reserve dedication within the Plan Area and that the DRC will be used to accommodate the Municipal Reserve dedication. Given that the required Municipal Reserve dedication for this phase is 2.9 ha (7.18 ac), the excess 11.48 acres will reduce the 16.365 acres DRC 001185759 to 4.885 acres at the time of subdivision.
6.1.9	Open Space areas, including the Resort Recreation Amenity Area, will connect to other trails located within the Plan Area for access by the public.	yes	The trails and pathways in TSV provide trail and pathways options that connect residents to the greater network which will connect into the Resort Recreation Amenity Area once the area is initiated through Phasing.
6.1.10	The planning and design of recreational and cultural facilities to be located within Municipal Reserve area will be determined at Subdivision	yes	Architectural Guidelines will be submitted at Subdivision stage with building form and materials detailed at the Development Permit stage. Urban design will comply with the ASP and the submitted Urban Design & Architectural Guidelines.
6.1.11	Off-leash dog parks will be provided in strategic locations considering convenience and accessibility.	n/a	Phase 1 does not contain an off-leash dog park as the Hubman off-leash park is located within 500 metres.
6.1.12	Off-leash dog parks will be fenced and should be designed to minimize conflict between off-leash dogs and other recreational uses.	n/a	Phase 1 does not contain an off-leash dog park as the Hubman off-leash park is located within 500 metres.
6.1.13	Recreational facilities should be located within a ten-minute walk from residential developments, provide options for children of varying ages and designed in collaboration with the Town of Canmore.	yes	Phase 1 proposes two (2) community parks and two (2) regional parks. Detailed Design will occur after subdivision approval.
6.1.14	Steep creek hazard mitigation work for Three Sisters Creek should be considered and incorporated appropriately into open space concepts and take into account both steep creek mitigations, EIS mitigations and potential recreation opportunities	yes	This Conceptual Scheme is in alignment with the Three Sisters Creek: Steep Creek Hazard & Mitigation report and the mitigation work is incorporated into the designation of MR and ER areas.
6.1.15	The steep creek mitigation zone should primarily be designated as Environmental Reserve. However, where pathway, access and recreational amenities can be safely provided, those areas will be designated as Municipal Reserve.	yes	The majority of ER designations within Phase 1 relates to floodplain for Three Sisters Creek. The floodplain includes diversion berms for steep creek workings. Substantial pathways and recreation offerings occur within the MR areas.
6.1.16	A mix of amenities such as off-leash dog parks and playgrounds will be provided via Municipal Reserve or via a title transferred to the Town with a covenant ensuring the recreational use of the land in perpetuity.	yes	Municipal Reserve titles will be transferred to the Town at subdivision stage with a registered covenant ensuring the protection of these lands for the purpose of recreation.
6.2	<b>Allocation and Credit for Municipal Reserve Lands</b>		
6.2.1	Up to a maximum 10% Municipal Reserve shall be designated per project phase, exclusive of any Deferred Reserve Caveats the Developer chooses to utilize in the area.	yes	Phase 1 allocates 28% of the total area as Municipal Reserve. MR provision in excess of the 10% will be credited to the remaining DRC 001185759 within the Smith Creek ASP
6.2.2	If more than the required of Municipal Reserve is dedicated within a particular phase, a credit will be applied to the subsequent phase.	yes	Phase 1 contains an excess of MR. Credit will be applied to reduce the remaining DRC.



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6.2.3	Outstanding Deferred Reserve Caveats should be reduced as a primary tool, or a credit will be applied to a future phase at the selection of the Developer.	yes	The excess dedication will be applied to DRC 00118579 within the Smith Creek ASP, reducing the DRC at subdivision.
6.2.4	Town of Canmore policy PD-011 "Three Sisters MR" applies to the lands within this Area Structure Plan. Where a conflict between the two documents exists, the policies within this ASP shall apply.	yes	Phase 1 aligns to the ASP policy and no conflicts with Town of Canmore policy PD-011 have been identified.
6.2.5	Municipal Reserve should be credited for 100% of the area of land that allows for public access and nearby publicly accessible lands available for passive public use or aesthetic considerations for the purpose of: <ul style="list-style-type: none"> <li>a. Parks and sporting fields;</li> <li>b. Separated walkways, trails and pathways;</li> <li>c. Handscaped plazas and amphitheatres;</li> <li>d. Land for the purposes of municipal cultural, community or recreation facilities;</li> <li>e. Other such facilities mutually agreeable to the Developer and the Town; and</li> <li>f. Where there is clear public access granted to an area in perpetuity (such as a plaza within a road right-of-way) or via a public access agreement signed between the Developer and the Town.</li> </ul>	yes	Municipal credit is anticipated for the entirety of the area of the MR dedication in alignment with ASP policy.
6.2.6	Where utility facilities or underground vaults such as lift stations or water pump houses (Public Utility Lots) are located, the area required by these facilities should not be counted towards Municipal Reserve. However: <ul style="list-style-type: none"> <li>a. Where utilities are located beneath an area that is formally and programmatically a park, that area should be counted towards Municipal Reserve;</li> <li>b. Where stormwater ponds are used for recreational activities such as skating, broomball and hockey in winter or non-motorized boating in summer; the area dedicated to stormwater should be credited as Municipal Reserve; and</li> <li>c. Dry pond facilities that can be used as sports fields or a park should be considered as Municipal Reserve.</li> </ul>	yes	Municipal credit is anticipated for the entirety of the area of the MR dedication in alignment with ASP policy. PUL have not been included in the proposed MR area.
6.3	<b>Municipal Services and Facilities</b>		
6.3.1	Municipal Reserve lands and conceptually located services and facilities are outlined in Map 12.	yes	Phase 1 aligns with the Open Space lands outlined in Map 12
6.3.2	Municipal services and facilities will be located and funded in alignment with Part 3 of the Municipal Government Act (MGA).	yes	The Conceptual Scheme, Infrastructure proposed to support the application area and the proposed MR and ER dedications align to the MGA.



<i>Item</i>	<i>Goals, Objectives &amp; Policies</i>	<i>Alignment</i>	<i>How?</i>
6.3.3	Municipal services and facilities located on Municipal Reserve land may include: a. Recreation facilities, baseball diamonds, soccer fields, pickle ball courts, playgrounds, off-leash dog parks, running and bicycle trails; a municipal service yard or other recreation or community facilities	yes	Phase 1 contains two (2) playgrounds, a bicycle skills and pump track, formal pathways and mountain bike trails.
6.3.4	Decisions on the design and construction of the amenities and their location will be determined at Subdivision.	yes	Conceptual locations have been provided at Conceptual Scheme. Finalization of the exact location and orientation of the facilities will be finalized during the detailed design stage.
6.4	Resort Recreation Amenity Area		
6.4.1	The Resort Recreation Amenity Area shall be generally accommodated within the open space area after designation of Municipal Reserve, Environmental Reserve and Steep Creek mitigation work as conceptually illustrated on Map 12.	n/a	The Resort Recreation Amenity Area is not located within Phase 1.
6.4.2	Activity Hubs should be located within the Resort Recreation Amenity Area providing an opportunity for residents and visitors to recreate outside of areas devoted to wildlife.	n/a	The Resort Recreation Amenity Area is not located within Phase 1.
6.4.3	Activity Hubs will occupy up to 40% of the Resort Recreation Amenity Area.	n/a	The Resort Recreation Amenity Area is not located within Phase 1.
6.4.4	Activity Hubs should be connected via a network of pathways for walking, cycling and other forms of recreation within the Resort Recreation Amenity Area	n/a	The Resort Recreation Amenity Area is not located within Phase 1.
6.4.5	The primary uses within the Activity Hubs should be focused on outdoor recreational pursuits and include private and/ or public amenities such as an outdoor climbing wall, campground, a bicycle pump track, trails, ropes course, skating, snowshoeing, kayaking, canoeing and zip-lines.	n/a	The Resort Recreation Amenity Area is not located within Phase 1.
6.4.6	Facilities required to support uses within the Resort Recreation Amenity Area such as bathrooms, change rooms and reception areas, storage areas, gathering spaces, amphitheatres or other recreational uses will be permitted.	n/a	The Resort Recreation Amenity Area is not located within Phase 1.
6.4.7	Buildings and storage areas must be clad in materials that align with the mountain architecture and design of the area.	yes	The Resort Recreation Amenity Area is not located within Phase 1.
6.4.8	Activity Hubs will be landscaped or fenced in such a way as to provide a distinction between public open space and areas that may require a fee for the recreational use.	n/a	The Resort Recreation Amenity Area is not located within Phase 1.
6.4.9	Outdoor storage will be allowed and must be screened with high-quality landscaping and kept in a clean and orderly manner	yes	The Resort Recreation Amenity Area is not located within Phase 1.
6.4.10	An integrated wayfinding system will inform, educate and direct visitors and residents to the activities within the Resort Recreation Amenity Area, and within the Plan Area.	yes	The Resort Recreation Amenity Area is not located within Phase 1.



<i>Item</i>	<i>Goals, Objectives &amp; Policies</i>	<i>Alignment</i>	<i>How?</i>
6.4.11	The Three Sisters Village wayfinding system will comply with the Town's signage standards	yes	The Resort Recreation Amenity Area is not located within Phase 1.
6.5	Trail Network		
6.5.1	A comprehensive pathway and trail network will be developed in Three Sisters Village to provide recreation opportunities for activities such as hiking and mountain biking as well as walking and exploring.	yes	Phase 1 contains a comprehensive pathway and trail network which integrates with the TSV ASP area and neighboring communities.
6.5.2	Conceptual onsite trail connections are outlined in Map 15 and conceptual regional and offsite connections are outlined in Map 16.	yes	Phase 1 aligns and expands on connections outlined in Map 15 and 16.
6.5.3	Trail alignment and design should facilitate a hierarchy of trails that accommodate a variety of diverse recreational needs.	yes	Phase 1 outlines a hierarchy of trails. Please refer to <b>DRAWING 7</b> and <b>DRAWING 11</b> submitted under separate cover.
6.5.4	The trail network should offer recreational or interpretive experiences that take advantage of local terrain (i.e. steeper slopes), natural vistas and vantage points within open space areas.	yes	The Conceptual Scheme identifies the alignments of the pathway and trail network for Phase 1. The trail network takes advantage of the steep creek berm mitigation to allow for diverse recreational experiences for the casual walker and cyclist and the more active recreationalists. Interpretive experiences will be examined at the detailed design stage.
6.5.5	The pathway and trail network shall facilitate community connectivity within the Plan Area. The trail network should offer trail options to move from one place to the other and be appropriately signed to ensure easy access.	yes	The pathway and trail network facilitates community connectivity through phase 1 and to other areas of Canmore. Please refer to <b>DRAWING 7</b> and <b>DRAWING 11</b> .
6.5.6	Trails should be consistent with the classification of trails outlined in the Town of Canmore Open Spaces and Trails Plan.	yes	Trail classification is consistent with those outlined in the Town of Canmore Open Spaces and Trails Plan.
6.5.7	Trails in Three Sisters Village should consider the following principles: a. Provide trail loops and stacked loops of varying length; b. Terrain may be incorporated into trail features or designs such as tight and open turns, straight sections and rolling hills; c. Trails should be designed to recognize 'desire lines' to keep users on designated trails or to encourage appropriate use of designated trails; and d. Trails should be located on soils that are naturally resistant to erosion or designed and constructed to minimize erosion with appropriate maintenance.	yes	These experiences have been considered in the design of the pathway network for Phase 1. Please refer to <b>DRAWING 7</b> and <b>DRAWING 11</b> .





<i>Item</i>	<i>Goals, Objectives &amp; Policies</i>	<i>Alignment</i>	<i>How?</i>
6.5.8	Trail links will be provided from the Plan Area trail system to existing and future designated Provincial trails in accordance with the following: a. The number and locations of links will depend on the existing and future external trail network. Regional trail connections that are currently designated will be used and future trail alignments on Provincial lands will be subject to approval from the relevant land managers; b. Signs will be erected at all entry points to the wildlife corridor informing trail users of the importance of the wildlife corridor. Provincial rules regarding conduct and to highlight the requirement to stay on approved Provincial trails and to keep pets leashed; and c. Where designated trails outside the ASP boundary do not exist, encourage the completion or construction of new trails in collaboration with applicable land managers while working to ensure any undermining constraints are considered.	yes	The intent is to provide connections build up to ensuring that these connections are provided over time. The pathway connections focus on facilitating the necessary connections within Phase 1 that aligns to this overall policy. Please refer to <b>DRAWING 7</b> , and <b>DRAWING 11</b> . Signage will be detailed at the detailed design stage.
6.6	Open Space Policies Related to the Wildlife Corridor		
6.6.1	Public pathways and trails may be lit at night. Lit pathways and trails should be subject to a lighting curfew to reduce the amount of illumination, save energy and minimize wildlife disruption.	yes	The Three Sisters Village Land Use Overlay establishes a lighting zone 0 permitting architectural and decorative lighting that aligns with dark sky standards.
6.6.2	Pathways, trails, walkways and other recreation focused amenities within the Plan Area may be lit. Techniques to ensure sensory disturbance to wildlife is minimized may include: a. Downward casting, full cut-off, exterior lighting; b. Lower wattage, timers, dimmers and motion sensors on exterior lighting; or c. Buildings should incorporate blinds or curtains on windows facing the wildlife corridor.	yes	Lighting would align with dark sky standards to reduce light pollution and sensory disturbance.
6.6.3	Landscaping on parcels on the periphery of the Plan Area should incorporate light screening techniques and should be designed to limit light from penetrating the Provincially approved Wildlife Corridor.	yes	Lighting would align with dark sky standards and incorporate light screening to reduce light pollution.
7	Transportation		
7.1	Transportation - General Policy		
7.1.1	A complete street network within the Plan Area should be accommodated in accordance with Map 13.	yes	The Phase 1 street network aligns with Map 13 and identified a complete street network in alignment with the EDCC and discussions at the ASP stage.
7.1.2	Street classifications will be determined at Conceptual Scheme in accordance with a Mobility Assessment, predicted trip generation and traffic volumes.	yes	The mobility assessment has identified two (2) street classifications: Local Residential and Collector. Please refer to Mobility Assessment, <b>DRAWING 9</b> , and <b>DRAWING 10</b> .
7.1.3	Where the complete street network or multi-use pathways cannot meet accessibility guidelines for all ages and abilities, alternative transportation linkages will be provided to that location within the Plan Area.	yes	Pathway grades have been planned to align with the EDCC requirements. The Collector Road A facilitates the primary connection through the Three Sisters Village area that meets sustainable transportation standards.



Item	Goals, Objectives & Policies	Alignment	How?
7.1.4	Collaboration between the Town and Three Sisters Mountain Village will be required to develop strategies and initiatives to jointly meet the multi-modal targets in alignment with Section 11.3.		The mobility assessment has identified two (2) street classifications: Local Residential and Collector. Please refer to Mobility Assessment, <b>DRAWING 9</b> , and <b>DRAWING 10</b> . Section 11.3 identifies a Transportation Monitoring Program which is proposed within the Mobility Assessment. No recommendations for off-site improvements are being proposed within the Mobility Assessment.
7.1.5	Intercept vehicle and destination bicycle parking areas should be provided at appropriate locations to efficiently serve but not over-park commercial districts within the Plan Area in alignment with Section 11.3.	n/a	Phase 1 is a residential area. Intercept vehicle parking does not occur within this phase and bicycle rack placement will be addressed at the detailed design and development permit stages as necessary and required.
7.1.6	If above-grade parking structures are proposed, they should be designed to integrate with the building and obscure the parking structure by using architectural treatments that are similar to the building facade.	n/a	This is a Development Permit item and does not apply to this Phase 1 Conceptual Scheme submission.
7.1.7	Snow storage will be accommodated primarily in landscaped areas, boulevards, and furnishing zones within the rights-of-way. Additional areas for snow storage shall be considered at the Conceptual Scheme stage within the planning process.	yes	Snow storage will be accommodated within municipal right-of-ways.
7.2	Sustainable Transportation Network		
7.2.1	Pedestrian connections shall be accommodated through a complete street and multi-use pathway network conceptually shown in Map 14.	yes	Phase 1 pedestrian connections align with Map 14. Refer to <b>DRAWING 11</b> .
7.2.2	Bicycle connections shall be accommodated through a complete street and multi-use pathway network conceptually shown in Map 15.	yes	Phase 1 bicycle connections align with Map 14. Refer to <b>DRAWING 11</b> .
7.2.3	Regional and offsite bicycle and multi-use pathway connections should be accommodated as conceptually shown in Map 16.	yes	Phase 1 integrates with existing regional and offsite bicycle and multi-use pathway connections.
7.2.4	A transit route and stops within the Plan Area should generally be accommodated as conceptually shown in Map 17. Transit routing and stop locations will be finalized at Conceptual Scheme.	yes	A transit route and stops align with Map 17 of the ASP and in alignment with the Phase 1 Mobility Assessment. Temporary cur-de-sacs have been provided at the termination of the Phase 1 route to provide adequate space for Transit to turn around if service is initiated before the completion of TSV ASP area in its entirety.
7.2.5	Multi-modal connections shall be accommodated through the overall transportation network as conceptually shown on Map 18.	yes	Please refer to <b>DRAWING 11</b> .
7.2.6	The commuter bicycle network will be integrated through active transport zones within the complete street network and on multi-use pathways to facilitate Plan Area and offsite connections.	yes	Please refer to <b>DRAWING 11</b> .
7.2.7	Private shuttle services may be operated by tourism and hotel partners to respond to evolving transportation demands for visitors within Three Sisters Village.	n/a	Not applicable to Phase 1.
7.2.8	Rideshare programs will be enabled to operate within the Plan Area.	yes	The transportation network enables ride share. This would be detailed during Phase 3, 4 and 5.
7.2.9	Missing links and improvements outside of the Plan Area within the multi-use pathway network shall be addressed through a combination of Town directed improvements and Developer initiated off-site improvements in accordance with Section 11.3.	yes	No Town or Developer off-site improvements are identified within the Phase 1 Mobility Assessment.



<i>Item</i>	<i>Goals, Objectives &amp; Policies</i>	<i>Alignment</i>	<i>How?</i>
7.2.10	Wayfinding and Information system will be provided consistent with Town of Canmore standards to guide visitors to major destinations through street, multi-use pathway and trail networks.	yes	Three Sisters Village has an established wayfinding system for pathways, expansion and improvements to this wayfinding will comply with Town signage standards. Signage and wayfinding will be detailed at the detailed design stage or the development permit stage.
8	Housing		
8.1	Affordable Housing		
8.1.1	A minimum of 10% of all residential units, regardless of tenure, within Three Sisters Village shall be provided as affordable housing. Residential units for the purpose of this policy, excludes tourist homes, visitor accommodation and employee housing.	yes	This policy is implemented within the Three Sisters Village Overlay requiring all developments provide 10% of their units to be affordable.
8.1.2	At Development Permit submission, projects shall be incentivized for affordable housing contributions through the bonusing system outlined in Section 5.8 of this Plan.	yes	The TSV Bonusing Overlay compensates the developer for the minimum 10% of affordable housing and then incentivizes developers to provide additional affordable housing beyond the 10%. Compensation for the 10% is provided at a lower rate than housing provided above the 10% requirement.
8.1.3	Affordable housing units may be provided outside of the Three Sisters Village Plan Area to the mutual satisfaction of the Town and the Applicant	n/a	To be determined at Development Permit submission
8.1.4	Affordable housing units should meet the Town of Canmore's Affordable Housing Guidelines.	yes	To be determined at Development Permit submission
8.1.5	At the sole discretion of the Town of Canmore, builders may provide development lands or cash-in-lieu for affordable housing units.	n/a	Assessment of development lands will occur at the Development Permit stage
8.1.6	Affordable housing units must be acquired by CCH or another agency within 12 months from building occupancy otherwise, the Developer is able to market the unit for sale at market prices	n/a	To be implemented at Development Permit submission and beyond.
8.2	Market Rental		
8.2.1	Concurrent processing of Land Use Redesignation and Development Permit applications to expediate the approvals process for market rental developments may be undertaken.	n/a	Land Use Redesignation for Phase 1 will happen concurrently with the Conceptual Scheme.
8.2.2	Commitments related to increasing the supply of rental housing within the Plan Area are outlined in Section 11.4.	yes	The Conceptual Scheme aligns with section 11.4 given Three Sisters Village must meet the Employee Housing commitment prior to Phase 3.
8.3	Employee Housing		
8.3.1	For visitor accommodation uses within the Plan Area, shall be required to provide the following employee housing units: a. For visitor accommodation developments < 60 units: 1 bedroom / 10 units; b. For visitor accommodation developments 60 - 100 units: 1 bedroom / 8 units; and c. For visitor accommodation developments > 100 units: 1 bedroom / 6 units.	n/a	To be determined at Development Permit submission for Visitor Accommodation



<b>Item</b>	<b>Goals, Objectives &amp; Policies</b>	<b>Alignment</b>	<b>How?</b>
8.3.2	A maximum of 6 employee housing bedrooms with a shared access bathroom and kitchen facilities will be considered the equivalent of one unit	n/a	To be determined at Development Permit submission
8.3.3	Commitments related to increasing the supply of employee housing within the Plan Area are outlined in Section 11.4.	yes	The Conceptual Scheme aligns with section 11.4
8.3.4	Any future employee housing requirements adopted by Council that applies uniformly to all new commercial development throughout the Town shall supersede other requirements contained within this ASP.	yes	The Town has not adopted employee housing requirements that meet this policy
<b>8.4</b>			
<b>Entry-Level Housing</b>			
8.4.1	Strategies to meet the ELH requirements and achieve attainable market housing may include: a. Focus on missing building forms like townhomes, stacked townhomes and apartments; b. Providing a variety of unit floor areas to accommodate both smaller and larger households and families based on the number of bedrooms provided; c. Modest amenities that reduce costs of condominium or maintenance fees and address basic living needs; d. Shared facilities such as laundry, indoor workspaces and parking to achieve maximum efficiency of space; e. Buildings designed with standardized unit construction and modest quality finishes; f. Maintaining mountain town architectural character through materiality; and g. Low maintenance, naturalized landscaping materials and plants that do not require irrigation.	yes	Three Sisters Village Phase 1 through the adopted of the proposed land use districts enables the creation of smaller built forms like townhomes, stacked townhomes and apartments in a variety of sizes. Other elements listed must be assessed at the time of Development Permit submission.
8.4.2	Smaller more compact units can be more attainable in a free market setting. Units may be encouraged to be within the following size ranges: a. Apartments: a total gross floor area in the range of 28m <sup>2</sup> to 93m <sup>2</sup> ; b. Stacked Townhomes: a total gross floor area in the range of 83 m <sup>2</sup> to 149 m <sup>2</sup> ; and c. Townhomes: a total gross floor area in the range of 139 m <sup>2</sup> to 177 m <sup>2</sup> .	n/a	Three Sisters Village Phase 1 through the adopted of the proposed land use districts enables the creation of smaller built forms like townhomes, stacked townhomes and apartments in a variety of sizes. Other elements listed must be assessed at the time of Development Permit submission.
<b>8.5</b>			
<b>Accessory Dwelling Units</b>			
8.5.1	ADUs shall be enabled in all semi-detached and townhouse units in Three Sisters Village.	yes	ADU's are a permitted use in the proposed R3 TSMV and R4 TSMV Districts.



<i>Item</i>	<i>Goals, Objectives &amp; Policies</i>	<i>Alignment</i>	<i>How?</i>
8.5.2	Where an ADU is allowed and not constructed at the initial development permit stage, dwellings are encouraged to be constructed as 'suite ready'. The minimum 'suite ready' requirements include: a. A minimum ceiling height of 1.95 metres; b. A direct exit from the secondary suites to the outdoors with the ability to provide lockable fire rated doors between the primary dwelling and the secondary suite; c. Fire protected walls and ceiling between the secondary suite and the main dwelling unit; d. Configured to accommodate independent heating and ventilation systems; and e. Roughed in utility connections, such as plumbing and electrical for future installation of a kitchen and washrooms.	n/a	Policy will be complied with at the time of Development Permit approval.
8.5.3	No additional parking will be required for units with an ADU, intent if located within 350 metres of a transit stop.	n/a	Policy will be complied with at the time of Development Permit approval.
9	Utility Infrastructure		
9.1	Utility Infrastructure - General Policy		
9.1.1	Development within the Plan Area shall be serviced with municipal water, sanitary sewer and stormwater utilities	yes	Phase 1 municipal water, sanitary sewer and stormwater utilities are in alignment with the ASP.
9.1.2	The alignment and capacity of water distribution and feeder mains, sanitary sewer feeder mains and trunks shall be based on utility servicing required for Three Sisters Village.	yes	See Three Sisters Village Servicing Report.
9.1.3	Utility rights-of-way, easements and public utility lots shall be provided to accommodate municipal utilities, where appropriate and in accordance with Section 662 of the Municipal Government Act (MGA)	yes	Utility rights-of-way, easements and public utility lots have been noted on the Phase 1 Conceptual Scheme Plan and <b>DRAWING 18</b> . These can be refined at the time of subdivision and detailed design.
9.1.4	Utility and road rights-of-way, easements and public utility lots may extend past the boundary of a current Subdivision application area into other areas of the ASP.	n/a	Utility rights-of-way, easements and public utility lots have been noted on the Phase 1 Conceptual Scheme Plan and <b>DRAWING 18</b> . These can be refined at the time of subdivision and detailed design.
9.1.5	Utility line assignments will be determined at Subdivision and should generally conform to the road network to minimize land disturbance.	n/a	To occur at detailed design stage and it will utilize the proposed road network, rights-of-way or easements as much as possible.
9.2	Water Distribution		
9.2.1	The Plan Area shall be serviced with an approved potable water service. Map 19 shows the conceptual layout of the major components of the water distribution system, which generally follows the proposed road network.	yes	Potable water service has been designed in alignment with <b>DRAWING 16</b> .



<i>Item</i>	<i>Goals, Objectives &amp; Policies</i>	<i>Alignment</i>	<i>How?</i>
9.2.2	More detailed analysis to confirm the estimate water demand requirements shall be undertaken at the Subdivision application stage. This analysis will consider the size and capacity of the water distribution system, Pressure Reducing Valve (PRV) and booster pump locations and any required upgrades to the Town of Canmore's water supply and distribution system. Some upgrades should be accommodated through the use of off-site lewies.	n/a	Detailed analysis will occur at subdivision stage.
9.3	Sanitary Sewer		
9.3.1	Major components of the sanitary sewer system are shown conceptually on Map 20.  Development within the Plan Area should be connected to the municipal sanitary sewer system. a. The Resort Recreation Amenity Area and other parks and open spaces may determine on a case by case basis whether municipal service connections are required.  A more detailed analysis will be undertaken at Subdivision application stage to confirm the estimated sewage flow, size and capacity of the proposed sanitary sewer system and any required upgrades to the Town's existing sewer infrastructure. Some upgrades should be accommodated through the use of off-site lewies.	yes	Sanitary sewer has been designed in alignment with <b>DRAWING 16</b> .  Phase 1 will tie in to the sanitary sewer through the Three Sisters Parkway.  Detailed analysis will occur at subdivision stage.
9.4	Stormwater Management		
9.4.1	Developments within the Plan Area must be served by a stormwater management system including on-site facilities as needed.	yes	Conceptual Scheme area will be serviced by a stormwater management system consisting of stormwater gravity mains located underneath public roadways and a storm pond facility designed to treat and retain storm water.
9.4.2	Major components of the stormwater management system are shown conceptually on Map 21. The system will be comprised of gravity mains and wet or dry ponds, as well as emergency overland drainage courses that should be naturalized and integrated into the open spaces.	yes	Storm gravity mains will be located underneath roadways and in public spaced as needed to convey stormwater. Overland drainage courses will be designed to handle and direct storm water run off towards dedicated storm water pond.
9.4.3	Stormwater ponds should be designed to accommodate snow storage in winter months.	yes	Stormwater pond will be designed and built with a gravel access driveway for maintenance. The maintenance driveway can be used to access the storm pond site in winter months for additional snow storage.
9.4.4	As part of the preparation of a Stormwater Management Plan, stormwater quality and quantity enhancements shall assess potential stormwater infiltration and/or re-use, if such applications are permitted by the Province.	yes	At the subdivision application stage, a more detailed analysis will investigate the feasibility of stormwater infiltration and reuse.



<b>Item</b>	<b>Goals, Objectives &amp; Policies</b>	<b>Alignment</b>	<b>How?</b>
9.4.5	Stormwater management systems should strive to treat stormwater runoff at the source.	yes	We propose LID techniques be applied at individual parcels through the Bonusing Overlay proposed by the development as it encourages their implementation by allowing higher density to the developer. We recognize that the subalpine forest environment is not the most desirable environment for the implementation of large-scale LID techniques in the public realm domain. Implementing LID in public realm may not be the most cost-effective solution for the treatment of stormwater for the Town of Canmore as it may require frequent maintenance due to poor soils and sub-optimal climate/environment.
9.4.6	A more detailed analysis will be undertaken at Conceptual Scheme to align the stormwater management system with the general direction of the Canmore Stormwater Master Plan and the Three Sisters Village Master Drainage Plan.	yes	A dry pond design has been provided in alignment with the Three Sisters Stormwater Master Drainage Plan. TSV Master Drainage Plan talks about the stormwater strategy for the entire development and the Conceptual Scheme is in alignment with the Master Drainage Plan.
9.4.7	Stormwater management analysis at Conceptual Scheme will include the final configuration of the stormwater infrastructure, as well as locations and widths of the overland drainage easements.	yes	Stormwater infrastructure shown on the site servicing figure includes the final location of the proposed stormwater gravity mains, overland drainage courses and stormwater ponds.
9.4.8	The design of the stormwater management system shall recognize the subalpine forest environment of the Plan Area and will account for freeze-thaw cycles and demonstrate the site can be appropriately serviced.	yes	The proposed stormwater management system does consider the existing topography and geography of the region to appropriately service the development. Subsequent Stormwater Management Plans at the subdivision stage will further investigate the feasibility of using stormwater infiltration.
9.4.9	The stormwater management system shall introduce mitigation measures where deemed appropriate to address the potential impact of water quality within existing watercourses.	yes	TSV aims to improve existing water quality through implementation of LID, through bonusing, as source control measure, bio-retention, rain gardens, etc., on individual parcels. This is laid out as part of the Bonusing strategy and targeting certain mm of rain capture of a storm event.
9.4.10	Oil/grit separators may be required within private developments at the Development Permit stage where stormwater cannot be addressed by other means.	yes	The use of naturalized methods for water quality is highly encouraged by the development, but oil and grit separators may be permitted on a case-by-case basis.
9.4.11	Encourage the development of stormwater facilities with a preference for source controls as opposed to end-of-pipe solutions.	yes	The development encourages and promotes the use of bioswales and infiltration facilities on an on-site level, i.e. at DP submission.
9.4.12	Naturalized methods, such as natural wetlands and bioswales, are encouraged to mitigate the effects of stormwater run-off into watercourses.	yes	The development will encourage the use of naturalized methods of retaining rainwater and enhancing water quality, by encouraging low impact development techniques, at development permit through the Bonusing Overlay.
9.5	Shallow Utilities		
9.5.1	The location of all shallow utilities and the provision of rights-of-way and easements shall be addressed to the mutual satisfaction of the Town of Canmore, the landowner and the utility companies	yes	Shallow utility infrastructure is available along Three Sisters Parkway. Further coordination will occur with ATCO, Fortis, Telus and Shaw at subdivision stage. To be provided at the detailed design stage.



<i>Item</i>	<i>Goals, Objectives &amp; Policies</i>	<i>Alignment</i>	<i>How?</i>
9.5.2	Common trench design will be the preferred approach when working with shallow utility providers.	yes	Electrical, communications and natural gas will be allocated within a shallow trench and installed within boulevards, right of ways and easements. To be provided at the detailed design stage
10	Environment & Sustainability		
10.1	Climate Change Adaptation and Mitigation		See Bonusing Overlay.
10.2	Sustainable Community Design		Conceptual Scheme is in alignment with the ASP Concept and is the first phase that implements sustainable community design principles into the development of the overall community.
10.3	Sustainable Site and Building Design		
10.3.1	Encourage energy efficient building construction for multi-residential and commercial development through the provision of density bonuses or other incentives for energy efficient construction beyond Alberta Building Code, as outlined in the Bonusing Toolkit (Section 5.8).	yes	Incentive has been provided for energy efficient building construction through use of the Bonusing Overlay.
10.3.2	Exemptions or relaxations from Canmore's Mountain Architectural Guidelines in the Land Use Bylaw may be considered if they inhibit building performance as it relates to more efficient and sustainable building standards	yes	The Conceptual Scheme has requested exemptions from sections 10, section 11 and section 12 of the Land Use Bylaw as they inhibit building performance. The request is in alignment with this policy.
10.3.3	At Development Permit, all residential buildings should be constructed to be "solar ready."	n/a	Solar readiness will be addressed at the Development Permit stage.
10.3.4	Multi-residential developments should be encouraged to incorporate some shared parking stalls with electric vehicle charging stations in both residential and commercial parking areas.	yes	Provision of electric vehicle charging stations has been encouraged through use of the Bonusing Overlay which incentivizes their inclusion.
10.3.5	Encourage green roofs, purple pipe rainwater reuse systems and LIDs where appropriate and when building and plumbing codes permit.	yes	Incentive has been provided through the use of the Bonusing Overlay.
10.3.6	Avoid multiple plantings of the same species (monocultures) to prevent the potential spread of disease and to promote biodiversity.	n/a	Landscaping will be provided at detailed design and Development Permit stage.
10.3.7	On-site recycling and organics collection services and facilities shall be provided.	n/a	To be detailed at the time of Development Permit submission.





<i>Item</i>	<i>Goals, Objectives &amp; Policies</i>	<i>Alignment</i>	<i>How?</i>
10.3.8	Rideshare programs will be enabled to operate in the Three Sisters Village area. Public street parking spaces will be allocated for rideshare programs within the Plan Area.	yes	Phase 1 provides street parking within the street. Rideshare can be accommodated. Rideshare would be addressed in more detail in discussions surrounding the commercial phases of development.
10.3.9	Landscapes should be designed utilizing native plant material for low requirements for watering and energy used for maintenance purposes.	n/a	Landscaping will be provided at detail design and Development Permit stage.
10.3.10	Where feasible in Municipal Reserve, Environmental Reserve or private recreation areas, consideration shall be given to retention of existing trees and protected during construction. It is acknowledged that lodgepole pine stands may not be suitable due to blowdown and FireSmart considerations	yes	Programming of the municipal reserve has been designed to incorporate and work with the existing environment. The tree fort is a design that can work with the existing tree cover. The slope adaptive playground can also use existing grades to retain the mountainous landscape.
10.3.11	Informal naturalistic planting should be used adjacent to wildlife corridors, habitat patches, riparian corridors and streetscapes with naturalized plant schemes.	yes	Detailed landscape planning for municipal reserve will be supplied at the time of subdivision during the detailed design stage. Landscaping will be proposed in alignment with required steep creek mitigation and other constraints identified for the site.
10.3.12	WildSmart and FireSmart planting principles shall be incorporated.	yes	Please see Wildfire Risk Assessment.
10.3.13	For programmed outdoor spaces, both sunny and shaded areas should be provided.	yes	A comprehensive open space plan for Phase 1 has been developed and is included in the Conceptual Scheme package
10.3.14	Plant material, berms and hard landscape elements should be used to offer privacy screening between residents as well as to screen views to service areas, surface parking, parking structures and utilities.	n/a	Landscaping plans will be submitted at detailed design and the development permit stage.
10.3.15	Site variation and grading required to achieve slope-adaptive development and architecture should be taken up with terraced landscaping, utilize retaining walls with planted areas or through the use of planted beaming.	n/a	Open space incorporates plans to use and retain existing natural features. Requirements to achieve slope-adaptive development and architecture will be fulfilled at Development Permit stage and will include grading and landscaping plans.
10.4	<b>Wildlife and Human-Use Management</b>		
10.4.1	Provincial wildlife movement corridors are identified on Map 5	n/a	Acknowledged
10.4.2	The Province is responsible to maintain and manage wildlife corridors, including any necessary enforcement.	n/a	Acknowledged
10.4.3	The requirements and responsibilities for long-term management of the wildlife corridors adjacent to the Plan Area shall be governed by the relevant provisions and definitions of the Wildlife Act and/or other relevant provincial regulatory documents.		Acknowledged
10.4.4	The provincial wildlife corridor decision shall be respected as outlined in Appendix A.1.	n/a	Acknowledged
10.4.5	It shall be the responsibility of the applicant to ensure compliance with all provincial requirements and seek the appropriate approvals.	yes	All necessary approvals will be obtained.



<i>Item</i>	<i>Goals, Objectives &amp; Policies</i>	<i>Alignment</i>	<i>How?</i>
10.4.6	The primary mitigations outlined within the EIS are: a. Education; b. Attractant management; c. Creating a space for recreation within the Plan Area; and d. A wildlife fence.	yes	Acknowledged
10.4.7	To address uncertainty in the efficacy of the mitigations outlined within the Three Sisters Village EIS, a Monitoring and Adaptive Management Plan is required. Further details are provided in Section 11.1.		See Monitoring and Adaptive Management Plan. The Province has accepted this plan to the extent of their Provincial jurisdiction and their governance of the wildlife corridor.
10.4.8	Monitoring will be used to determine whether any adaptation to the mitigation is required.		See Monitoring and Adaptive Management Plan. The Province will lead this work with TSMVPL.
10.4.9	The Town and Three Sisters Mountain Village shall continue to work with adjacent jurisdictions and government agencies to cooperatively protect and maintain the functionality of wildlife corridors (eg. monitoring, enforcement and education initiatives).		The Province is the sole authority on wildlife corridors. TSMVPL and the Town will work together with the Province as required by the Monitoring and Adaptive Management Plan and with regards to their commitments to wildlife / human interaction education.
10.4.10	Three Sisters Mountain Village or their designate, along with the Province and the Town of Canmore will work together to address issues related to Wildlife and Human-Use Management regarding wildlife corridors as outlined in Appendix A.1.		The Province will take a lead on the Monitoring and Adaptive Management Plan compliance using the Monitoring and Adaptive Management Plan (MAMP) and the Fence Design Report. TSMVPL has commenced monitoring and will execute the plan as required. Any mitigations will be discussed with the Province. The Province will work with TSMVPL and the Town when required.
10.4.11	Three Sisters Mountain Village or their designate, shall participate as a member of the Technical Working Group for Improving Human-Wildlife Coexistence in the Bow Valley to develop and support initiatives regarding wildlife conflict and co-existence (or similar).		The Province has taken a lead role on this matter and does not require TSMVPL membership on this committee. The Province will lead when there is a matter to discuss that involved TSMVPL.
10.4.12	The Town and Three Sisters Mountain Village will work together to update attractant management bylaws if and when required.		An update is not currently identified or required.
10.4.13	The Town of Canmore shall work with relevant stakeholders, including the Developer and residents, to apply and enforce attractant management techniques as identified in the Three Sisters Mountain Village EIS and/or Town bylaws applicable throughout Canmore.		The Town is continuing to look for ways to improve attractant management. The Monitoring and Adaptive Management Plan will identify any further actions that would be required.
10.4.14	Builders and businesses operating within the Plan Area should incorporate Bow Valley WildSmart conservation strategies into their operating guidelines to educate employees and visitors within the Plan Area	yes	TSMV aligns to Bow Valley WildSmart and is incorporating these principles and education items within their development. Signage and other elements will be proposed at the detailed design stage.
10.5	Wildlife Fence		
10.5.1	A wildlife fence shall be erected along the perimeter of the Plan Area and east through Stewart Creek Golf Course to the west side of the Stewart Creek Across Valley Corridor in accordance with the Three Sisters Village EIS.	yes	See Fence Design Report and <b>DRAWING 2</b> for the proposed wildlife exclusion fence.



<b>Item</b>	<b>Goals, Objectives &amp; Policies</b>	<b>Alignment</b>	<b>How?</b>
10.5.2	The Town of Canmore shall maintain the wildlife fence. Timing of when the Town will take over the management of this piece of infrastructure is addressed in Section 11.1.		The Province will maintain the wildlife fence.
10.5.3	Access points to Provincially designated trails above the Along Valley Wildlife Corridor may be facilitated through clearly designated and Provincially approved gated entry points. Gates and signs will be used to demarcate the corridor at access point locations to the sole satisfaction of the Province.	yes	The Province has accepted the Fence Design Report and the Monitoring and Adaptive Management Plan.
10.5.4	Adaptive management will be implemented if monitoring of mitigation indicates that changes to proposed mitigation outlined within the Three Sisters Village EIS are required. Adjustments to future mitigation that may be applied, if required, at future planning stages or development phases include: a. Working with the Province to close trails within the wildlife corridor; b. Working with the Province and Town to increase enforcement or change the methodology of how enforcement is conducted; c. Adjustments to design elements that could influence sensory disturbance (e.g. lighting, noise reduction and operating hours); d. Working with the Town to make adjustments to the recreational amenities offered within the Plan Area; e. Working with the Province, Town and other relevant agencies to increase and/or updating wildlife education methods and materials; and f. Adjusting the fence design.	yes	See Monitoring and Adaptive Management Plan. The province has accepted the Monitoring and Adaptive Management Plan.
10.5.5	Adaptive Management does not apply to the overall unit count, conceptual road network and building forms but may apply to the reorganization of these elements within the Plan Area without requiring an ASP amendment. Some anchor elements like the Village Centre and the Innovation District capitalize on topographic and geological features and should remain where proposed.	Yes	See Monitoring and Adaptive Management Plan.
10.5.6	After all other adaptive management options are exhausted, if the fence proves to be an ineffective mitigation for the effects of the development as described in the Three Sisters Village EIS then, the location and/or use of the wildlife fence as a mitigation may be reconsidered. a. If movement or removal of the wildlife fence is required, the Developer shall be responsible for the cost; and b. If movement of the fence is required, any gross acres of additional lands rendered undevelopable as a result of fence movement shall be subject to Section 664.2 of the MGA.	yes	See Fence Report. Province is now responsible for the fence. This is Phase 1 and given it is in the early stages of the CS application, adaptive management has yet to be employed.



<i>Item</i>	<i>Goals, Objectives &amp; Policies</i>	<i>Alignment</i>	<i>How?</i>
10.6	Preserve and Manage Water Resources		
10.6.1	Use the Provincial guidelines "Stepping Back from the Water: A Beneficial Management Practices Guide for New Development Near Water Bodies in Alberta's Settled Region" as a guideline for the identification of retained wetlands and riparian areas and development of management options to determine waterbody setback distances.	yes	Provincial guidelines and Canmore Bylaws have been used for the creek setbacks.
10.6.2	Where a natural wetland will not be retained, compensation will occur in accordance with the Water Act and the Alberta Wetland Policy.	yes	Water Act approvals will proceed at the time of subdivision approval.
11	Implementation		
11.1	Environmental Impact Statement		
11.1.1	The wildlife fence associated with the Plan Area will be erected in its entirety as outlined within the Three Sisters Village EIS. a. The wildlife fence construction shall be initiated at the time of construction of the first Phase of development within the Plan Area; b. A professional wildlife biologist will undertake the final design of the wildlife fence, including design at creek crossings, fence ends, outline performance parameters (construction and performance as a piece within a suite of mitigations) and evaluate the design to confirm that it is consistent with the principles described in the EIS; and c. The wildlife fence design will balance ASP objectives when considering the design of fence gates along multi-use pathways (e.g. sustainable transportation).	yes	See Fence Report and <b>DRAWING 2</b> of the Wildlife Exclusion Fence for phase 1 alignment.
11.1.2	The Monitoring and Adaptive Management Plan for Three Sisters Mountain Village shall be developed prior to the first Conceptual Scheme approval in accordance with the Three Sisters Village EIS in accordance with the following parameters: a. The Monitoring and Adaptive Management Plan shall focus on the potential effects of the project; b. The Adaptive Management Plan creates a mechanism to evaluate if and when an additional action may be required in response to monitoring; c. Monitoring shall evaluate the performance of the mitigations as outlined in the Three Sisters Village EIS; d. The Adaptive Management Plan will set thresholds for performance of the mitigations and include setting performance parameters for the wildlife fence; and e. The thresholds defined to represent mitigation efficacy shall be drawn from the conclusions of the Three Sisters Village EIS	yes	See the Monitoring and Adaptive Management Plan submitted under separate cover.



<i>Item</i>	<i>Goals, Objectives &amp; Policies</i>	<i>Alignment</i>	<i>How?</i>
11.1.3	The Town of Canmore shall take over maintenance of the wildlife fence when the performance parameters outlined within the Monitoring and Adaptive Management Plan are met or when the last area of land is subdivided and transferred to the Town through the Construction Completion Certificate (CCC) and Final Acceptance Certificate (FAC) process at subdivision.		The Province will be taking on this responsibility.
11.1.4	At subdivision, a mechanism to provide the Town of Canmore access to maintain the fence shall be determined.	n/a	The Province will be taking on this responsibility.
11.1.5	Prior to the first Conceptual Scheme approval, the Town, Province and Three Sisters Mountain Village shall establish a mechanism to coordinate cross-jurisdictional matters regarding the implementation of the mitigations defined in the Three Sisters Village EIS as discussed in Appendix A.1.		The Province will be taking on a leadership role as to when coordination meetings are required between the Town, the Province and TSMVPL.
11.1.6	Coordination of cross-jurisdictional matters between the province, the Town of Canmore and Three Sisters Mountain Village will focus on: <ul style="list-style-type: none"> <li>a. The design and construction of the wildlife fence;</li> <li>b. Any proposed adaptations in response to monitoring data;</li> <li>c. Enforcement;</li> <li>d. Establishment of new trails, location of gates, closing of unsanctioned trails or setting new restrictions within the wildlife corridor;</li> <li>e. Matters related to human-wildlife co-existence; and</li> <li>f. Consider input from the Technical Working Group for Improving Human-Wildlife Co-existence (or similar).</li> </ul>		The Province will be taking on a leadership role as to when coordination meetings are required between the Town, the Province and TSMVPL.
11.1.7	The Adaptive Management Plan and any proposed adaptations shall consider the multi-modal objectives for the Plan Area prior to closure of any key linkages from this Plan to other areas of Canmore.	n/a	See the Monitoring and Adaptive Management Plan and Fence Design Report. Both align with this principle.
11.1.8	Monitoring shall commence prior to construction within the first Conceptual Scheme area. Monitoring will conclude when the last area of land is subdivided and the Developer has incorporated any adaptations that may be required to the sole satisfaction of the Province.	Yes	See the Monitoring and Adaptive Management Plan and Fence Design Report. Both align with this principle. Monitoring program commenced in September 2022.
11.1.9	After development completion, the Province or the Town may choose to continue monitoring wildlife corridors at their discretion.	n/a	See the Monitoring and Adaptive Management Plan
11.1.10	The Town shall work with the Province to undertake an education and enforcement campaign over the first five years that the fence is in place. This will maximize efficacy of fencing and education in achieving compliance with trail use, off-leash dog use and seasonal closure regulations within wildlife corridors. Three Sisters Mountain Village will contribute to education efforts.		The Province and TSMV are working together on the implementation of the fence and the Monitoring and Adaptive Management Plan. Educational campaigns will be developed with the Province. The Province will be managing the wildlife corridor and the fence.
11.2	Conceptual Scheme and Land Use Amendments		



<i>Item</i>	<i>Goals, Objectives &amp; Policies</i>	<i>Alignment</i>	<i>How?</i>
11.2.1	The maps and policy described in this Plan shall be affected by amendment to the Town of Canmore Land Use Bylaw through the redesignation of the lands.	yes	The conceptual scheme is being submitted for concurrent land use redesignation.
11.2.2	The policy contained within this document shall supersede any interpretation derived from graphic images (i.e. photos, illustrations and renderings) within this document. Images are meant to be illustrative only.	yes	Images are intended for conceptual purposes and reflect the vision of the conceptual scheme. After subdivision approval, detailed design drawings will be developed. Development permits will be submitted to determine architecture and building placement for each multi-residential parcel.
11.2.3	Conceptual Scheme and Land Use Redesignations will be done concurrently.	yes	A conceptual scheme and land use redesignation is being submitted concurrently.
11.2.4	Conceptual Scheme areas should generally align to the phrases identified in Map 22, unless otherwise specified in policy.	yes	Phase 1 aligns with Map 22 of the ASP.
11.2.5	Standard land use districts contained within Canmore's Land Use Bylaw should be used where appropriate.	yes	TSV proposes two (2) new standard land use districts to be adopted into the Canmore Land Use Bylaw.
11.2.6	Direct Control Land Use districts should only be used in accordance with the rules outlined within Canmore's Land Use Bylaw	yes	TSV proposes two (2) new standard land use districts to be adopted into the Canmore Land Use Bylaw.
11.2.7	Future land use redesignations within the Plan Area shall be made with respect to the direction outlined within this ASP. The direction contained within this ASP shall be the primary consideration with respect to future planning approvals.	yes	TSV Conceptual Scheme and land use redesignation application aligns to the policy within the ASP.
11.2.8	Conceptual Schemes shall provide Land Use approval for approximately 5 years of proposed buildout and can contain multiple phases intended for Subdivision	yes	The proposed CS area encompasses a 3 year land supply.
11.2.9	Subdivision applications should account for approximately 3 years of developable land supply	yes	The proposed CS area encompasses a 3 year land supply.
11.3	Transportation		
11.3.1	The Town of Canmore shall establish a program for data collection and traffic monitoring to assess if travel behavior and traffic volumes are aligned with long term mobility objectives.	yes	Refer to mobility assessment where one is described and proposed
11.3.2	The Town of Canmore will work with Three Sisters Mountain Village to determine the transportation monitoring program associated for the development at the time of Conceptual Scheme.	yes	Refer to mobility assessment where one is described and proposed
11.3.3	Three Sisters Mountain Village shall fund the capital cost of a maximum of three monitoring stations related to the development and the Town will operate the program. Initial contribution of the capital is expected in 2021 with further investment as roads and intersections are completed in future phases	yes	Refer to mobility assessment where one is described and proposed
11.3.4	The Town of Canmore shall be responsible for operational maintenance of a complete street and multi-use pathway network in a manner that will encourage year-round walking, biking and transit use.	yes	Roads and Municipal Reserve and pathways that are within the Resort Recreation Amenity Area (RRAA) via a public access easement will be released to the Town during the normal course of CCC and FAC.



<b>Item</b>	<b>Goals, Objectives &amp; Policies</b>	<b>Alignment</b>	<b>How?</b>
11.3.5	The Developer shall provide a Mobility Assessment at each Conceptual Scheme phase to assess existing multi-modal performance within the Plan Area against the intended targets within the TIA. A single Mobility Assessment should be considered for Phase 2a and 2b.	Yes	Refer to mobility assessment
11.3.6	Mobility Assessments should demonstrate the following: a. Predicted road volumes; b. Provide an assessment on how the development is performing against the intended modal-split objectives; c. On-site and off-site infrastructure, transit frequency and service and initiatives needed to support modal-shift and level of service targets; d. Evaluate triggers and timing for off-site infrastructure improvements; and e. Outline the responsibility for costs mechanisms for collecting those costs and implementation.	Yes	Refer to mobility assessment
11.3.7	Where a Mobility Assessment indicates that: a. The development is performing as predicted or better, the predicted infrastructure within the TIA may be delayed; or b. The development is not performing as intended (i.e. level of service and capacity) the need for infrastructure improvements or other mitigations may need to be completed ahead of schedule or new mitigations applied.	n/a	Development must be built out to align to this policy.
11.3.8	The recommendation of the Mobility Assessment shall be undertaken in a manner that proactively ensures that the level of service and capacity targets are met.	Yes	No improvements identified for Phase 1 within the Mobility Assessment.
11.3.9	The Mobility Assessment may adjust the mode share targets based on the Conceptual Scheme phase, the land uses proposed, the progress of the development and data collected.	Yes	No improvements identified for Phase 1 within the Mobility Assessment.
11.3.10	Infrastructure improvements predicted for Three Sisters Mountain Village and the Developer's and Town's contributions to those improvements are identified within the TIA in Appendix C.1. A map is provided in Appendix A.4.		No improvements identified for Phase 1 within the Mobility Assessment.
11.3.11	Intercept parking occupancy will be monitored and measured to evaluate parking demand over time. It may result in adjustments to parking location, cost and supply based on data gathered.	n/a	Shared parking strategy is not required until Phase 3.
11.3.12	A combined Shared Parking Strategy for the Innovation and Village Centre Districts should be submitted as a part of the Phase 3 Conceptual Scheme.	n/a	Shared parking strategy is not required until Phase 3.



<b>Item</b>	<b>Goals, Objectives &amp; Policies</b>	<b>Alignment</b>	<b>How?</b>
11.3.13	The Shared Parking Strategy should examine combined parking demand taking into consideration town wide modal split targets, opportunities to work with existing or future businesses, accessibility, daily, hourly and seasonal variations and location of supply. The objective being to provide the desired level of parking in the most efficient manner possible while minimizing the physical impacts, construction and operating cost for all users and may include: <ul style="list-style-type: none"> <li>a. Parking recommendations specific to the lands being assessed;</li> <li>b. Recommendations on parking requirements by use for the area;</li> <li>c. Day visitor parking strategies for potential intercept parking within and/or outside the Plan Area;</li> <li>d. Other transportation demand management strategies aligned to the policies contained within the Plan Area; and</li> <li>e. Consider other parking management strategies outlined in the Town of Canmore's Integrated Parking Management Plan.</li> </ul>	yes	Shared parking strategy is not required until Phase 3.
11.3.14	The Town and Three Sisters Mountain Village will enter into discussions to jointly develop and manage the intercept parking area identified in location 3 on Map 23 (Appendix A.4). The intercept parking area could be used to accommodate: <ul style="list-style-type: none"> <li>a. Meet-up area for people to car-pool to destinations within and outside the Plan Area;</li> <li>b. Transit park and ride to destinations within provincial and federal parks;</li> <li>c. Transit park and ride to destination within the Plan Area and Town of Canmore;</li> <li>d. Overnight and oversized vehicle parking for the Plan Area;</li> <li>e. Visitor centre and washrooms;</li> <li>f. Transit timed and transfer point and turn-around;</li> <li>g. Event parking and overflow;</li> <li>h. A potential firehall site; and</li> <li>i. Area surplus to the operational needs for intercept parking may be developed</li> </ul>	n/a	Discussions with the Town on the Transit Hub have already occurred and a preliminary design contemplated. Further discussions will be required when commercial phases are proposed and the Mobility Assessment requires.
11.4	<b>Affordable Housing</b>		
11.4.1	A minimum of 200 market rental units shall be developed within Three Sisters Mountain Village. a. Prior to Phase 3 within the Plan Area, a minimum of 100 market rental units will be constructed; and b. Prior to Phase 5 within the Plan Area, a minimum of an additional 100 market rental units will be constructed.	yes	Phase 1 may have rental units proposed within it. Market rental units must be built prior to Phase 3 and may be constructed anywhere within Three Sisters Mountain Village.
11.4.2	A minimum of 375 Employee Housing beds shall be developed within Three Sisters Mountain Village. a. Prior to Phase 3 within the Plan Area, 200 Employee Housing beds will be constructed; and b. Prior to Phase 5 within the Plan Area, an additional 150 Employee Housing beds will be constructed.	yes	Employee Housing is listed as a use in the TSV Conceptual Scheme proposed R3 TSMV and R4 TSMV Districts. Employee Housing may be proposed anywhere within the TSMV community to satisfy this requirement and only has to be built prior to Phase 3.





<i>Item</i>	<i>Goals, Objectives &amp; Policies</i>	<i>Alignment</i>	<i>How?</i>
11.5	Steep Creek Mitigation		
11.5.1	Mitigation will be designed in detail at Concept Scheme and through detailed risk analysis based on land use and building types finalized at that time.	yes	Steep Creek mitigation has been design and submitted with the TSV Conceptual Scheme.
11.5.2	Upon completion of all offsite mitigation requirements (outside of private development parcels that will be built upon), the hazard zones of the Land Use Bylaw will be updated in accordance with Town policies, at the time of construction of the subdivision.	n/a	To be completed at subdivision stage
11.5.3	Cost sharing related to steep creek mitigations between the Developer and the Town will be determined in alignment with the accepted cost sharing report	n/a	To be negotiated at the subdivision stage
11.5.4	Mitigations shall be constructed prior to the commencement of construction of each subdivision within the ASP.	n/a	To be completed at subdivision stage
11.6	Growth and Phasing		
11.6.1	The conceptual phasing plan for Three Sisters Village is identified on Map 22.	yes	The conceptual scheme is for Phase 1 of TSV as identified in Map 22 of the ASP.
11.6.2	Phases in this ASP may overlap or commence concurrently with phases in other Three Sisters Mountain Village ASP Plan Areas, if they are in alignment with policy.	n/a	
11.6.3	Phases to accommodate commercial development within the Indoor Recreation, Village Centre or Innovation District Areas may commence earlier than what is outlined in Map 23, providing the requirements identified within the EIS are satisfied.	n/a	Phase 1 is being proposed.
11.6.4	Prior to proceeding with Phase 5, the Village Centre shall be generally complete.	n/a	
11.6.5	Development within this ASP shall occur in a sequence that reflects the availability of the required utility infrastructure and roadways and in accordance with this ASP	yes	Phase 1 contains one (1) of the two (2) vehicular access points from Three Sisters Parkway.
11.6.6	Development of future phases will proceed in accordance with the Monitoring and Adaptive Management Plan in accordance with the EIS	yes	TSV Conceptual Scheme complies with the recommendations from the EIS (Please see <b>APPENDIX IV</b> EIS alignment table)
11.6.7	The Resort Recreation Area should be developed in alignment with the development phases on Map 22.	n/a	Area not contained within Phase 1



# Appendix IV

## Three Sisters Village Phase 1 Conceptual Scheme and Land use Resignation Three Sisters Village EIS Compliance Table

Note: Highlighted below are the mitigations that must be addressed at this Conceptual Scheme and Land Use Application stage. How this application meets the requirements are identified in the right hand column.

VEC/Other Wildlife Species	Serious Risk(a) Identified Under Existing Conditions	Pre-mitigation Environmental Consequence	Proposed Mitigation	Post-mitigated Environmental Consequence	Does the Conceptual Scheme comply with the EIS?	How does the Conceptual Scheme comply with the Environmental Impact Statement?
Grizzly Bear	Yes	High	<ul style="list-style-type: none"> <li>Avoid clearing where feasible during the migratory/bird nesting season (April 11 to August 25); if clearing is required during this period perform nest sweep (Pre-construction: Developer and Builder)</li> </ul>	Moderate		<p>The Conceptual Scheme (CS) initiates the recommendations specified in the EIS regarding environmental considerations and mitigation of human-wildlife interactions. A Wildlife Fence (CS, DWG 21) will be initiated at the time of construction as a means of mitigating negative interactions. Open space is being dedicated to start the process of creating recreational uses for humans within the ASP Area, decreasing human use of the corridors. The Monitoring and Adaptive Management Plan (MAAMP) has outlined how TSMV and the Province will evaluate the efficacy of the fence in deterring human/animal interactions and inform, when and where adaptation of the plans will be necessary.</p>
Cougar	No	High	<ul style="list-style-type: none"> <li>Conduct nest sweeps if clearing occurs between March 1 and April 30 to protect active owl nests in accordance with the Alberta Wildlife Act (Pre-construction: Developer and Builder)</li> </ul>	Low		
Wolf	Yes	High	<ul style="list-style-type: none"> <li>Conduct pre-construction surveys to locate sensitive wildlife features (e.g., active nest sites, dens, hibernacula) and surveys targeting amphibians and reptiles (Pre-construction: Developer and Builder)</li> </ul>	Low		
Elk	Yes	High	<ul style="list-style-type: none"> <li><b>Comply with requirements of the Alberta Wetland Policy (At subdivision: Developer)</b></li> </ul>	Low	Yes	
Listed large to medium bodied mammals	No	Negligible	<ul style="list-style-type: none"> <li>Keep construction equipment out of wetlands and riparian areas (Construction: Developer and Builder) without proper erosion and sediment control measures in place and in accordance with Alberta Wetland Policy</li> </ul>	Negligible		
Non-listed large to medium bodied mammals	No	Low	<ul style="list-style-type: none"> <li>Develop and implement an Erosion and Sediment Control Plan as described in the Engineering Design &amp; Construction Guidelines (Construction: Developer)</li> <li>Implement the TSMV Construction Management Guidelines (Construction: Developer and Builder)</li> </ul>	Low		
Listed bat species	No	Moderate	<ul style="list-style-type: none"> <li>Restrict construction activities according to Town bylaw 1997-11 (0700 to 2200 hrs Monday to Saturday; no construction noise on Sundays) (Construction: Developer and Builder)</li> </ul>	Low		
Non-listed small mammals	No	Low	<ul style="list-style-type: none"> <li>Awareness training for employees and contractors to confirm personal awareness of key issues for wildlife and stewardship responsibility while working in the area (e.g., identify opportunities to limit noise and other forms of sensory disturbance) (Construction: Developer and Builder).</li> </ul>	Low		
Birds	No	High	<ul style="list-style-type: none"> <li><b>Final design of the wildlife fence, including design at creek crossings, fence ends, and other aspects will be undertaken at subdivision construction approval and should include the design principles outlined in the EIS. A qualified professional should evaluate the final design of the wildlife fence to confirm that it is consistent with the fence described in the EIS. (Developer)</b></li> </ul>	Low		
Amphibians	No	Moderate	<ul style="list-style-type: none"> <li><b>Implement a phased development approach, including completing the wildlife fence construction prior to occupancy of first subdivision. (Developer)</b></li> </ul>	Low		
Reptiles	No	Moderate		Low		

VEC/Other Wildlife Species	Serious Risk(a) Identified Under Existing Conditions	Pre-mitigation Environmental Consequence	Proposed Mitigation	Post-mitigated Environmental Consequence	Does the Conceptual Scheme Comply with the EIS?	How does the Conceptual Scheme Comply with the Environmental Impact Statement?
			<ul style="list-style-type: none"> <li>Evaluate the efficacy of the fence for 1) excluding large mammals from the ASP footprint, and 2) improving compliance with existing regulations in wildlife corridors. <b>Responsibility: Developer to evaluate the efficacy of the fence against these two metrics. Report to be submitted after completion of construction of Phase 3 and Phase 5. TSMV's contribution to monitoring would continue no longer than the issuance of construction completion certificate for surface works of Phase 5</b></li> <li>Recommend an education and enforcement campaign developed through the Human-Wildlife Coexistence Technical Working Group. Education and enforcement will serve to maximize efficacy of fencing and education in achieving compliance with trail use, off-leash dog use, and seasonal closure regulations within wildlife corridors. (Enforcement: Town and Province; Education: Developer, Town, Province)</li> <li><b>Participate in the Human-Wildlife Coexistence Technical Working Group to develop and implement an adaptive wildlife monitoring plan. (Responsibility: Developer)</b></li> <li>Erect a page wire wildlife fence around the ASP footprint and adjacent TSMV developments (as defined in this EIS) with access points restricted to designated trails or utility maintenance access points or other right of ways (Construction: Developer)</li> <li>Install gates with signs at the public entrances of wildlife corridors to inform users of expected behaviour. (Construction: Developer)</li> <li>Minimize wildlife attractants in accordance with Town Bylaws 2017-10 (Wildlife Attractant Bylaw) and 2016-11 (Recyclables and Waste Disposal Bylaw) (Construction: Developer and Builder)</li> <li>Reduce the negative effects of lighting in accordance with Town bylaw 2018-22 (Section 2.10) (Construction: Developer and Builder).</li> <li><b>Plan trail systems inside the ASP footprint and connect it to existing designated trails where appropriate.</b> TSMV has a memorandum of understanding (MOU) with the Carrière and Area Mountain Bike Association (CAMBA), which outlines a framework for the development, construction and maintenance of mountain bike trails on TSMV property (CAMBA and TSMV 2018). Short-term objectives of the MOU include installing signage and discouraging unsanctioned trail use and building (Pre-Construction: Developer)</li> <li>Provide off-leash areas for dogs as appropriate and approved by the Town of Carrière as Municipal Reserve inside the ASP footprint. (Construction: Developer and Builder)</li> </ul>			



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<p>Native Vegetation Communities</p>	<p>No</p>	<p>Moderate</p>	<p><b>Comply with requirements of the Alberta Wetland Policy (Developer at Conceptual Scheme and subdivision)</b></p> <p><b>Implement the Master Drainage Plan (Developer)</b></p> <ul style="list-style-type: none"> <li>Awareness training for employees and contractors to confirm personal awareness of key issues for environmentally sensitive areas and stewardship responsibility while working in the area (e.g., identify opportunities to avoid disturbances) (Construction: Builders and Developers)</li> <li>Restrict workers and vehicles to designated work areas and will not be permitted to access other parts of the property without written authorization from TSMV (Construction: Builders and Developers)</li> <li>Progressively reclaim areas of temporary disturbance with native species (Post-construction: Developer and Builders)</li> <li>Apply guidelines from Flowering Landscapes of TSMV (Stantec 2004a) and the Woody Plants of TSMV (Stantec 2004b) where feasible and in compliance with any Town of Canmore requirements</li> <li><b>Plan trail system inside the ASP footprint to encourage use of trails (Pre-construction: Developer)</b></li> <li>Apply guidelines from Town bylaws for vegetation maintenance standards for residual and planted vegetation such as plant health care programs and tree protection plans (Construction/Operation: Developer and Builders)</li> <li><b>Implement the Master Drainage Plan (Construction: Developer)</b></li> <li>Develop and implement an Erosion and Sediment Control Plan as described in the Engineering Design &amp; Construction Guidelines (Construction: Developer and Builder)</li> <li>Follow applicable Town bylaws related to herbicides, pesticides, fertilizers, and other chemicals (Construction: Developer and Builder; Operation: Town)</li> <li>Limit use of introduced soils from outside the Bow Valley where feasible and use soil and seed mixtures certified as being Free of noxious weeds (Construction: Developer and Builder)</li> <li>Spray native soil stockpiles with herbicide as needed to kill weed growth (Construction/Operation: Developer and Builder)</li> <li>Seed or plant disturbed soils using Town of Canmore guidelines or Bylaws and over seed with approved seed mixes in seeded areas that have not germinated (Construction: Developer and Builder)</li> <li>Use spot application techniques for herbicides per Town of Canmore Bylaw and Weed Control Bylaw, when required (Construction/Post-Construction: Developer and Builder)</li> </ul>	<p>Low</p>	<p>Yes</p>	<p>The CS aligns with the recommended mitigation measures. Further detail will be provided at later phases of the municipal planning process.</p>
<p>Environmentally Sensitive Areas</p>	<p>No</p>	<p>Moderate</p>	<p><b>Comply with requirements of the Alberta Wetland Policy (Developer at Conceptual Scheme and subdivision)</b></p> <p><b>Implement the Master Drainage Plan (Developer)</b></p> <ul style="list-style-type: none"> <li>Awareness training for employees and contractors to confirm personal awareness of key issues for environmentally sensitive areas and stewardship responsibility while working in the area (e.g., identify opportunities to avoid disturbances) (Construction: Builders and Developers)</li> <li>Restrict workers and vehicles to designated work areas and will not be permitted to access other parts of the property without written authorization from TSMV (Construction: Builders and Developers)</li> <li>Progressively reclaim areas of temporary disturbance with native species (Post-construction: Developer and Builders)</li> <li>Apply guidelines from Flowering Landscapes of TSMV (Stantec 2004a) and the Woody Plants of TSMV (Stantec 2004b) where feasible and in compliance with any Town of Canmore requirements</li> <li><b>Plan trail system inside the ASP footprint to encourage use of trails (Pre-construction: Developer)</b></li> <li>Apply guidelines from Town bylaws for vegetation maintenance standards for residual and planted vegetation such as plant health care programs and tree protection plans (Construction/Operation: Developer and Builders)</li> <li><b>Implement the Master Drainage Plan (Construction: Developer)</b></li> <li>Develop and implement an Erosion and Sediment Control Plan as described in the Engineering Design &amp; Construction Guidelines (Construction: Developer and Builder)</li> <li>Follow applicable Town bylaws related to herbicides, pesticides, fertilizers, and other chemicals (Construction: Developer and Builder; Operation: Town)</li> <li>Limit use of introduced soils from outside the Bow Valley where feasible and use soil and seed mixtures certified as being Free of noxious weeds (Construction: Developer and Builder)</li> <li>Spray native soil stockpiles with herbicide as needed to kill weed growth (Construction/Operation: Developer and Builder)</li> <li>Seed or plant disturbed soils using Town of Canmore guidelines or Bylaws and over seed with approved seed mixes in seeded areas that have not germinated (Construction: Developer and Builder)</li> <li>Use spot application techniques for herbicides per Town of Canmore Bylaw and Weed Control Bylaw, when required (Construction/Post-Construction: Developer and Builder)</li> </ul>	<p>Low</p>	<p>Yes</p>	<p>The CS aligns with the recommended mitigation measures. Further detail will be provided at later phases of the municipal planning process.</p>



VEC/Other Wildlife Species	Serious Risk(a) Identified Under Existing Conditions	Pre-mitigation Environmental Consequence	Proposed Mitigation	Post-mitigated Environmental Consequence	Does the Conceptual Scheme comply with the EIS?	How does the Conceptual Scheme comply with the Environmental Impact Statement?
			<ul style="list-style-type: none"> <li>Follow the Town's Construction and Landscaping Standards and Land Use Bylaw and approved permits, including preparing the site for a Construction Completion Certificate followed by the standard warranty/maintenance period leading up to the Final Acceptance Certificate (Construction/Operation; Developer and Builder)</li> <li>Monitor disturbed areas in ASP after development; spray herbicides as required when new weed infestations are documented per Town of Canmore Weed Control Bylaw. (Operation/Monitoring; Developer until FAC, Town of Canmore after FAC)</li> </ul>			



VEC/Other Wildlife Species	Serious Risk(a) Identified Under Existing Conditions	Pre-mitigation Environmental Consequence	Proposed Mitigation	Post-mitigated Environmental Consequence	Does the Conceptual Scheme comply with the EIS?	How does the Conceptual Scheme comply with the Environmental Impact Statement?
Fish, including species at risk	No	Low	<ul style="list-style-type: none"> <li>• <b>Implement the TSMV Master Drainage Plan: TSV ASP (WSP 2020) (Construction; Developer and Town)</b></li> <li>• Implement the Construction Management Guidelines (TSMV 2015) (Construction; Developer)</li> <li>• Develop and implement a Spill Response Plan (may form part of the Construction Management Plan that will be provided to the Town) (Construction; part of the construction plan, Contractor)</li> <li>• Develop and implement an Erosion and Sediment Control Plan as described in the Engineering Design &amp; Construction Guidelines (Construction; part of the construction plan, Developer)</li> <li>• Develop and implement a Storm Water Management Plan in alignment with the Master Drainage Plan in accordance with the Town Engineering, Design and Construction Guidelines (Town of Canmore 2010) and Stormwater Management Guidelines for the Province of Alberta (CH2M Core &amp; Storie Ltd. 1999) as updated from time to time (Construction; Developer).</li> <li>• Clearly delineate boundary of construction zone (Construction; part of the construction plan; Developer or Builder)</li> <li>• Maintain or restore riparian vegetation (Construction; Developer and Builder)</li> <li>• Locate staging areas at least 20 m from permanent watercourse streambanks (Construction; Developer and Builders)</li> <li>• Adhere to timing windows when working in or near rivers and streams that connect with fish bearing waterbodies (ie, Bow River); the Restricted Activity/ Period for the Bow River is September 1 to April 30 (Construction; Developers and Builders)</li> <li>• Conduct environmental monitoring during in-stream activities in fish-bearing watercourses (Construction; Developer)</li> <li>• Conduct pre-construction surveys for fish and fish habitat in affected watercourses (Pre-construction; Developer)</li> <li>• Isolate streambeds as much as possible during in-stream activities (Construction; Developer)</li> <li>• Limit the time and extent of equipment and vehicles operating in riparian zones or watercourse banks (Construction; Developer and Builders)</li> <li>• Suspend construction activities during wet conditions (heavy rainfall and run-off events) (Construction; Developer and Builders)</li> </ul>	Negligible	Yes	The CS describes a stormwater management facility proposed to manage the runoff at an appropriate release rate while providing water quality treatment. Other mitigations will be implemented at more detailed municipal approval stages.



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			<ul style="list-style-type: none"> <li>Maintain all machinery used for in-stream activities in clean condition and free of fluid leaks to prevent any deleterious substances from entering the water (Construction; Developer, Builder and Contractors)</li> <li>Wash, refuel, and service machinery at least 30 m from the banks of any watercourse (Construction; Developer and Builders)</li> <li>Use of fertilizers, herbicides, pesticides, and other chemicals should follow applicable Town Bylaws (Construction; Developer and Builder; Operation; Town)</li> <li>Washing of vehicles should follow applicable Town Bylaw (Operation; Town)</li> <li>Runoff monitoring conducted by AEP at established stations on the Bow River, downstream from the Project (Operation/ Monitoring; AEP)</li> </ul>			
Surface and bedrock geology, soils and terrain	No	Moderate	<ul style="list-style-type: none"> <li><b>Implement the steep creek hazard mitigation plan for Three Sisters Creek per the Steep Creek Hazard Report approved at ASP (Construction; Developer).</b></li> <li>Inform staff and contractors of proper procedures to follow to limit environmental effects (Construction; Developer and Builders)</li> <li>Apply the Construction Management Guidelines (TSMV 2015) (Construction; Developer and Builders)</li> <li>Develop and apply the Construction Management Plan (Developer and Builder)</li> <li>Apply the Engineering Design &amp; Construction Guidelines (Developer and Builder)</li> <li>Develop and implement an Erosion and Sediment Control Plan as described in the Engineering Design &amp; Construction Guidelines (Construction; part of the construction plan; Developer)</li> <li>Salvage and store surface soil for post-construction site reclamation (Construction; Developer and Builders)</li> <li>Enable the use of stored soils salvaged from Canmore area to improve reclamation success (Construction; Town of Canmore)</li> <li>Protect topsoil stockpiles with prompt revegetation or erosion control measures (Construction; Developer and Builders)</li> <li>Delimitate areas designated for terrain modification (Construction; Developer and Builders)</li> <li>Develop and implement erosion and sediment control plan (e.g., silt fencing around perimeter of cleared areas; promptly revegetating or covering areas of exposed mineral soils; suspending all earthwork activities during and following heavy rainfall) (Construction; Developer and Builders)</li> </ul>	Negligible	Yes	The CS specifies recommendations for mitigating hazards related to past undermining and steep creeks. ER and MR is being proposed in alignment with the policies of the ASP and EIS. Other mitigations will be implemented at more detailed municipal approval stages.



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Surface Water	No	Moderate	<ul style="list-style-type: none"> <li>Inform staff and contractors of proper procedures to follow to limit environmental effects (Construction; Developers and Builders)</li> <li>Apply Construction Management Guidelines (TSMV 2015) (Construction; Developers and Builders; Environmental Monitoring; Province; Inspection; Town)</li> <li>Use of fertilizers, herbicides, pesticides, and other chemicals should follow applicable Town bylaws (Construction; Developers and Builders; Operations; Town bylaws)</li> <li>Develop and apply the Construction Management Plan (Construction; Developers and Builders; Environmental Monitoring; Province; Inspection; Town)</li> <li>Apply the Engineering Design &amp; Construction Guidelines (Construction; Developers and Builders)</li> <li>Develop and implement a Spill Response Plan (may form part of the Construction Management Plan that will be provided to the Town) (Construction; Contractor)</li> <li>Develop and implement an Erosion and Sediment Control Plan as described in the Engineering Design &amp; Construction Guidelines (Construction; Developers and Builders. A qualified Engineer retained by the developer/builder, must monitor sediment and erosion controls)</li> <li>Maintain native vegetation where feasible (Construction; Developers and Builders)</li> <li>Comply with requirements of the Alberta Wetland Policy with respect to native vegetation around aquatic systems (At subdivision; Developer)</li> <li>Reclaim disturbed areas as soon as possible (Construction; Developers and Builders)</li> <li>No direct discharge of surps and equipment rinse water released to surface water (Construction; Developers and Builders)</li> <li>Conduct all in-stream works in accordance with the Water Act, Public Lands Act and Fisheries Act outside of the AEP Restricted Activity Periods (Construction; Developers and Builders)</li> <li>Areas affected by undermining will be mitigated according to provincial regulations (Construction; Developers and Builders)</li> <li><b>Apply the Master Drainage Plan (Construction; Developers and Builders)</b></li> <li>Develop and implement a Storm Water Management Plan in accordance with the Town Engineering, Design and Construction Guidelines (Town of Canmore 2010), Stormwater Management Guidelines for the Province of Alberta and the Master Drainage Plan submitted at ASP (CH2M Core &amp; Storie Ltd. 1999) (Construction; Developers);</li> <li>Water quality monitoring conducted by AEP on the Bow River, downstream from the Project (Operations/Monitoring; AEP)</li> </ul>	Low	Yes	The density bonusing encourages maintaining existing vegetation. Other mitigations will be implemented at more detailed municipal approval stages.
Groundwater	No	Moderate	<ul style="list-style-type: none"> <li>Reclaim disturbed areas as soon as possible (Construction; Developers and Builders)</li> <li>No direct discharge of surps and equipment rinse water released to surface water (Construction; Developers and Builders)</li> <li>Conduct all in-stream works in accordance with the Water Act, Public Lands Act and Fisheries Act outside of the AEP Restricted Activity Periods (Construction; Developers and Builders)</li> <li>Areas affected by undermining will be mitigated according to provincial regulations (Construction; Developers and Builders)</li> <li><b>Apply the Master Drainage Plan (Construction; Developers and Builders)</b></li> <li>Develop and implement a Storm Water Management Plan in accordance with the Town Engineering, Design and Construction Guidelines (Town of Canmore 2010), Stormwater Management Guidelines for the Province of Alberta and the Master Drainage Plan submitted at ASP (CH2M Core &amp; Storie Ltd. 1999) (Construction; Developers);</li> <li>Water quality monitoring conducted by AEP on the Bow River, downstream from the Project (Operations/Monitoring; AEP)</li> </ul>	Low	Yes	The density bonusing encourages maintaining existing vegetation. Other mitigations will be implemented at more detailed municipal approval stages.





VEC/Other Wildlife Species	Serious Risk(a) Identified Under Existing Conditions	Pre-mitigation Environmental Consequence	Proposed Mitigation	Post-mitigated Environmental Consequence	Does the Conceptual Scheme Comply with the EIS?	How does the Conceptual Scheme Comply with the Environmental Impact Statement?
Air	No	Moderate	<ul style="list-style-type: none"> <li>Run equipment with valid emissions and noise controls as per Federal regulations (Construction: Developer and Builder)</li> <li>Vehicles and machinery/run only when in use (Construction: Developer and Builder)</li> <li>Develop and implement an Erosion and Sediment Control Plan as described in the Engineering Design &amp; Construction Guidelines that includes dust control during construction.</li> <li>Consider early paving and sweeping combined with water or chemical suppressants on site roadways. (Construction; part of the construction plan; Developer)</li> <li><b>Design trail network within ASP footprint to access other portions of TSMV and Canmore to encourage non-vehicular transportation where feasible (Pre-construction Conceptual Scheme): TSMV)</b></li> </ul>	Low	Yes	The MR proposed provides trails and encourages non-vehicular transportation in the design of its streets and road network. Other mitigations will be implemented at more detailed municipal approval stages.
Noise	No	Moderate to High	<ul style="list-style-type: none"> <li>Adhere to Canmore Noise Control Bylaw 11.97 (Canmore 1997) (Construction: Developer and Builder: Town responsible for enforcing their Bylaws)</li> <li>Adhere to the Town Land Use Bylaw (2018-22) and associated Town policies and guidelines (e.g., Municipal Development Plan - Bylaw 2016-03) with respect to architectural and landscaping considerations as a minimum to confirm that buildings achieve a harmony of form and materials with the surrounding environment and are framed by natural landscaping, including the potential effects of the Project on views from the Trans-Canada Highway (Construction: Developer)</li> </ul>	Low	Yes	The development will comply with the Canmore Noise Control Bylaw or equivalent regulation.
Visual Resources	No	Moderate	<ul style="list-style-type: none"> <li>Retain native vegetation where feasible, use natural vegetation features and native plant species in keeping with the TSMV Flowering Landscape (2004a) and Woody Plants of TSMV (Stantec 2004b) as per Town bylaws (Construction: Developer)</li> <li>Dispose of debris and slash as per the Construction Management Plan (TSMV 2015) to maintain aesthetic quality of the site (Construction: Developer)</li> <li>Where possible, rehabilitate existing disturbances during Project reclamation (Operator/Reclamation: Developer)</li> <li>Apply appropriate downcast exterior lighting as per Town Land Use Bylaws (Construction: Developer)</li> </ul>	Low	Yes	At the subdivision stage, architectural guidelines will be developed to ensure a cohesive built form that addresses harmony between the built and natural environment. Density bonusing also encourages builders to leave areas of the parcel in a "naturalized state" and utilize low impact development techniques.
Historic Resources	No	High	<ul style="list-style-type: none"> <li><b>Comply with requirements of the Historical Resources Act (Pre-construction/Construction: Developer). Province ensures compliance with Provincial regulations.</b></li> </ul>	Negligible	Yes	A Historical Resources Act approval has been granted.



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Land and Resource Use	No	Moderate	<ul style="list-style-type: none"> <li>Adhere to relevant land use plans and bylaws (Construction/Operation: Developer)</li> <li>As part of Project design, a Public Open Space District and Resort Recreation District will be created (Pre-construction: Developer)</li> <li>Open spaces and trails will be set aside in the ASP footprint, consisting of municipal reserve lands and private open spaces. Multi-use and recreational trails will strategically link development areas to the open space areas (Operation: Developer).</li> <li>Development will continue to mitigate or avoid environmentally sensitive areas where feasible, and will continue to be restricted through the designation of such areas as Environmental Reserve or creditable Municipal Reserve (Operation: Developer)</li> <li>Provide off-leash areas for dogs as appropriate and approved by the Town of Canmore as Municipal Reserve inside the ASP footprint (Construction: Developer and Builder)</li> <li>Conservations easement on TSMV property set aside for the approved 2004 Resort Centre ASP (including a 35 m low human use conservation easement) will remain in place as open space and part of existing corridors. These conservation easements are outside the ASP footprint (Operation: Developer).</li> </ul> <p>Additional amenities within the recreation zone may include opportunities for other recreational activities such as terrain parks, ziplines, and/or rope courses, as well as trails for hiking, cross-country skiing and recreation/mountain biking (Operation: Developer)</p>	Negligible	Yes	<p>The Conceptual Scheme and Land Use application adhere to the ASP policy. Open space has been set aside as both MR and ER and a trail network through the phase established for a variety of different users.</p> <p>Off-leash areas have been considered, but the proximity of another hubman off-leash area less than 500 m away would make another in phase 1 redundant.</p> <p>All areas of the corridor owned by TSMV or under the 35 m conservation area will be transferred to the Province. The fence will sit at the northern edge of this 35 m Conservation Easement.</p> <p>This phase is not adjacent to the Resort Recreation Amenity area so the recreation zone referred to is not part of this phase.</p> <p>Appropriate amenities to the MR have been conceptually identified.</p>

